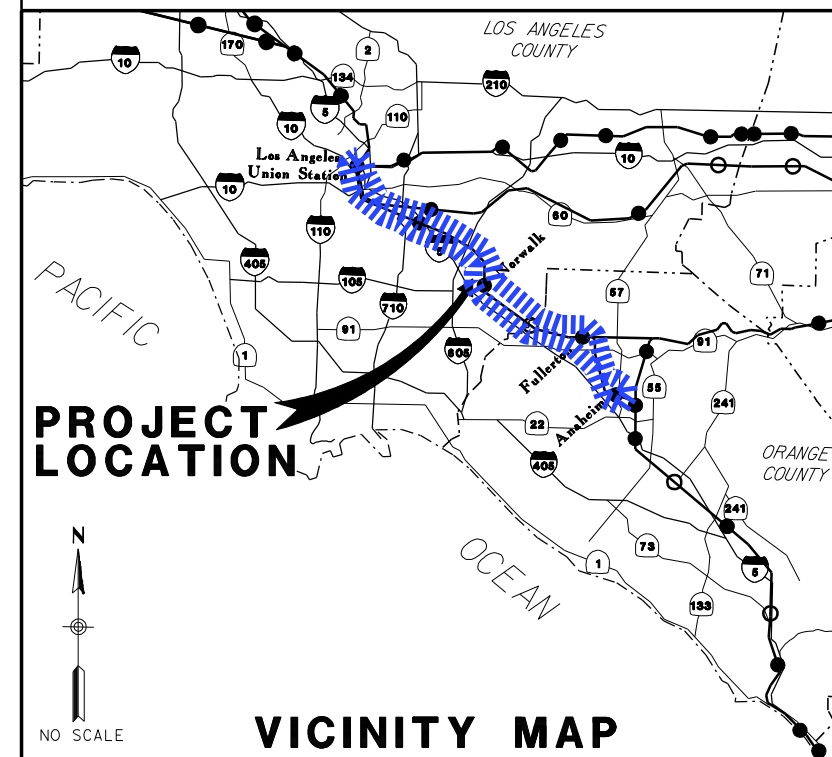
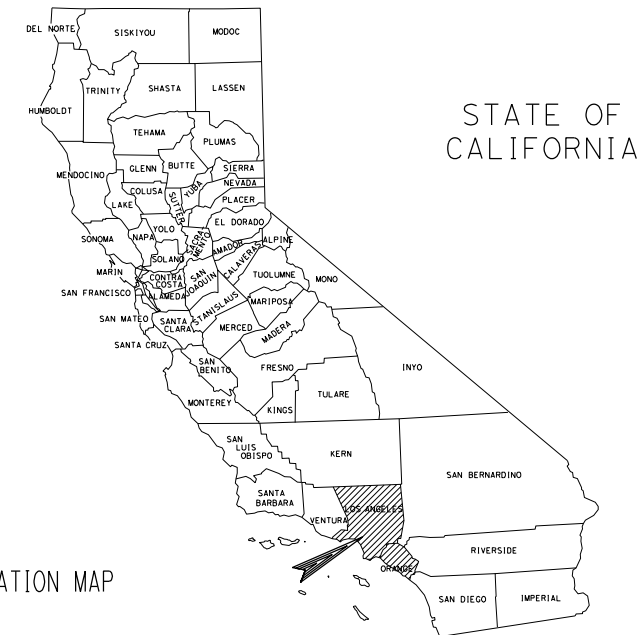




*CALIFORNIA
HIGH - SPEED RAIL
AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.*

**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES SEGMENT**




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**10% DESIGN
IN-PROGRESS REVIEW
FEBRUARY 13, 2009**

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY J. MARTINEZ
DRAWN BY J. MARTINEZ
CHECKED BY P. YANKEY
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
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CALIFORNIA HIGH SPEED RAIL AUTHORITY
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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT

TITLE SHEET AND VICINITY MAP

CONTRACT NO. HSR06-0005
DRAWING NO. B0100
SCALE AS SHOWN
SHEET NO. 1 OF 245

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INDEX OF DRAWINGS					SHEET	DRAWING	DRAWING TITLE	SHEET	DRAWING	DRAWING TITLE
SHEET	DRAWING	DRAWING TITLE								
GENERAL										
1	B0100	TITLE SHEET AND VICINITY MAP	51	T0230	BASELINE ALIGNMENT PLAN AND PROFILE	100	T0279	FULLERTON STATION OPTION 1 ALIGNMENT		
2	B0101	INDEX OF DRAWINGS	52	T0231	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
3	B0102	INDEX OF DRAWINGS	53	T0232	BASELINE ALIGNMENT PLAN AND PROFILE	101	T0280	FULLERTON STATION OPTION 1 ALIGNMENT		
4	B0110	GENERAL SYMBOLS & ABBREVIATIONS	54	T0233	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
5	B0120	KEY MAP	55	T0234	BASELINE ALIGNMENT PLAN AND PROFILE	102	T0281	FULLERTON STATION OPTION 1 ALIGNMENT		
6	B0121	KEY MAP	56	T0235	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
TRACK ALIGNMENT			57	T0236	BASELINE ALIGNMENT PLAN AND PROFILE	103	T0282	FULLERTON STATION OPTION 1 ALIGNMENT		
* 7A	T0300A	STANDARD CROSS SECTION AERIAL	58	T0237	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
* 7B	T0300B	STANDARD CROSS SECTION AT-GRADE	59	T0238	BASELINE ALIGNMENT PLAN AND PROFILE	104	T0283	FULLERTON STATION OPTION 1 ALIGNMENT		
7	T0300	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	60	T0239	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
8	T0301	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	61	T0240	BASELINE ALIGNMENT PLAN AND PROFILE	105	T0284	FULLERTON STATION OPTION 1 ALIGNMENT		
9	T0302	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	62	T0241	BASELINE ALIGNMENT PLAN AND PROFILE			AERIAL STATION PLAN AND PROFILE		
10	T0303	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	63	T0242	BASELINE ALIGNMENT PLAN AND PROFILE	106	T0285	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
11	T0304	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	64	T0243	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
12	T0305	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	65	T0244	BASELINE ALIGNMENT PLAN AND PROFILE	107	T0286	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
13	T0306	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	66	T0245	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
14	T0307	BASELINE ALIGNMENT TYPICAL CROSS SECTIONS	67	T0246	BASELINE ALIGNMENT PLAN AND PROFILE	108	T0287	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
15	T0308	OPTIONAL ALIGNMENTS TYPICAL CROSS SECTIONS	68	T0247	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
16	T0309	OPTIONAL ALIGNMENTS TYPICAL CROSS SECTIONS	69	T0248	BASELINE ALIGNMENT PLAN AND PROFILE	109	T0288	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
17	T0310	OPTIONAL ALIGNMENTS TYPICAL CROSS SECTIONS	70	T0249	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
18	T0311	OPTIONAL ALIGNMENTS TYPICAL CROSS SECTIONS	71	T0250	BASELINE ALIGNMENT PLAN AND PROFILE	110	T0289	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
19	T0400	BASELINE ALIGNMENT TRACK GEOMETRY DATA	72	T0251	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
20	T0401	OPTIONS ALIGNMENT TRACK GEOMETRY DATA	73	T0252	BASELINE ALIGNMENT PLAN AND PROFILE	111	T0290	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
21	T0200	BASELINE ALIGNMENT PLAN AND PROFILE	74	T0253	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
22	T0201	BASELINE ALIGNMENT PLAN AND PROFILE	75	T0254	BASELINE ALIGNMENT PLAN AND PROFILE	112	T0291	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
23	T0202	BASELINE ALIGNMENT PLAN AND PROFILE	76	T0255	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
24	T0203	BASELINE ALIGNMENT PLAN AND PROFILE	77	T0256	BASELINE ALIGNMENT PLAN AND PROFILE	113	T0292	ANHEIM 50’ R/W OPTION 1 ALIGNMENT BORED		
25	T0204	BASELINE ALIGNMENT PLAN AND PROFILE	78	T0257	BASELINE ALIGNMENT PLAN AND PROFILE			TUNNEL PLAN AND PROFILE		
26	T0205	BASELINE ALIGNMENT PLAN AND PROFILE	79	T0258	BASELINE ALIGNMENT PLAN AND PROFILE	UTILITIES				
27	T0206	BASELINE ALIGNMENT PLAN AND PROFILE	80	T0259	BASELINE ALIGNMENT PLAN AND PROFILE	114	U0600	EXISTING COMPOSITE UTILITIES PLAN		
28	T0207	BASELINE ALIGNMENT PLAN AND PROFILE	81	T0260	BASELINE ALIGNMENT PLAN AND PROFILE	115	U0601	EXISTING COMPOSITE UTILITIES PLAN		
29	T0208	BASELINE ALIGNMENT PLAN AND PROFILE	82	T0261	BASELINE ALIGNMENT PLAN AND PROFILE	116	U0602	EXISTING COMPOSITE UTILITIES PLAN		
30	T0209	BASELINE ALIGNMENT PLAN AND PROFILE	83	T0262	BASELINE ALIGNMENT PLAN AND PROFILE	117	U0603	EXISTING COMPOSITE UTILITIES PLAN		
31	T0210	BASELINE ALIGNMENT PLAN AND PROFILE	84	T0263	LAUS OPTION 1 ALIGNMENT WEST BANK STATION PLAN AND PROFILE	118	U0604	EXISTING COMPOSITE UTILITIES PLAN		
32	T0211	BASELINE ALIGNMENT PLAN AND PROFILE	85	T0264	LAUS OPTION 1 ALIGNMENT WEST BANK STATION PLAN AND PROFILE	119	U0605	EXISTING COMPOSITE UTILITIES PLAN		
33	T0212	BASELINE ALIGNMENT PLAN AND PROFILE	86	T0265	LAUS OPTION 1 ALIGNMENT WEST BANK STATION PLAN AND PROFILE	120	U0606	EXISTING COMPOSITE UTILITIES PLAN		
34	T0213	BASELINE ALIGNMENT PLAN AND PROFILE	87	T0266	LAUS OPTION 1 ALIGNMENT WEST BANK STATION PLAN AND PROFILE	121	U0607	EXISTING COMPOSITE UTILITIES PLAN		
35	T0214	BASELINE ALIGNMENT PLAN AND PROFILE	88	T0267	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	122	U0608	EXISTING COMPOSITE UTILITIES PLAN		
36	T0215	BASELINE ALIGNMENT PLAN AND PROFILE	89	T0268	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	123	U0609	EXISTING COMPOSITE UTILITIES PLAN		
37	T0216	BASELINE ALIGNMENT PLAN AND PROFILE	90	T0269	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	124	U0610	EXISTING COMPOSITE UTILITIES PLAN		
38	T0217	BASELINE ALIGNMENT PLAN AND PROFILE	91	T0270	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	125	U0611	EXISTING COMPOSITE UTILITIES PLAN		
39	T0218	BASELINE ALIGNMENT PLAN AND PROFILE	92	T0271	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	126	U0612	EXISTING COMPOSITE UTILITIES PLAN		
40	T0219	BASELINE ALIGNMENT PLAN AND PROFILE	93	T0272	DT JUNCTION OPTION 1 ALIGNMENT AERIAL STRUCTURE PLAN AND PROFILE	127	U0613	EXISTING COMPOSITE UTILITIES PLAN		
41	T0220	BASELINE ALIGNMENT PLAN AND PROFILE	94	T0273	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF	128	U0614	EXISTING COMPOSITE UTILITIES PLAN		
42	T0221	BASELINE ALIGNMENT PLAN AND PROFILE			IMPERIAL HIGHWAY PLAN AND PROFILE	129	U0615	EXISTING COMPOSITE UTILITIES PLAN		
43	T0222	BASELINE ALIGNMENT PLAN AND PROFILE	95	T0274	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF	130	U0616	EXISTING COMPOSITE UTILITIES PLAN		
44	T0223	BASELINE ALIGNMENT PLAN AND PROFILE			IMPERIAL HIGHWAY PLAN AND PROFILE	131	U0617	EXISTING COMPOSITE UTILITIES PLAN		
45	T0224	BASELINE ALIGNMENT PLAN AND PROFILE	96	T0275	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF	132	U0618	EXISTING COMPOSITE UTILITIES PLAN		
46	T0225	BASELINE ALIGNMENT PLAN AND PROFILE			IMPERIAL HIGHWAY PLAN AND PROFILE					
47	T0226	BASELINE ALIGNMENT PLAN AND PROFILE	97	T0276	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF					
48	T0227	BASELINE ALIGNMENT PLAN AND PROFILE			IMPERIAL HIGHWAY PLAN AND PROFILE					
49	T0228	BASELINE ALIGNMENT PLAN AND PROFILE	98	T0277	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF					
50	T0229	BASELINE ALIGNMENT PLAN AND PROFILE			IMPERIAL HIGHWAY PLAN AND PROFILE					
			99	T0278	NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF					
					IMPERIAL HIGHWAY PLAN AND PROFILE					
* NOTE: SHEETS 7A AND 7B CREATE A TOTAL SHEET COUNT OF 245. BUT BECAUSE OF SHEET 7A AND 7B THE LAST SHEET WILL BE 243 OF 245										
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						CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES ALIGNMENT / OPTION INDEX OF DRAWINGS				CONTRACT NO. HSR06-0005
										DRAWING NO. B0101
										SCALE AS SHOWN
										SHEET NO. 2 OF 245
REV	DATE	BY	SUB	APP	DESCRIPTION					

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ACRONYMS

A	
AA	AFFECTED AGENCY
AACE	ASSOCIATION FOR THE ADVANCEMENT OF COST ENGINEERING
MDT	AVERAGE ANNUAL DAILY TRAFFIC
AAR	ASSOCIATION OF AMERICAN RAILROADS
AASHTO	AMERICAN ASSOCIATION OF STATE HIGHWAY TRANSPORTATION OFFICIALS
AC	ASPHALT CONCRETE; ALTERNATING CURRENT
ACI	AMERICAN CONCRETE INSTITUTE
ACHP	ADVISORY COUNCIL ON HISTORIC PRESERVATION (FEDERAL)
ACR	ASSEMBLY CONCURRENT RESOLUTION
ADA	AMERICANS WITH DISABILITIES ACT (FEDERAL)
ADT	AVERAGE DAILY TRAFFIC
A&E	ARCHITECTURAL AND ENGINEERING
AIS	APPEARANCE INFORMATION SHEET
AC	ASPHALT CONCRETE
ACI	AMERICAN CONCRETE INSTITUTE
ACHP	ADVISORY COUNCIL ON HISTORIC PRESERVATION (FEDERAL)
ACR	ASSEMBLY CONCURRENT RESOLUTION
ACSR	ALUMINUM CONDUCTOR STEEL REINFORCED
ADA	AMERICANS WITH DISABILITIES ACT (FEDERAL)
ADAAGA	ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES
ADT	AVERAGE DAILY TRAFFIC
A&E	ARCHITECTURAL AND ENGINEERING
AEC	AERIAL EARTH (GROUND) CONDUCTOR
AIS	APPEARANCE INFORMATION SHEET
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
ANAC	APPROACHING NOISE ABATEMENT CRITERIA
ANSI	AMERICAN NATIONAL STANDARDS INSTITUTE
APCD	ALR POLLUTION CONTROL DISTRICT
APE	AREA OF POTENTIAL EFFECTS
APEFZ	ALQUIST-PRIOLO EARTHQUAKE FAULT ZONE
AQMD	AIR QUALITY MANAGEMENT DISTRICT
AREMA	AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION
ASC	ACCOUNTING SERVICE CENTER (CAFTANS HQ)
ASCE	AMERICAN SOCIETY OF CIVIL ENGINEERS
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
AT	AUTOTRANSFORMER
ATC	APPLIED TECHNOLOGY COUNCIL
AWS	STRUCTURAL WELDING STANDARDS
B	
BDA	BRIDGE DESIGN AIDS
BDD	BRIDGE DESIGN DETAILS
BDP	BRIDGE DESIGN PRACTICE
BDS	BRIDGE DESIGN SPECIFICATIONS
BEG	BURIED EARTH (GROUND) CONDUCTOR
BEES	BASIC ENGINEERING ESTIMATING SYSTEM
BNCF	BURLINGTON NORTHERN SANTA FE RAILROAD
BMPs	BEST MANAGEMENT PLANS
BR	BRIDGE RESTORATION AND REPLACEMENT PROGRAM (FEDERAL)
BRT	BUS RAPID TRANSIT
BSO	BASIC SAFETY OBJECTIVE
C	
CA	CERTIFICATION ACCEPTANCE
CAAA	CLEAN AIR ACT AMENDMENTS (FEDERAL)
CADD	COMPUTER-AIDED DESIGN AND DRAFTING
CAH	CONTROLLED ACCESS HIGHWAY
CALNET	CALIFORNIA INTEGRATED TELECOMMUNICATIONS NETWORK
CALTRANS	CALIFORNIA DEPARTMENT OF TRANSPORTATION
CAM	COOPERATIVE AGREEMENT MANUAL
CAR	COOPERATIVE AGREEMENT REPORT
CBC	CALIFORNIA BUILDING CODE
CBDM	CALIFORNIA DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL
CCC	CALIFORNIA COASTAL COMMISSION (STATE)
CCEM	CAPITAL COST ESTIMATING METHODOLOGY
CCEP	CAPITAL COAT ESTIMATING PROGRAM
CCEWG	CAPITAL COST ESTIMATING WORKING GROUP
CCJPA	CAPITAL CORRIDOR JOINT POWERS AUTHORITY
CCO	CONTRACT CHANGE ORDER
CCS	CALIFORNIA COORDINATE SYSTEM

CCTV	CLOSED CIRCUIT TELEVISION
C-D	COLLECTOR-DISTRIBUTOR
CDC	CHST DESIGN CRITERIA
CDF	CALIFORNIA DEPARTMENT OF FORESTRY (STATE)
C-E	CONSTRUCTION EVALUATED
CE1.	CATEGORICAL EXCLUSION (FEDERAL)
CE2.	CATEGORICAL EXEMPTION (STATE)
CEC	CALTRANS ENCROACHMENT COMMITTEE (OBSOLETE)
CEG	CERTIFIED ENGINEERING GEOLOGIST
CEQA	CALIFORNIA ENVIRONMENTAL QUALITY ACT (STATE)
CES	CUSTOMER EMERGENCY STATIONS
CFR	CODE OF FEDERAL REGULATIONS
CFS	CHST FACILITY STANDARDS
CGS	CALIFORNIA GEOLOGICAL SURVEY
CHD	COUNTY HEALTH DEPARTMENT
CHP	CALIFORNIA DEPARTMENT OF HIGHWAY PATROL (STATE)
CHST	CALIFORNIA HIGH-SPEED TRAIN
FTIP	FEDERAL TRANSPORTATION IMPROVEMENT PROGRAM (FEDERAL)
G	
GBR	GEOTECHNICAL BASELINE REPORT
GBR-B	GEOTECHNICAL BASELINE REPORT FOR BIDDING
GBR-C	GEOTECHNICAL BASELINE REPORT FOR CONSTRUCTION
GDR	GEOTECHNICAL DATA REPORT
GIS	GEOGRAPHIC INFORMATION SYSTEM
GMA	GROUND MOTION ANALYSIS
GO	GENERAL ORDER
GS	(DEPARTMENT OF) GENERAL SERVICES (STATE)
GSHA	GEOLOGIC AND SEISMIC HAZARDS ANALYSIS
GTGM	FHWA GEOTECHNICAL TECHNICAL GUIDANCE MANUAL
H	
HD	HARD DRAWN
HDM	HIGHWAY DESIGN MANUAL
HDPE	HIGH DENSITY POLYETHYLENE
HES	HAZARD ELIMINATION SAFETY PROGRAM (FEDERAL)
HIA	HIGHWAY IMPROVEMENT AGREEMENT
HOV	HIGH-OCCUPANCY VEHICLE
HP	HIGHWAY PLANTING
HP&R	HIGHWAY PLANTING AND RESTORATION
HPSR	HISTORIC PROPERTIES SURVEY REPORT
HQ	CALTRANS HEADQUARTERS
HSR	HIGH-SPEED RAIL
HST	HIGH-SPEED TRAIN
HV	HIGH VOLTAGE
HW	HAZARDOUS WASTE
HWMP	HAZARDOUS WASTE MANAGEMENT PLAN
I	
IA	INTERAGENCY AGREEMENT
IBC	INTERNATIONAL BUILDING CODE
IC	INTERSTATE COMPLETION PROGRAM (FEDERAL)
IFA	INVOLVED FEDERAL AGENCY
IGR	INTERGOVERNMENTAL REVIEW
IM	INTERSTATE MAINTENANCE PROGRAM (FEDERAL)
IMP	IMPEDANCE BAND
IR	INFRARED
IRR	INTERCITY RAIL PROGRAM (STATE)
IRRS	INTERREGIONAL ROAD SYSTEM (STATE)
IRS	INTERREGIONAL ROAD SYSTEM PROGRAM (STATE)
IS	INITIAL STUDY (CEQA)
ISA	INITIAL SITE ASSESSMENT
ISRM	INTERNATIONAL SOCIETY FOR ROCK MECHANICS
ISTEA	INTERMODAL SURFACE TRANSPORTATION EFFICIENCY ACT OF 1991 (FEDERAL)
ITMS	INTERMODAL TRANSPORTATION MANAGEMENT SYSTEM
J	
JPB	JOINT POWERS BOARD
K	
KP	KILOMETER POST; KILOMETRIC POINT
KPA	KILO PASCAL
KPH/KPH	KILOMETERS PER HOUR
L	
LA	LOS ANGELES (CALIFORNIA, USA); LOCAL AGENCY; LANDSCAPE ARCHITECT

LAAPS	LOCAL AGENCY AUTOMATED PAY SYSTEM
LADWP	LOS ANGELES DEPARTMENT OF WATER AND POWER
LAP	LANDSCAPE ARCHITECTURE PROGRAM, OF THE DOD(CALTRANS HQ)
LAQMD	LOCAL AIR QUALITY MANAGEMENT DISTRICT
LAUS	LOS ANGELES UNION STATION
LCC	LOCAL COASTAL COMMISSION
LCCA	LIFE CYCLE COST ANALYSIS
LDBE	LOWER-LEVEL DESIGN BASIS EARTHQUAKE
LEDPA	LEAST ENVIRONMENTALLY DAMAGING, PRACTICABLE ALTERNATIVE
LEQ	EQUIVALENT SOUND LEVEL
LOC	LOCALLY FUNDED STATE HIGHWAY PROJECTS
LOS	LEVEL OF SERVICE
LOTB	LOGS OF TEST BORINGS
LRFD	LOAD AND RESISTANCE FACTOR DESIGN
LRT	LIGHT RAIL TRANSIT
LSA	LOW SUN ANGLE
LTC	LOCAL TRANSPORTATION COMMISSION
LUSAS	CIVIL ENGINEERING ANALYTICAL SOFTWARE
LV	LOW VOLTAGE
M	
M	METER
MCE	MAXIMUM CONSIDERED EARTHQUAKE
METS	OFFICE OF MATERIALS ENGINEERING AND TESTING SERVICES (CALTRANS HQ)
MHW	MEAN HIGHEST HIGH WATER
MIS	MAJOR INVESTMENT STUDY
MM	MILLIMETER
MOA	MEMORANDUM OF AGREEMENT
MOU	MEMORANDUM OF UNDERSTANDING
MP	MILE POST
MPH/MPH	MLLES PER HOUR
MPO	METROPOLITAN PLANNING ORGANIZATIONS
MSA	MANAGEMENT SYSTEMS ACTIVITIES
MTD	MEMO TO DESIGNERS
MTMC	MILITARY TRAFFIC MANAGEMENT COMMAND (FEDERAL)
MW	MESSENGER WIRE
N	
NAAQS	NATIONAL AMBIENT AIR QUALITY STANDARDS
NAC	NOISE ABATEMENT CRITERIA
NB	NORTHBOUND
NBSSR	NOISE BARRIER SCOPE SUMMARY REPORT
ND	NEGATIVE DECLARATION (CEQA)
NEPA	NATIONAL ENVIRONMENTAL POLICY ACT (FEDERAL)
NEHRP	NATIONAL EARTHQUAKE HAZARDS REDUCTION PROGRAM
NESC	NATIONAL ELECTRIC SAFETY CODE
NF	NEGATIVE FEEDER
NFPA	NATIONAL FIREROTECTION ASSOCIATION
NGA	NEXT GENERATION OF ATTENUATION
NH	NATIONAL HIGHWAY SYSTEM PROGRAM (FEDERAL)
NHI	NATIONAL HIGHWAY INSTITUTE
NHS	NATIONAL HIGHWAY SYSTEM
NIST	NATIONAL INSTITUTE OF STANDARDS AND TECHNOLOGY
NOD	NOTICE OF DETERMINATION (CEQA)
NO1	NOTICE OF INTENT (NEPA)
NOP	NOTICE OF PREPARATION (CEQA)
NPDES	NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM
NPRM	NOTICE OF PROPOSED RULE MAKING
O	
OA	OBLIGATION AUTHORITY (FEDERAL)
OC	OVERCROSSING
OCS	OVERHEAD CONTACT SYSTEM
OH	OTHER HIGHWAY CONSTRUCTION PROGRAM (STATE)
OJT	ON-THE-JOB TRAINING
OLPPD	OFFICE OF LOCAL PROGRAMS, PROCEDURES DEVELOPMENT
OQE	OFFICE OF OFFICE ENGINEER, OF THE DES (CALTRANS HQ)
OPL	OPERABILITY PERFORMANCE LEVEL
OPR	OFFICE OF PLANNING AND RESEARCH (STATE)
OSD	OFFICE OF STRUCTURE DESIGN, OF THE ESC (CALTRANS HQ)
OSF	OFFICE OF STRUCTURE FOUNDATIONS, OF THE ESC (CALTRANS HQ)
OSHA	OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION
OSM&I	OFFICE OF STRUCTURES MAINTENANCE AND INVESTIGATIONS (CALTRANS HQ)
O&M	OPERATIONS AND MAINTENANCE
	PROJECT APPROVAL REPORT (OBSOLETE) AUTORIZATION REQUEST
P	
PAR	(OBSOLETE)
PC	PERSONAL COMPUTER
PCC	PORTLAND CEMENT CONCRETE
PCR	PROJECT CHANGE REQUEST
PCPT	PIEZOCONE PENETROMETER TEST
PCJPB	PENINSULA CORRIDOR JOINT POWERS BOARD
PD	PROJECT DEVELOPMENT
PDDM	FHWA PROJECT DEVELOPMENT AND DESIGN MANUAL

PDF	PERSONAL DOCUMENT FILE
PDPM	CALTRANS PROJECT DEVELOPMENT PROCEDURES MANUAL
PDT	PROJECT DEVELOPMENT TEAM
DU	DESIGN UNIT
PDWTM	PROJECT DEVELOPMENT WORKFLOW TASKS MANUAL
PE	PROJECT ENGINEER
PEE	PRELIMINARY ENVIRONMENTAL EVALUATION
PEER	PACIFIC EARTHQUAKE ENGINEER RESEARCH CENTER
PEIR	PROGRAMMATIC ENVIRONMENTAL IMPACT REPORT
PFDHA	PROBABILISTIC FAULT DISPLACEMENT HAZARD ANALYSIS
PG&E	PACIFIC GAS AND ELECTRIC COMPANY
PHF	PROJECT HISTORY FILE
PID	PROJECT INITIATION DOCUMENT
PIR	PROJECT INFORMATION REPORT
PISA	PROJECT INFORMATION SYSTEMS AND ANALYSIS
PM	PROJECT MANAGER, PROGRAM MANAGER
PMCS	PROJECT MANAGEMENT CONTROL SYSTEM
PMP	PROJECT MANAGEMENT DIVISION (CALTRANS HQ)
PMPM	PROJECT MANAGEMENT PROCEDURES MANUAL
PMS	PAVEMENT MANAGEMENT SYSTEM
PMT	PROGRAM MANAGEMENT TEAM
PPNO	PLANNING PROGRAM NUMBER
P & PPR	PERMIT AND PORT PLANNING REGULATIONS
PR	PROJECT REPORT
PS	PARALLELING STATION (WITH AUTOTRANSFORMER)
PS&E	PLANS, SPECIFICATIONS, AND ESTIMATE
PSHA	PROBABILISTIC SEISMIC HAZARD ASSESSMENT
PSIG	POUNDS PER SQUARE INCH GAUGE
PSP	PRODUCT SAFETY PLAN
PSR	PROJECT STUDY REPORT
PSSR	PROJECT SCOPE SUMMARY REPORT
PSTIP	PROPOSED STATE TRANSPORTATION IMPROVEMENT PROGRAM
PTZ	PAN-TILT-ZOOM
PUC	PUBLIC UTILITIES COMMISSION (STATE)
PUMS	PYPSCAN UNIT II MONITORING
PVC	POLYVINYL CHLORIDE
PY	PERSON YEAR
PYPSCAN	PERSON YEAR AND PROJECT SCHEDULE AND COST ANALYSIS
Q	
QL	QUALITY LEVEL
R	
RAD	REMEDIAL ACTION DESIGN
RAP	RELOCATION ASSISTANCE PROGRAM; REMEDIAL ACTION PLAN
RAS	REHABILITATION AND STABILIZATION PROGRAM (STATE)
RC	REGIONAL CONSULTANT
RCE	REGISTERED CIVIL ENGINEER
RCR	ROUTE CONCEPT REPORT
RE	RESIDENT ENGINEER
RFP	REQUEST FOR PROPOSAL
RFQ	REQUEST FOR QUALIFICATIONS
RI	REMEDIAL INVESTIGATION
RIGS	REMOTE IRRIGATION CONTROL SYSTEM
RIS	RELINQUISHMENT INFORMATION SHEET
RM	REGIONAL MANAGER
ROD	RECORD OF DECISION (NEPA)
RON	RESOLUTION OF NECESSITY
ROW	RIGHT-OF-WAY
ROWM	RIGHT-OF-WAY MANUAL
RPA	RULE OF PARTICULAR APPLICABILITY
RR	RAILROAD
RRR	RESURFACING, RESTORATION, REHABILITATION (3R)
RRRR	RESURFACING, RESTORATION, REHABILITATION. RECONSTRUCTION (4R)
RSTP	REGIONAL SURFACE TRANSPORTATION PROGRAM (FEDERAL)
RTIP	REGIONAL TRANSPORTATION IMPROVEMENT PROGRAM
RTL	READY TO LIST
RTP	REGIONAL TRANSPORTATION PLAN
RTPA	REGIONAL TRANSPORTATION PLANNING AGENCY
RTRI	RAILWAY TECHNICAL RESEARCH INSTITUTE
RU	RESPONSIBLE UNIT
R/W	RIGHT OF WAY
RWOCB	REGIONAL WATER QUALITY CONTROL BOARD (STATE)

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY J. MARTINEZ
DRAWN BY J. MARTINEZ
CHECKED BY P. YANKE
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

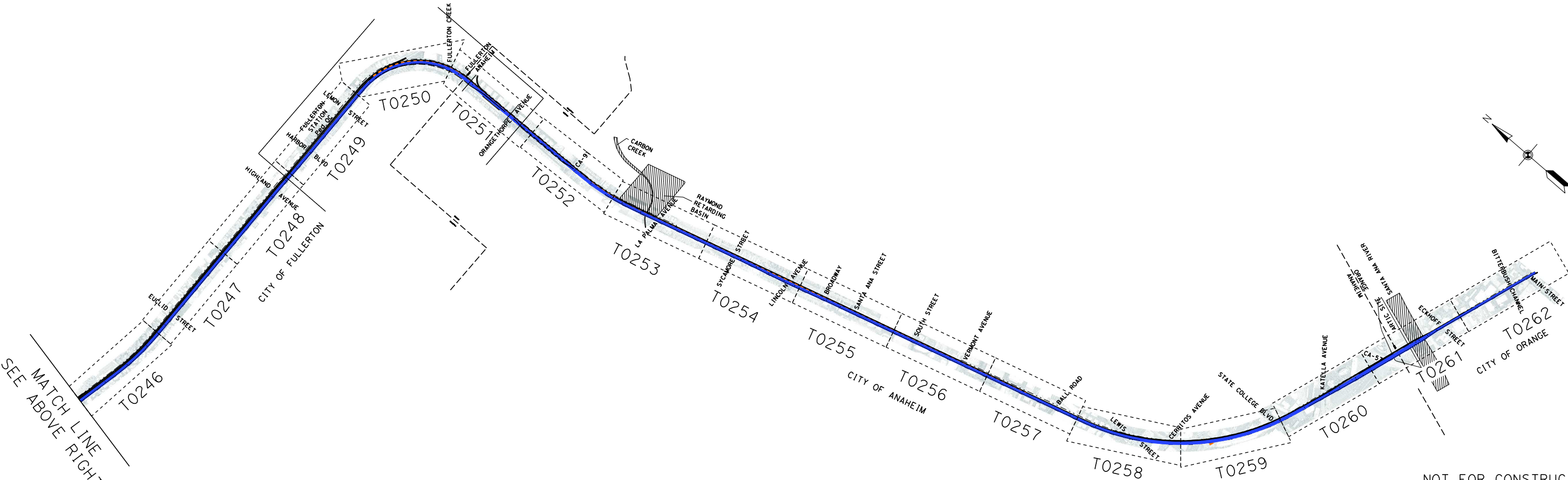
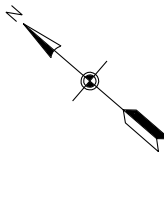
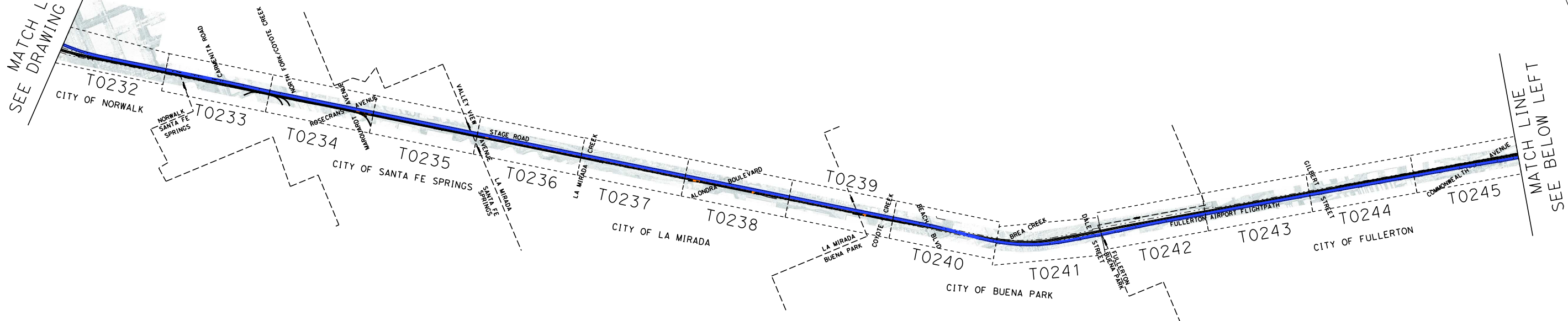
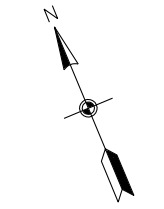


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

GENERAL SYMBOLS AND ABBREVIATIONS

CONTRACT NO. HSR06-0005
DRAWING NO. B0110
SCALE NO SCALE
SHEET NO. 4 OF 245

MATCH LINE
SEE DRAWING B0120



MATCH LINE
SEE ABOVE RIGHT

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\\Drawings\CMVOLA-B-0121-A.dwg
2/12/2009
martin

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
J. MARTINEZ
DRAWN BY
J. MARTINEZ
CHECKED BY
P. YANKEY
IN CHARGE
D. THOMSON
DATE
02-13-09

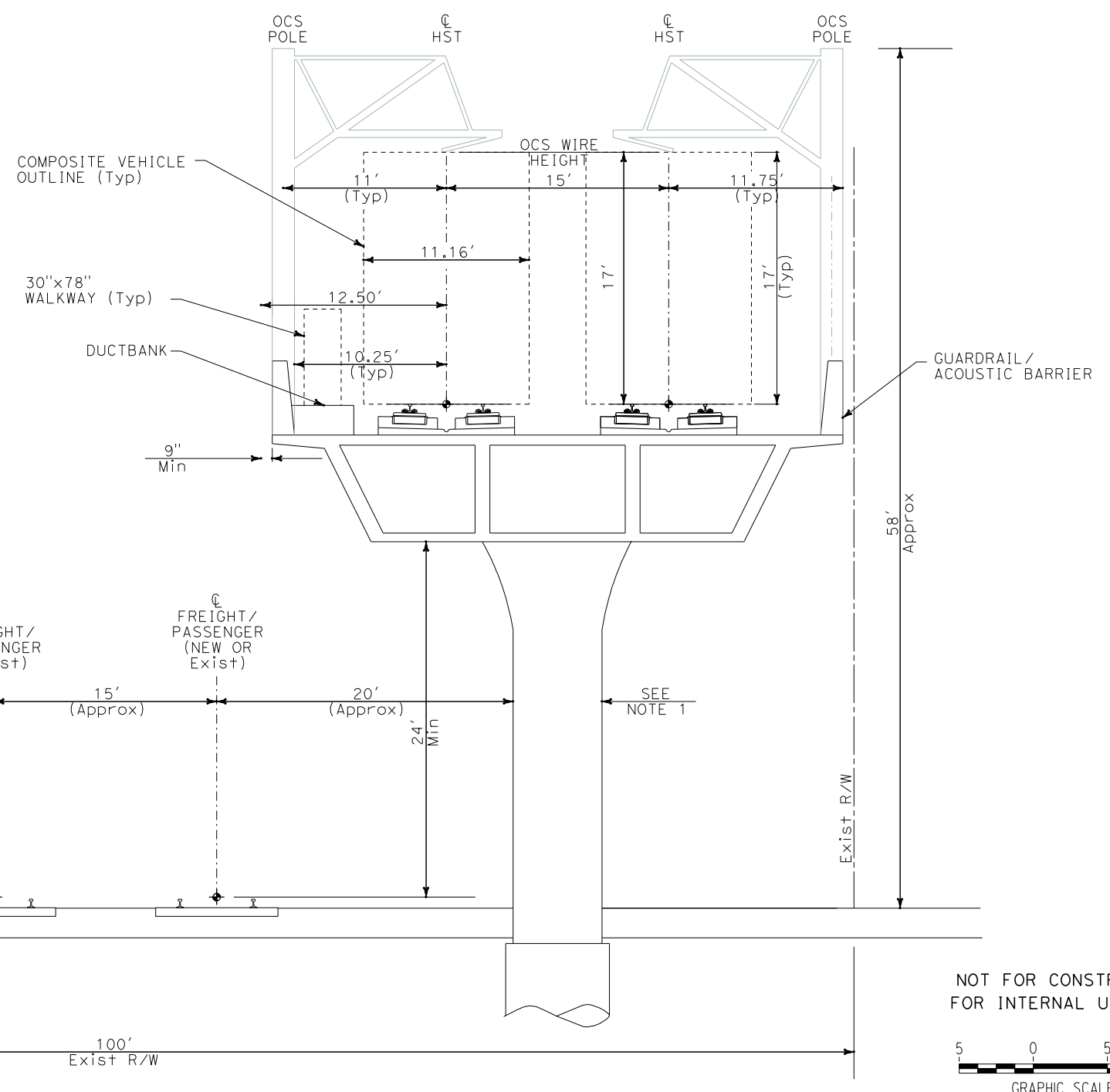
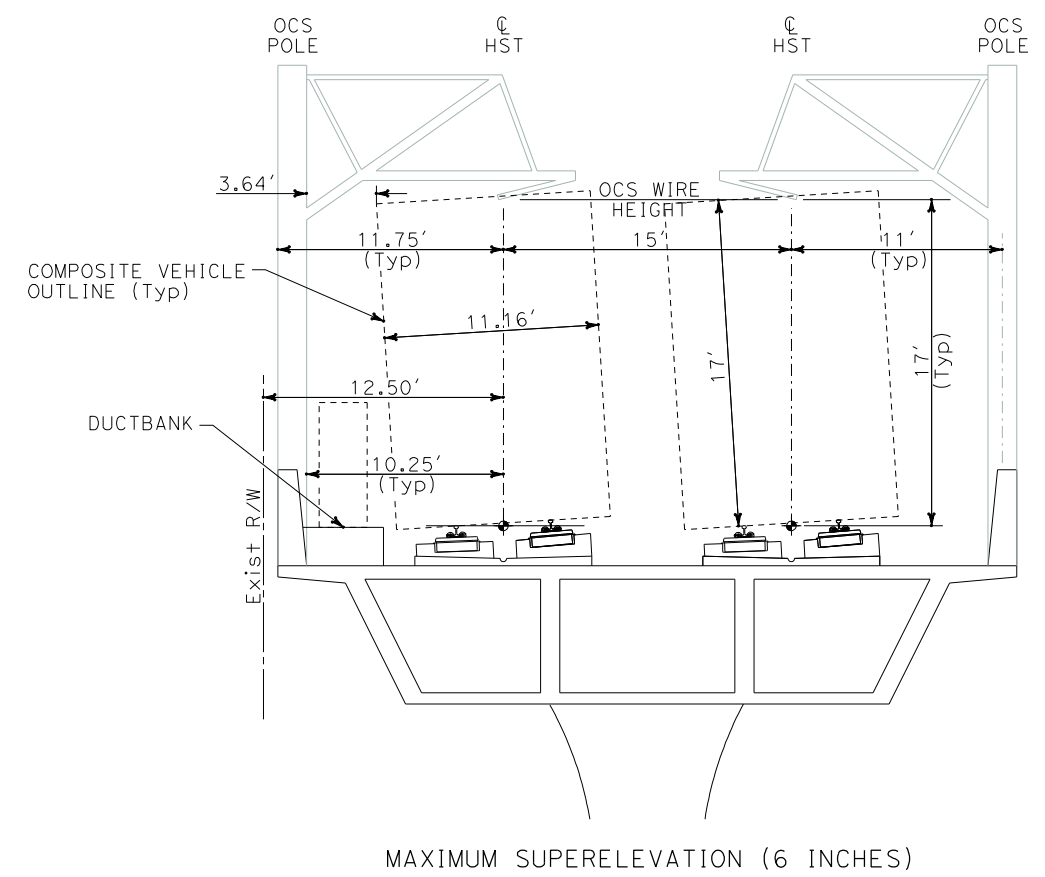


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
KEY MAP

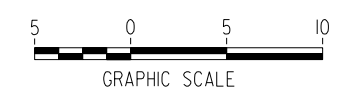
CONTRACT NO.	HSR06-0005
DRAWING NO.	B0121
SCALE	NO SCALE
SHEET NO.	6 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

- NOTES:
1. SIZING OF STRUCTURAL ELEMENTS SHOWN IS CONCEPTUAL. ACTUAL SIZING MAY VARY BASED UPON FINAL DESIGN CONSIDERATIONS.
 2. MAXIMUM WIDTH OF OCS POLES IS ASSUMED TO BE 18".



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

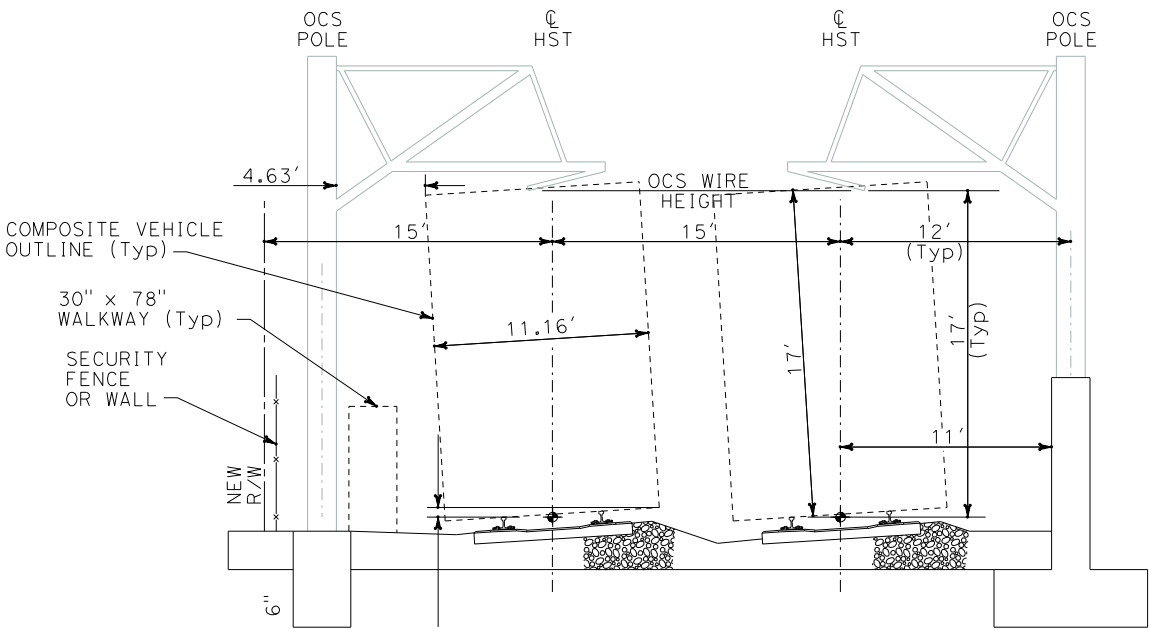
STANDARD CROSS SECTION
AERIAL

CONTRACT NO. HSR06-0005
DRAWING NO. T0300A
SCALE 1" = 5'
SHEET NO. 7A OF 245

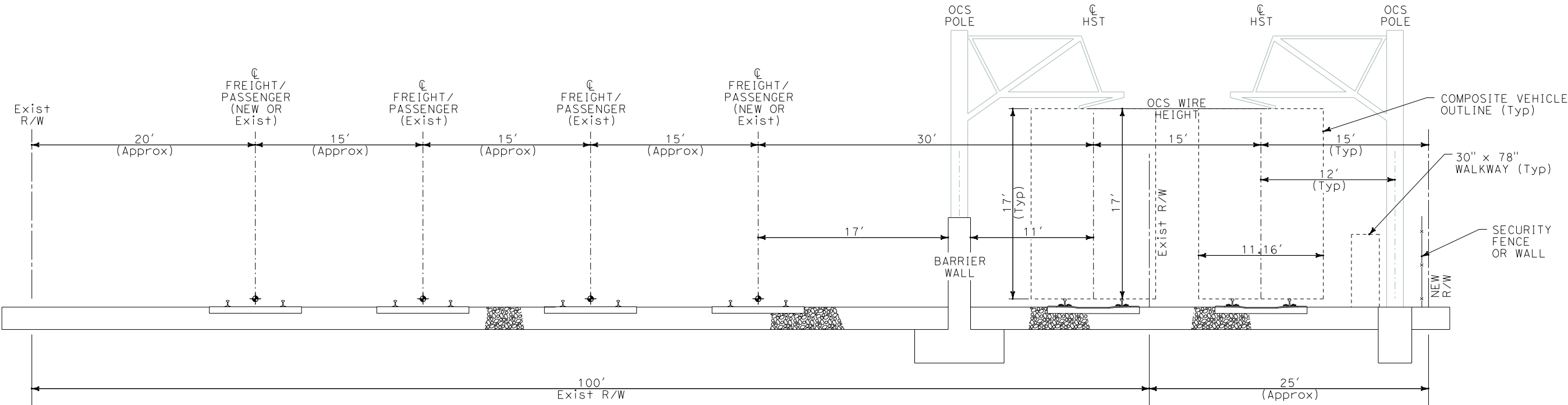
NOTES:

1. SIZING OF BARRIER WALL SHOWN IS CONCEPTUAL. ACTUAL SIZING MAY VARY BASED UPON FINAL DESIGN CONSIDERATIONS.
2. MAXIMUM WIDTH OF OCS POLES IS ASSUMED TO BE 18".

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
MAXIMUM SUPERELEVATION (6 INCHES)



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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





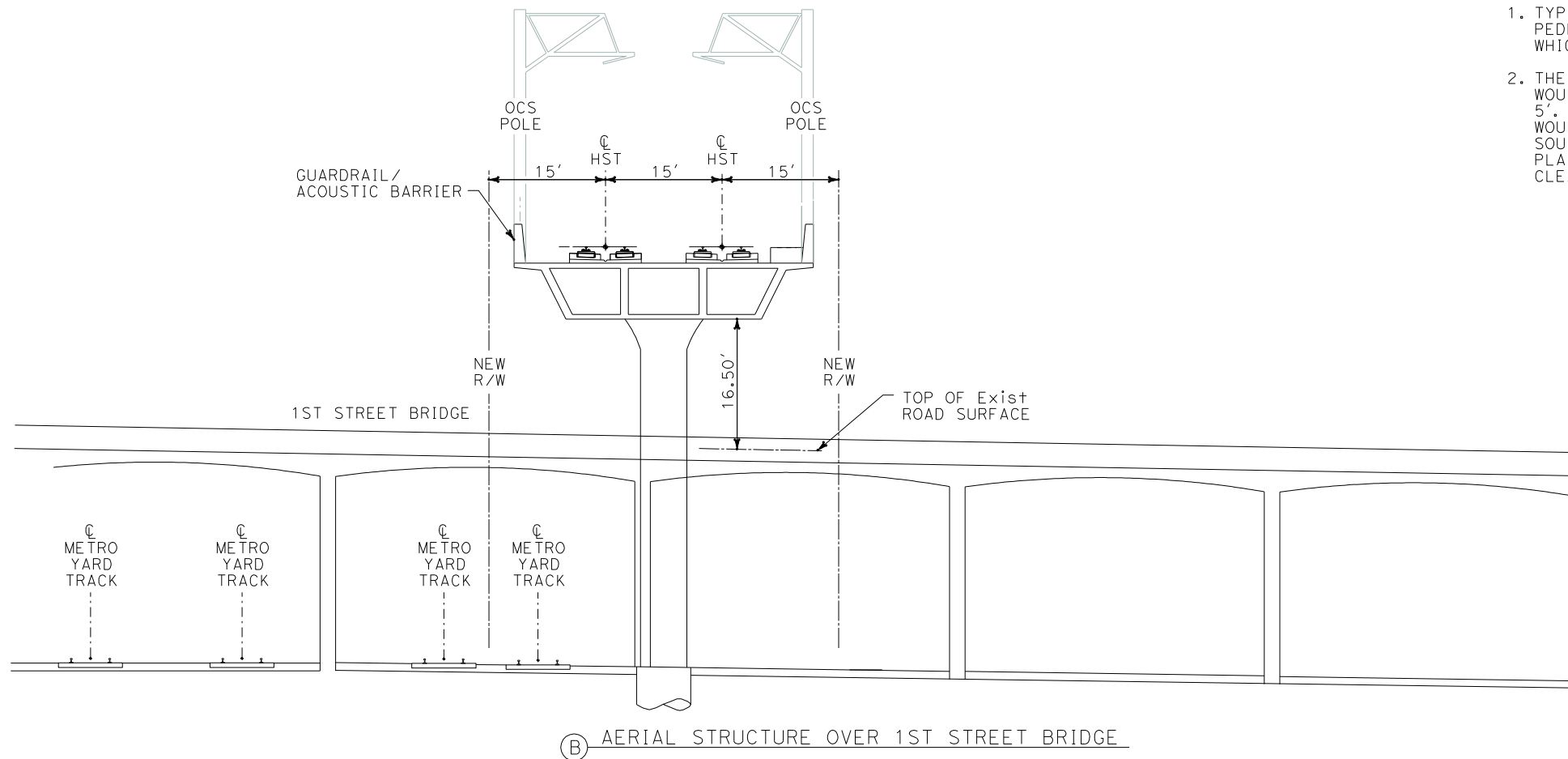
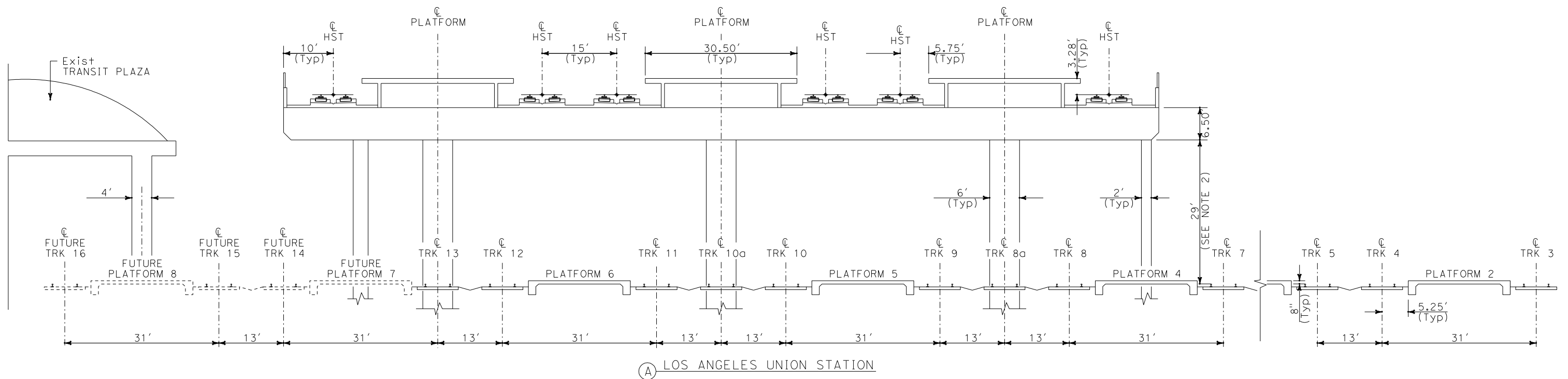
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

STANDARD CROSS SECTION
AT-GRADE

CONTRACT NO. HSR06-0005
DRAWING NO. T0300B
SCALE 1" = 5'
SHEET NO. 7B OF 245

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Sperry.km
\$\$\$\$\$SYTIME\$\$\$\$\$



- NOTE:
1. TYPICAL SECTION AT LAUS DOES NOT INCLUDE PEDESTRIAN ACCESS/BRIDGE OR CANOPIES, WHICH ARE CURRENTLY UNDER DEVELOPMENT.
 2. THE PROPOSED RUN-THROUGH TRACK PROJECT WOULD RAISE TRACKS 3, 4, 5 AND 6 BY APPROX 5'. BECAUSE THE RUN-THROUGH TRACKS WOULD CROSS UNDER THE HST TRACKS JUST SOUTH OF THE STATION, THE HST TRACKS AND PLATFORM ARE SET AT AN ELEVATION THAT CLEARS OVER THE PROPOSED RUN-THROUGH TRACKS.

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

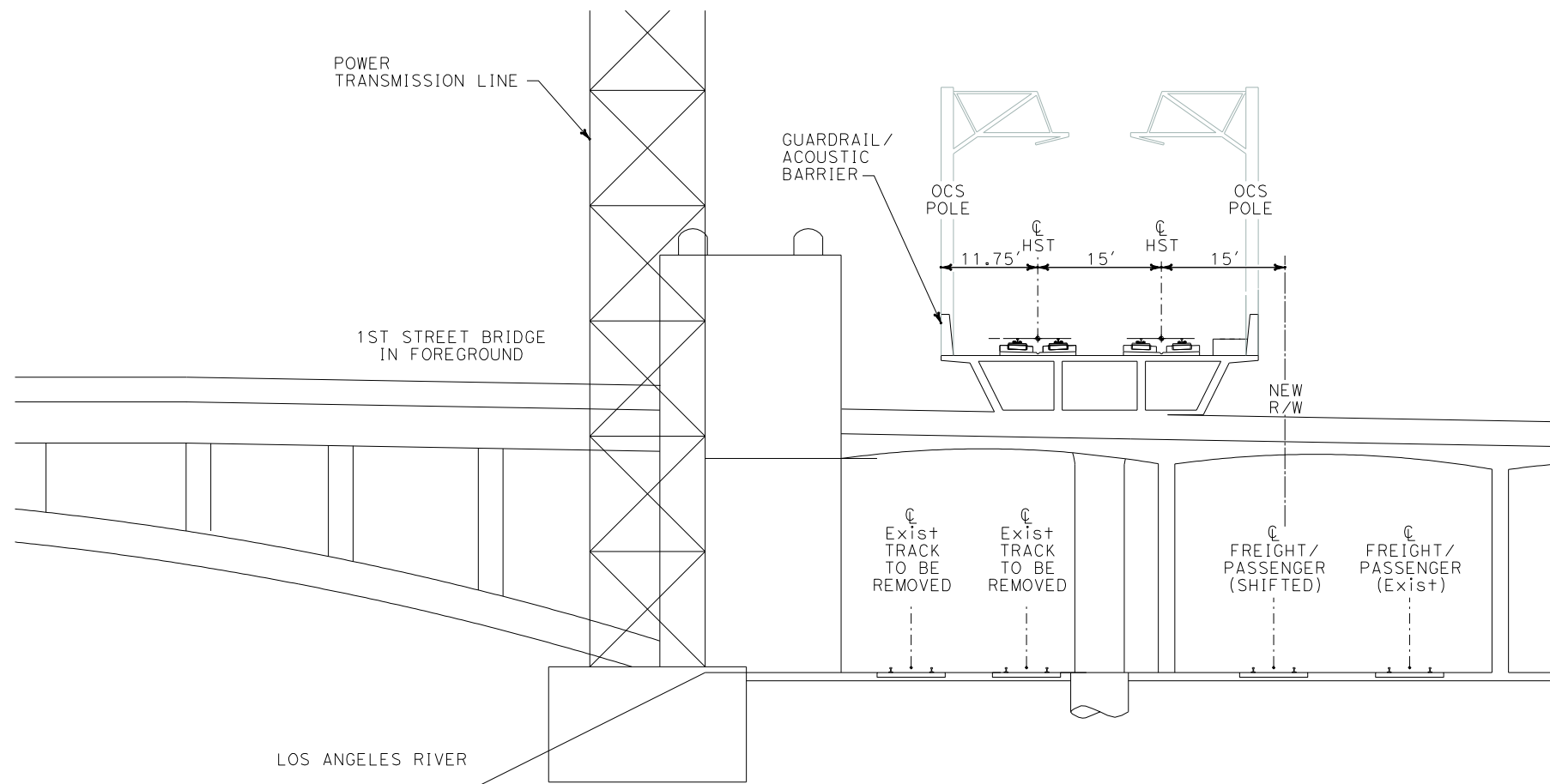
STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



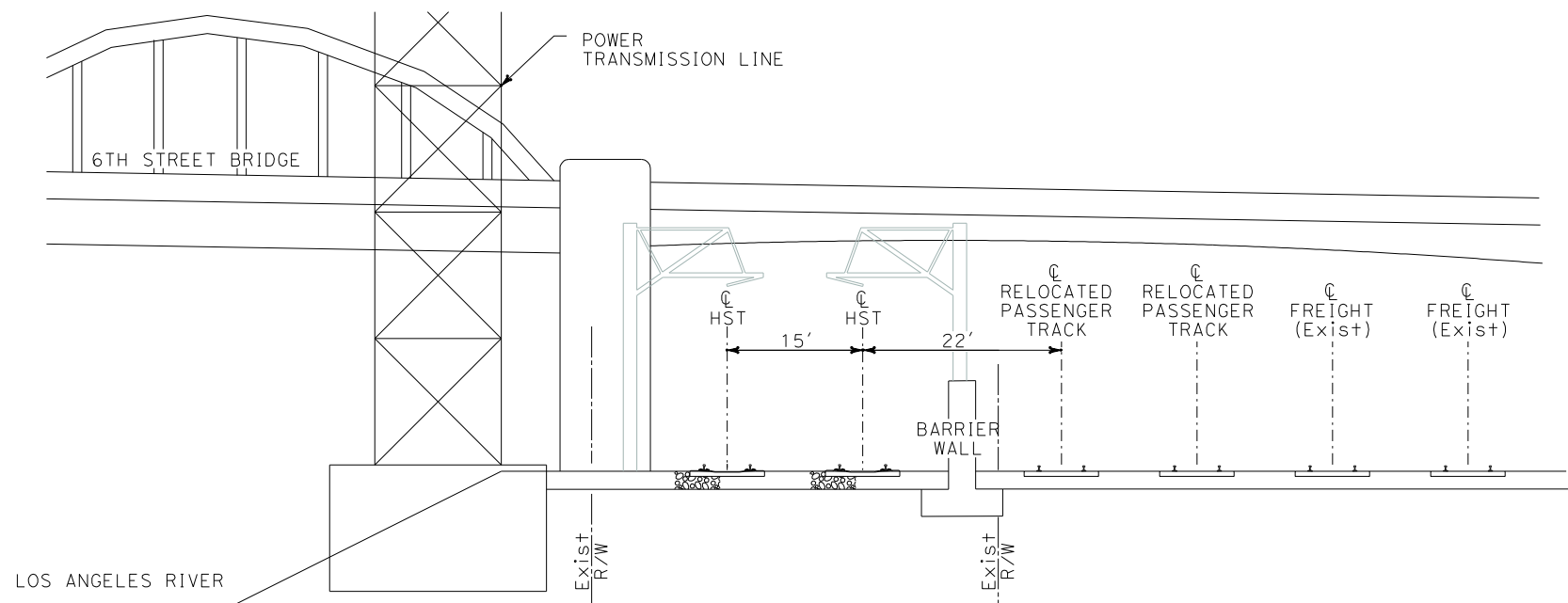
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0300
SCALE 1" = 10'
SHEET NO. 7 OF 245

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Sperry\K.m
\$\$\$\$\$SYTIME\$\$\$\$\$

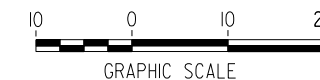


C AERIAL STRUCTURE BETWEEN 1ST AND 4TH STREET BRIDGES



D AT-GRADE ALONG WEST BANK OF LA RIVER

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

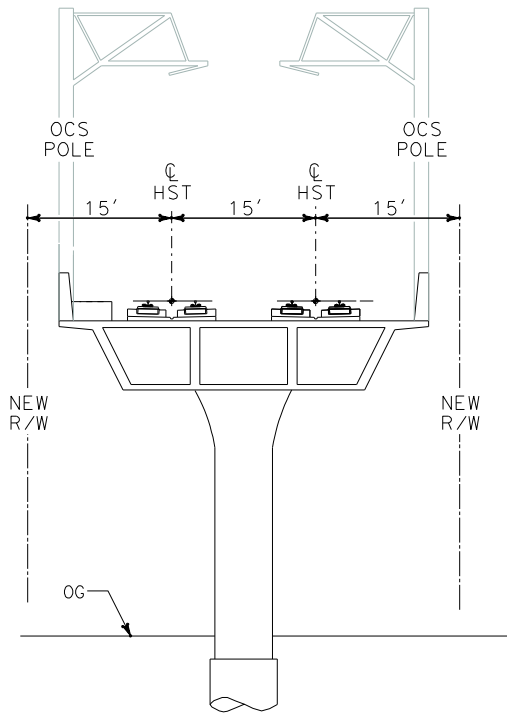


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

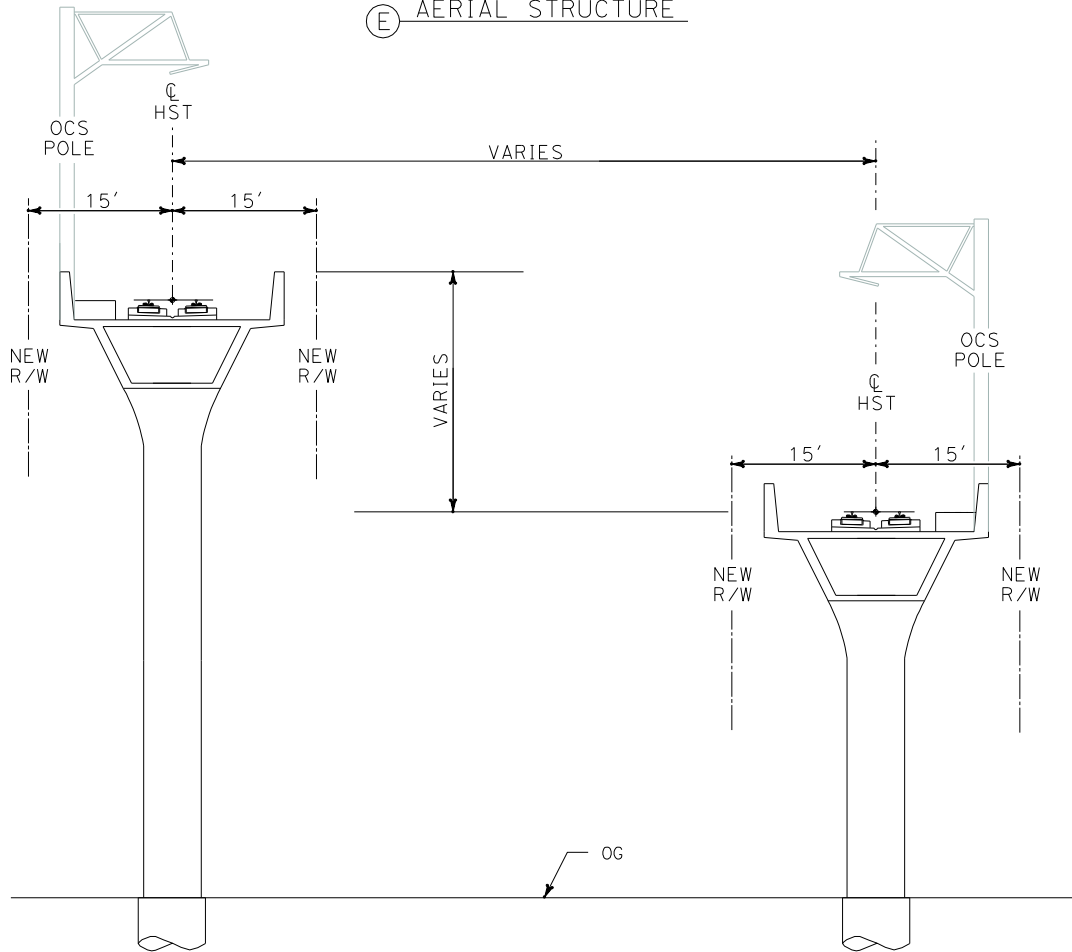
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0301
SCALE 1" = 10'
SHEET NO. 8 OF 245

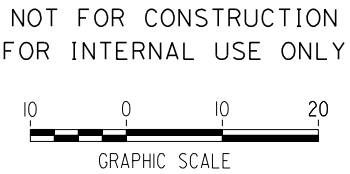
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\$\$\$\$\$SYTIME\$\$\$\$\$



(E) AERIAL STRUCTURE



(F) SEPARATE NB AND SB AERIAL STRUCTURES



REV	DATE	BY	SUB	APP	DESCRIPTION

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DRAWN BY K. SPERRY
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IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

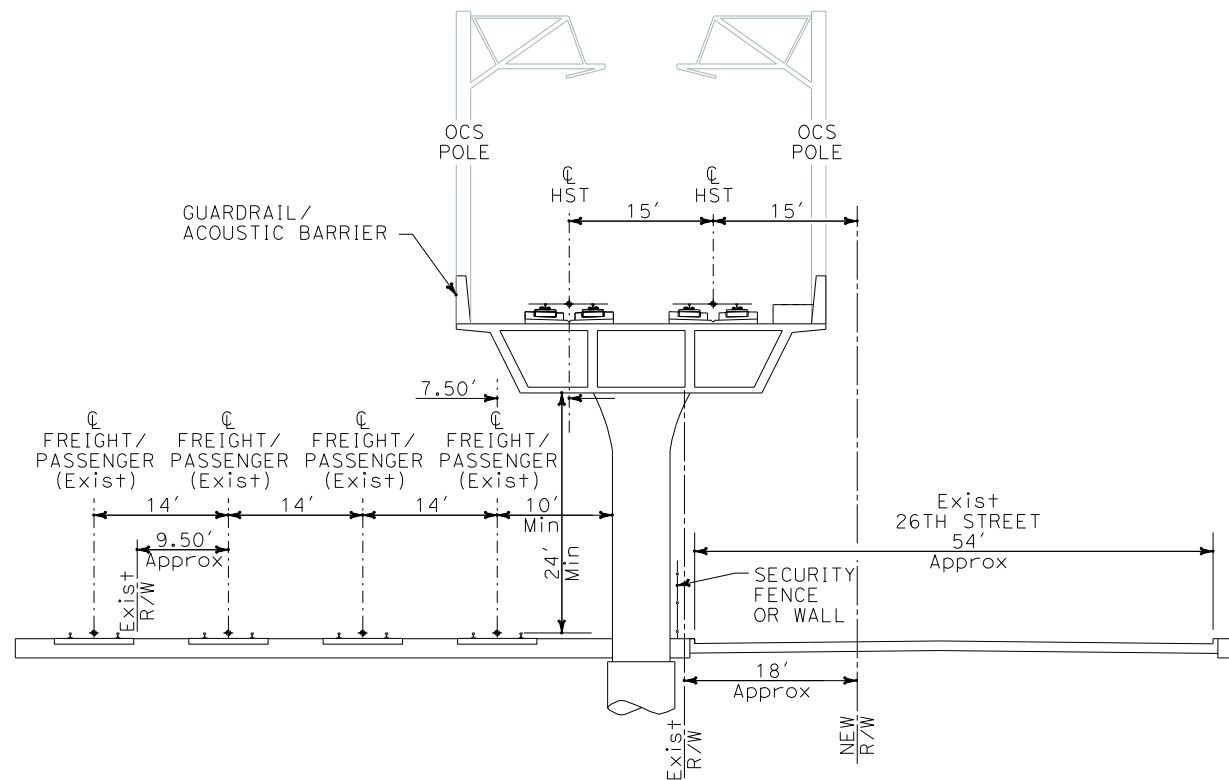


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

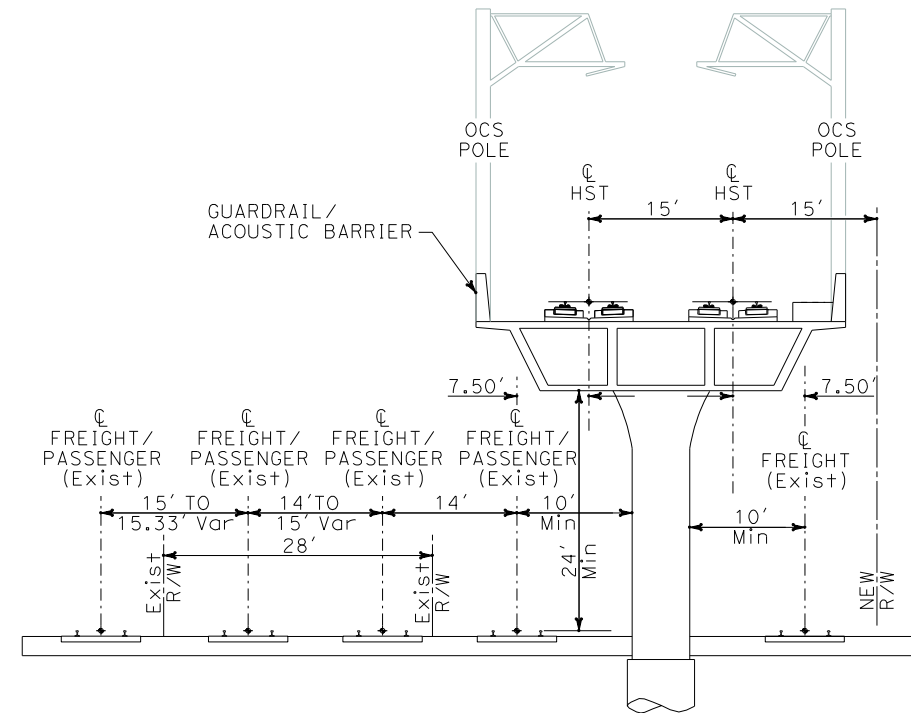
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0302
SCALE 1" = 10'
SHEET NO. 9 OF 245

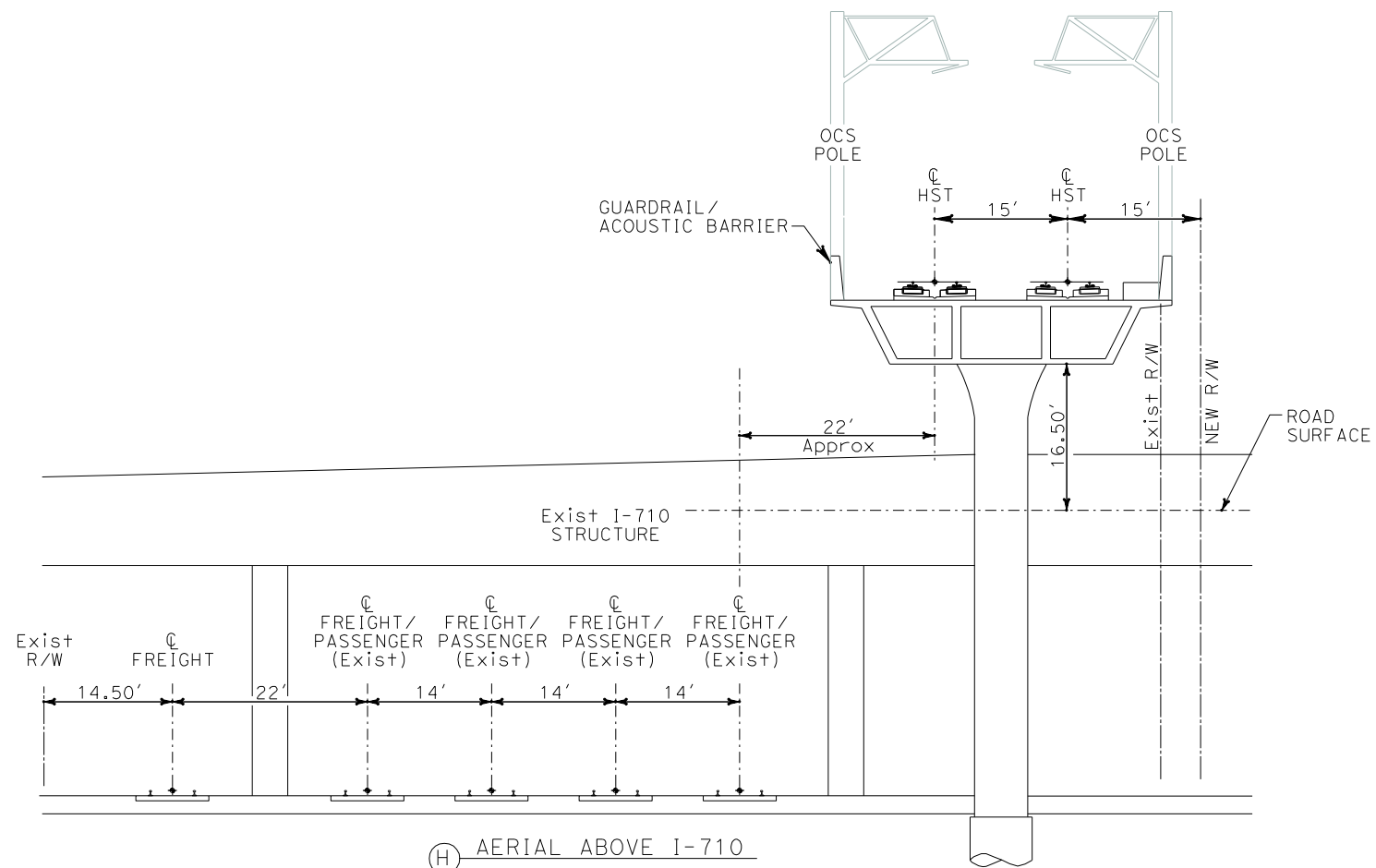
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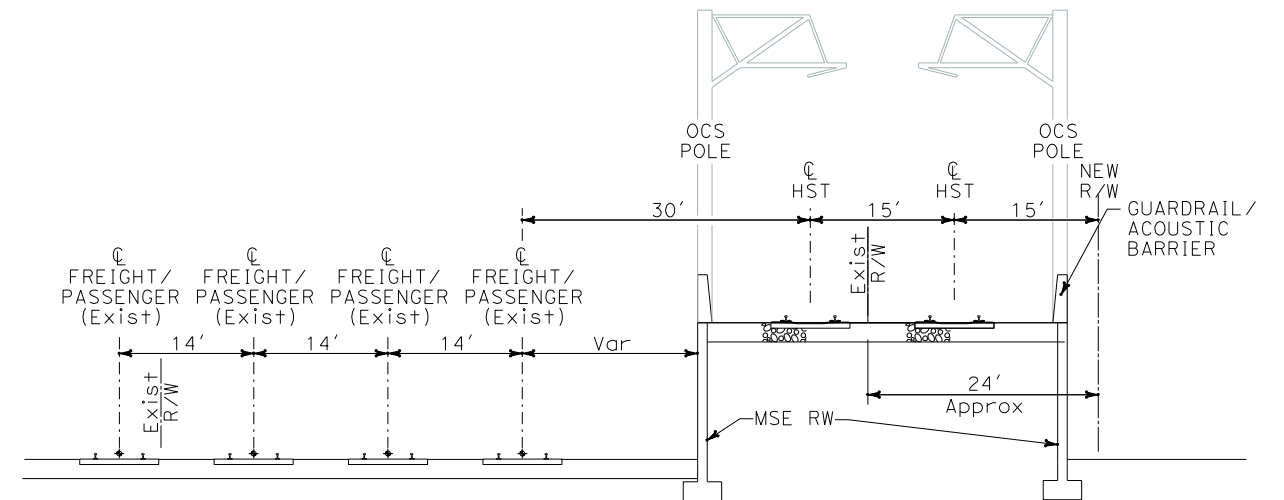
(G) AERIAL ALONG 26TH STREET



① AERIAL WITH INDUSTRY TRACK ON SOUTH SIDE OF HST STRUCTURE



(H) AERIAL ABOVE I-710



(J) TRANSITION FROM AERIAL TO AT-GRADE, ON RETAINED FILL

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY	K. SPERRY
DRAWN BY	K. SPERRY
CHECKED BY	A. SOKOL
IN CHARGE	D. THOMSON
DATE	02-13-09



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CALIFORNIA HIGH SPEED RAIL AUTHORITY

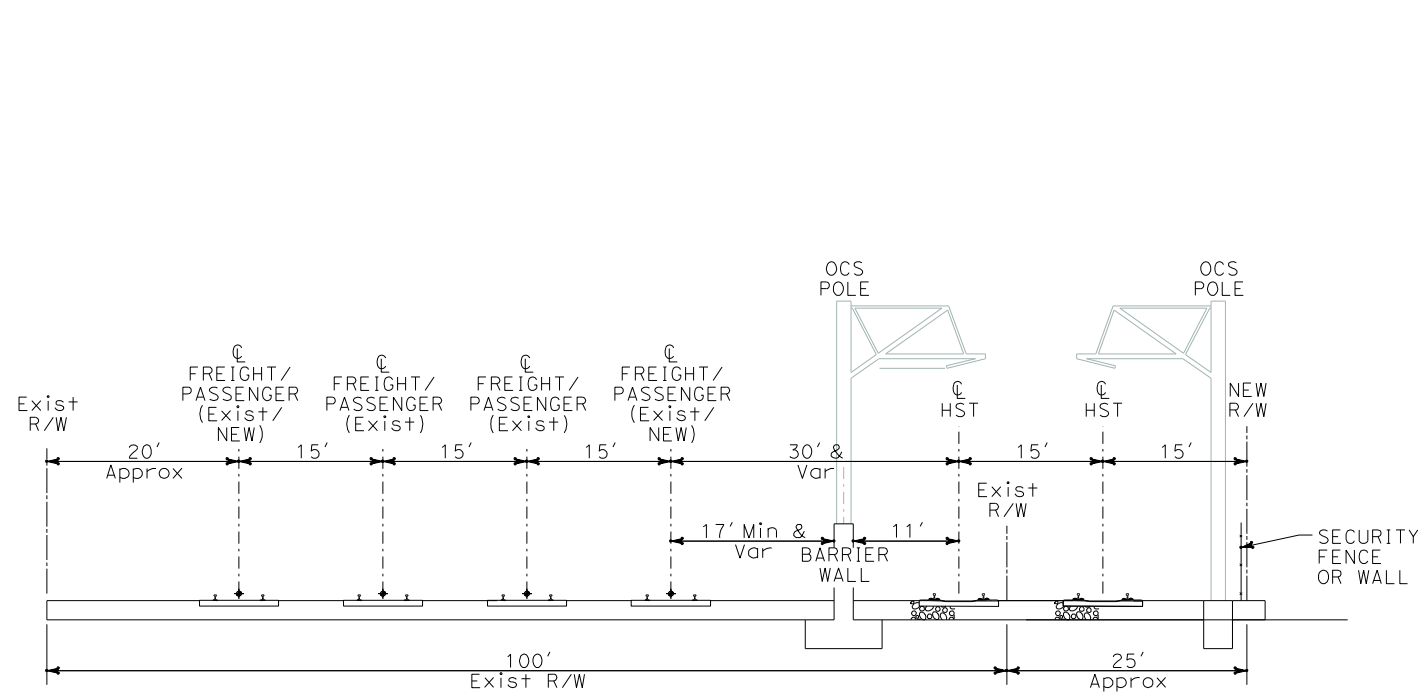
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES BASELINE ALIGNMENT

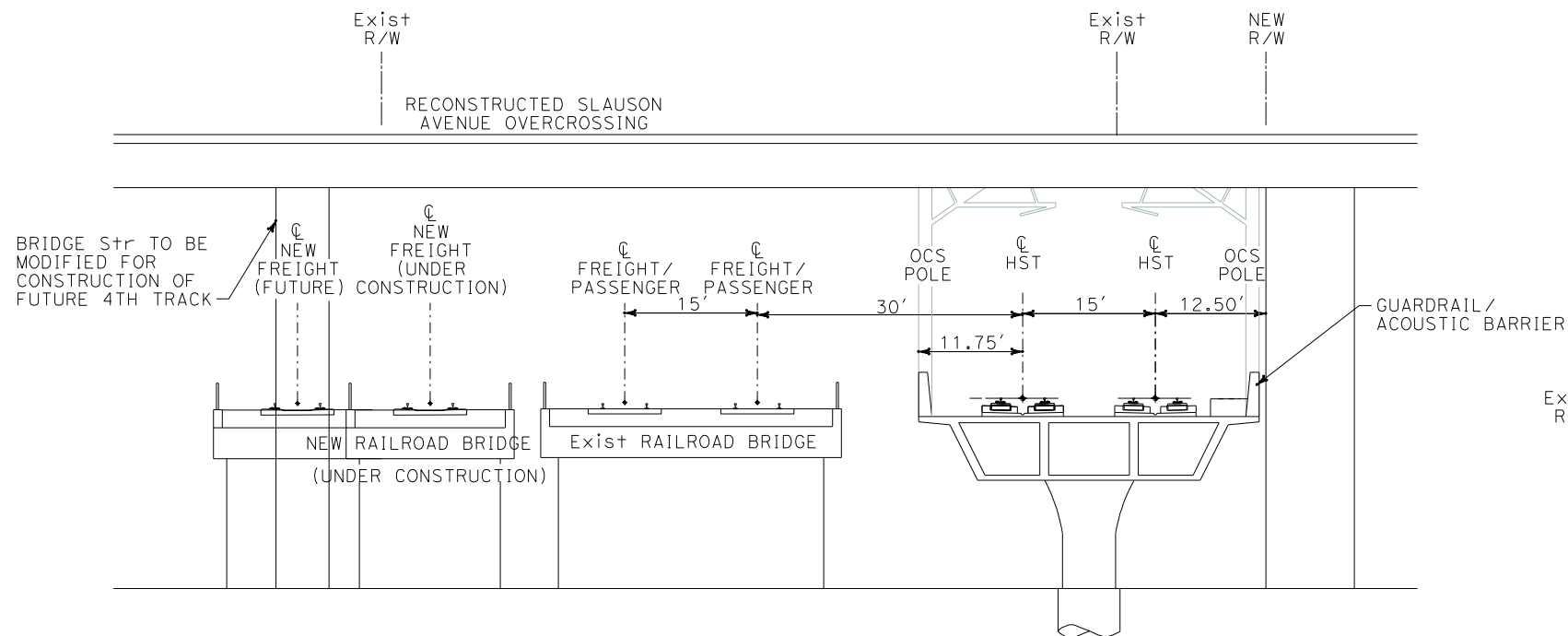
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0303
SCALE 1" = 10'
SHEET NO. 10 OF 245

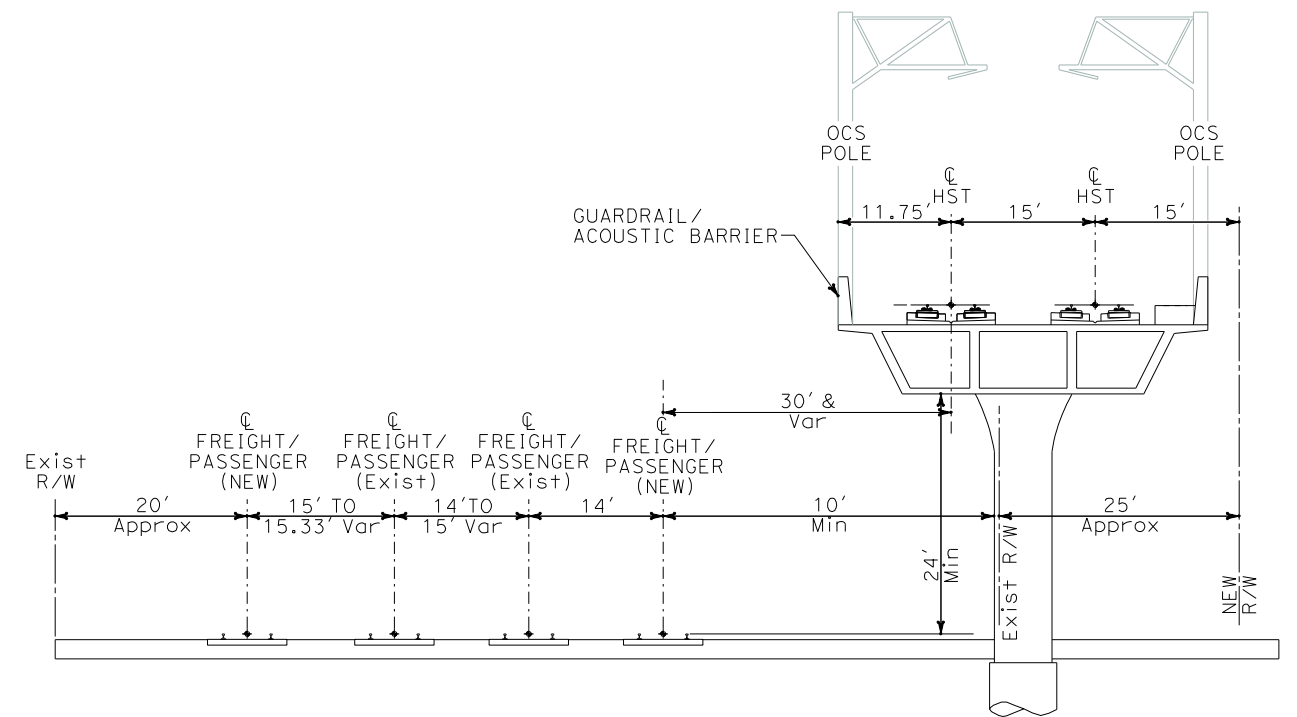
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Sperry\km



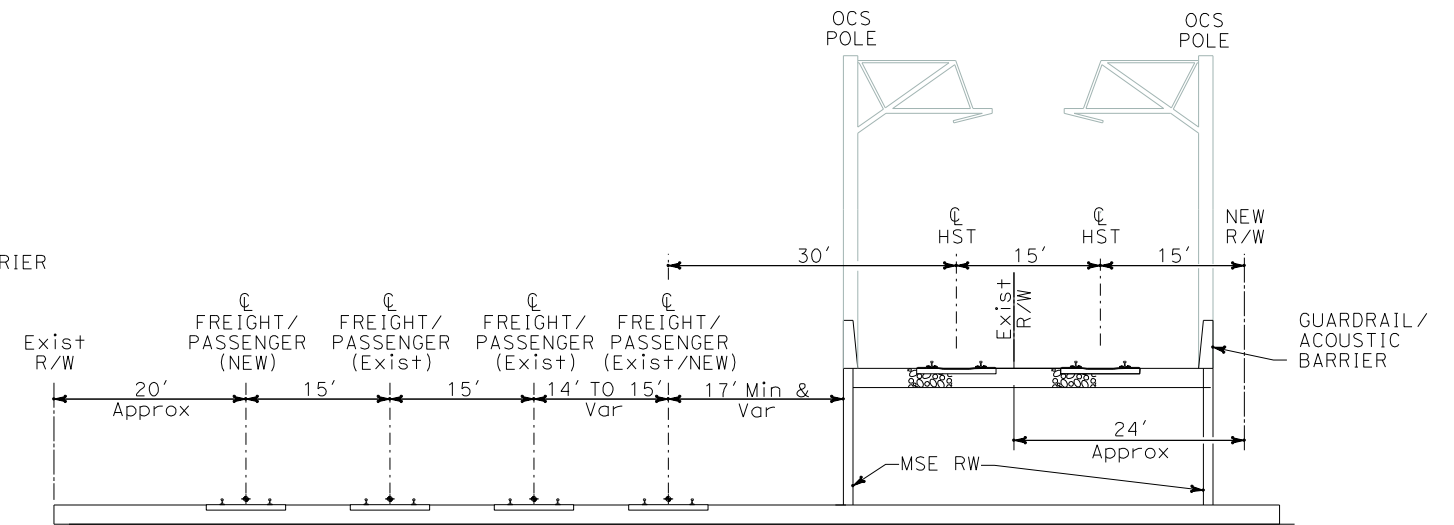
(K) AT-GRADE SOUTH SIDE OF SAN BERNADINO SUB



(L) AT GRADE AT DT JCT, UNDER SLAUSON BRIDGE



(M) AERIAL STRUCTURE TO FLY OVER LA HABRA SUBDIVISION




(N) TRANSITION FROM AERIAL TO AT-GRADE, ON RETAINED WALL

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
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IN CHARGE D. THOMSON
DATE 02-13-09

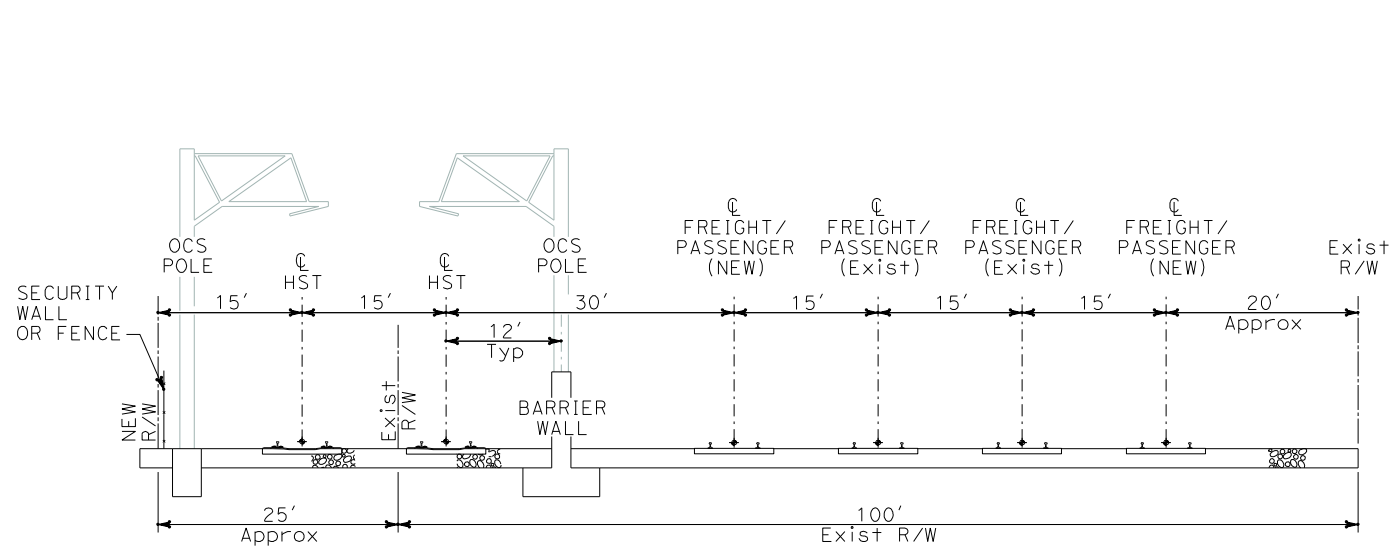
 **STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
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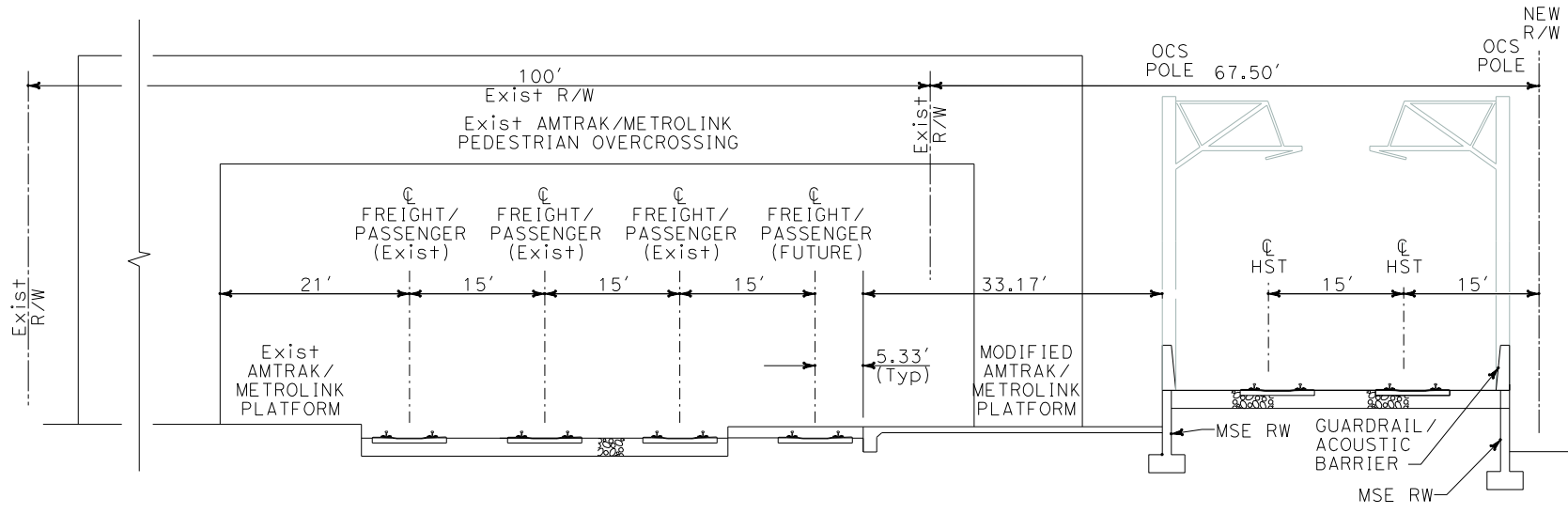
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0304
SCALE 1" = 10'
SHEET NO. 11 OF 245

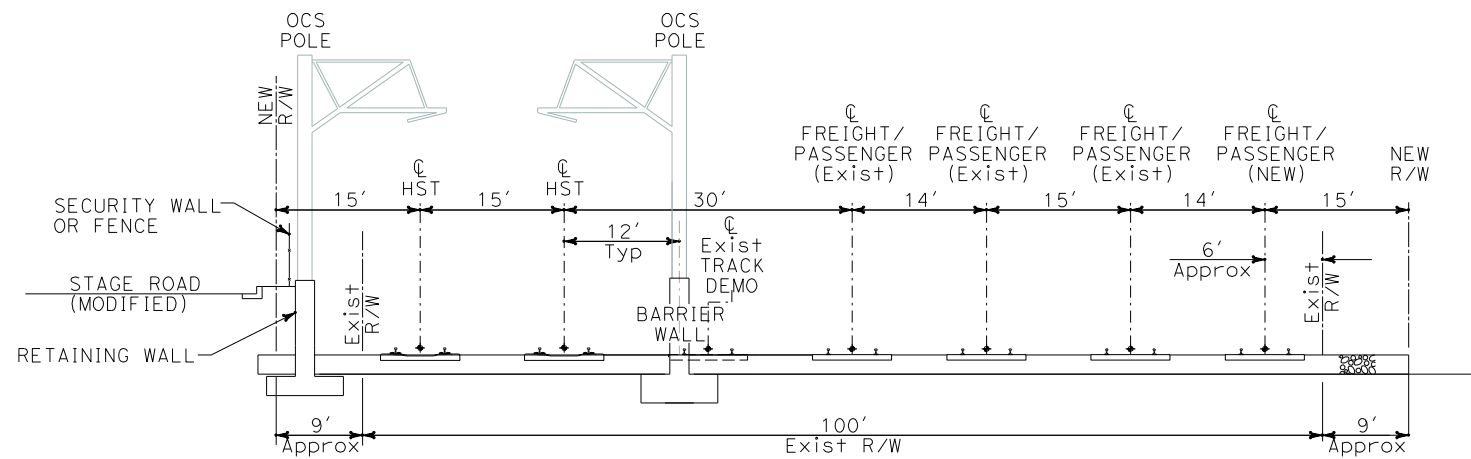
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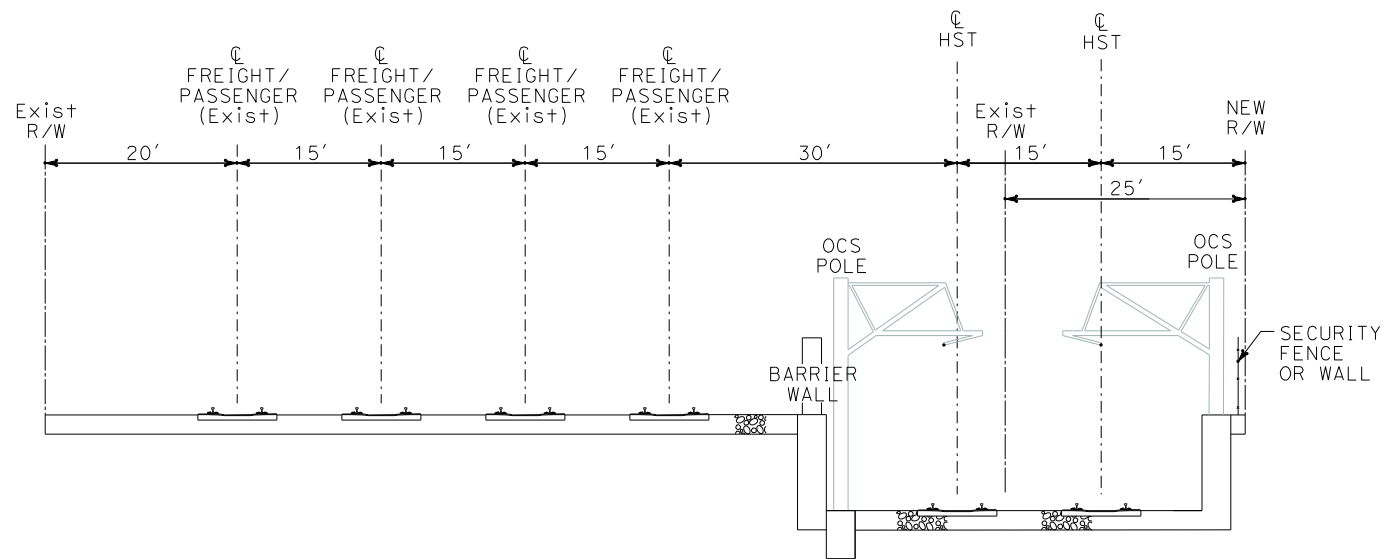
① AT-GRADE IN LA MIRADA AREA, NORTH SIDE OF SAN BERNADINO SUB



② DESCENDING TO GRADE ADJACENT TO EXISTING BUENA PARK STATION



③ AT-GRADE ALONG STAGE ROAD, IN RETAINED CUT



④ IN OPEN TRENCH UNDER FULLERTON AIRPORT FLIGHT PATH

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



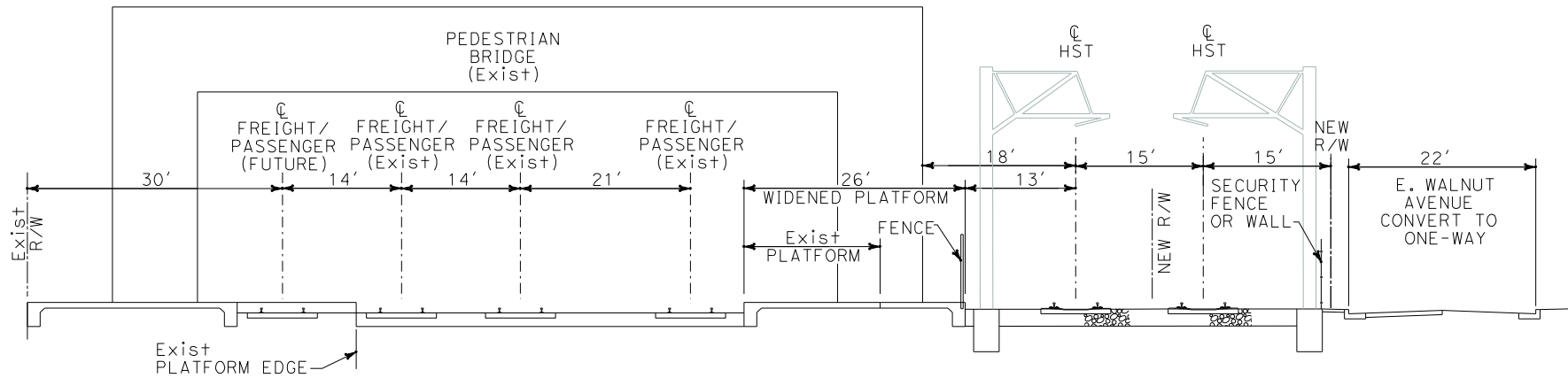
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

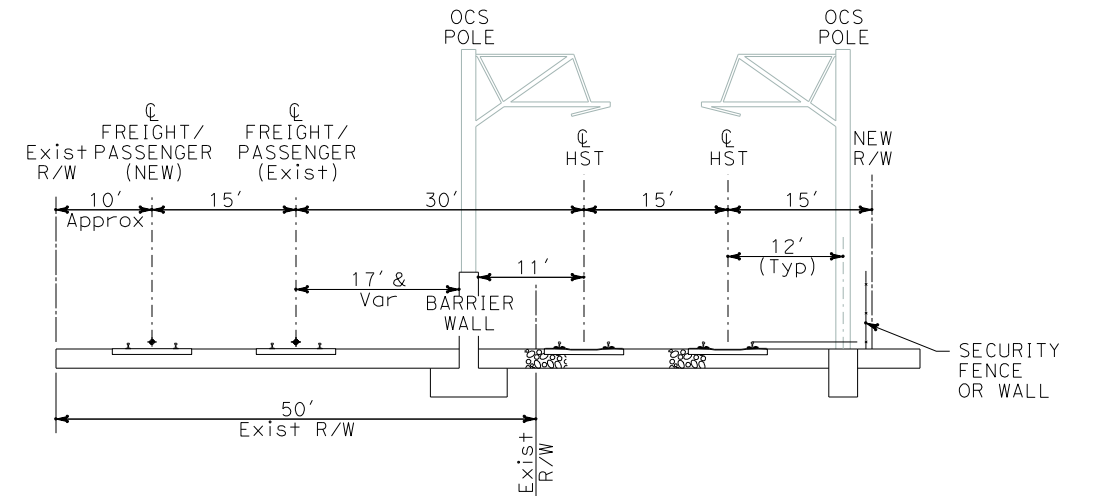
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0305
SCALE 1" = 10'
SHEET NO. 12 OF 245

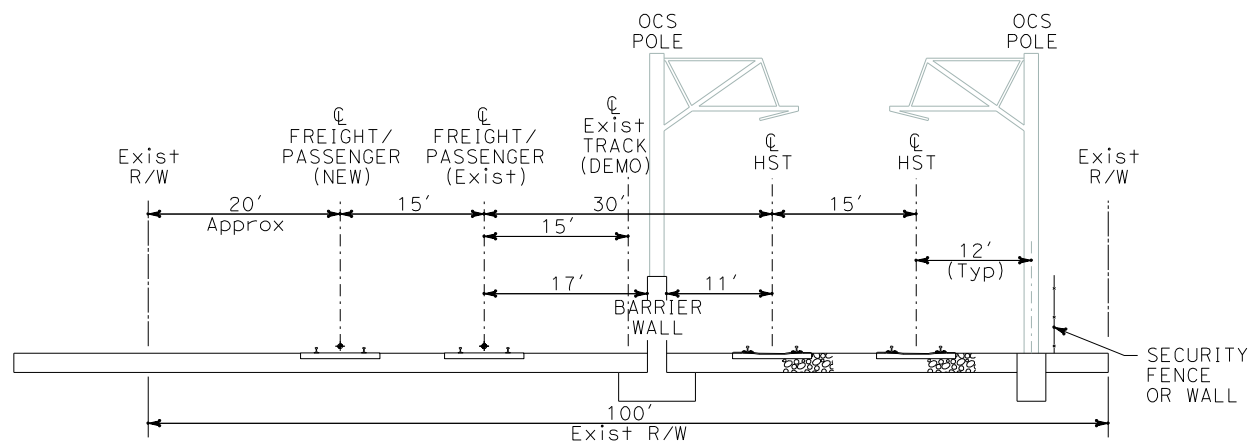
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Ⓢ AT-GRADE ADJACENT TO EXISTING FULLERTON STATION



Ⓤ AT-GRADE ON WEST SIDE OF ORANGE SUBDIVISION, 50' R/W




Ⓣ AT-GRADE ON WEST SIDE OF ORANGE SUBDIVISION, 100' R/W

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

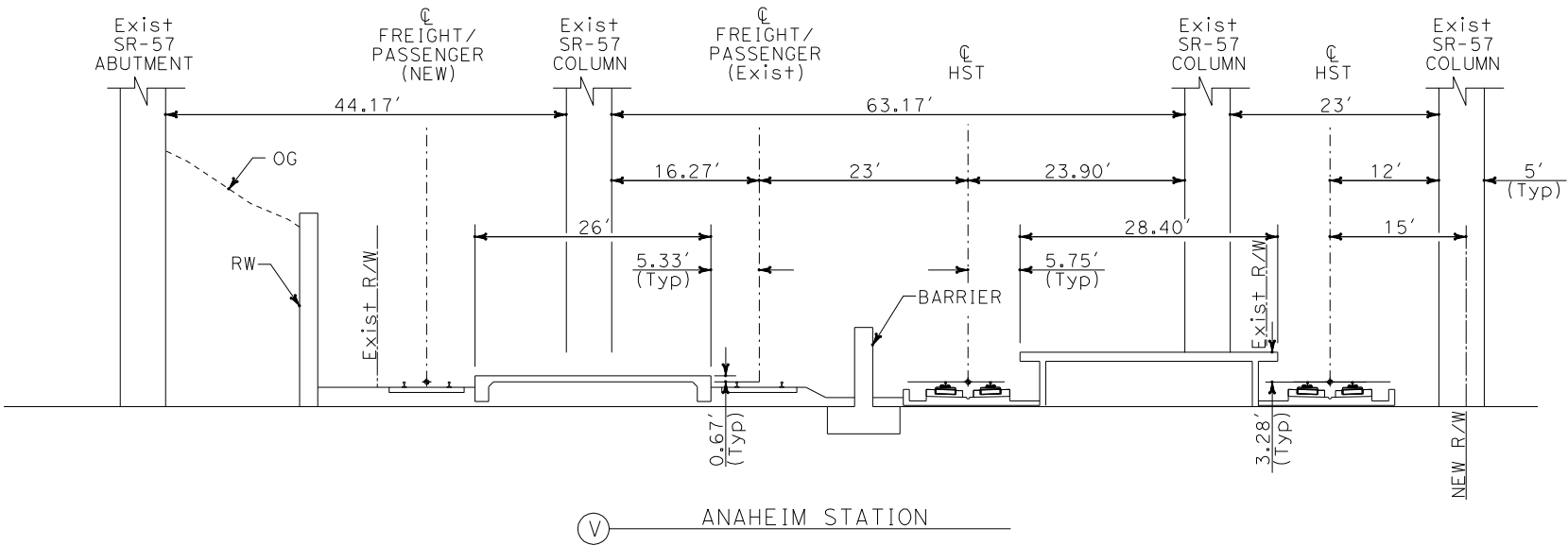
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
TYPICAL CROSS SECTIONS

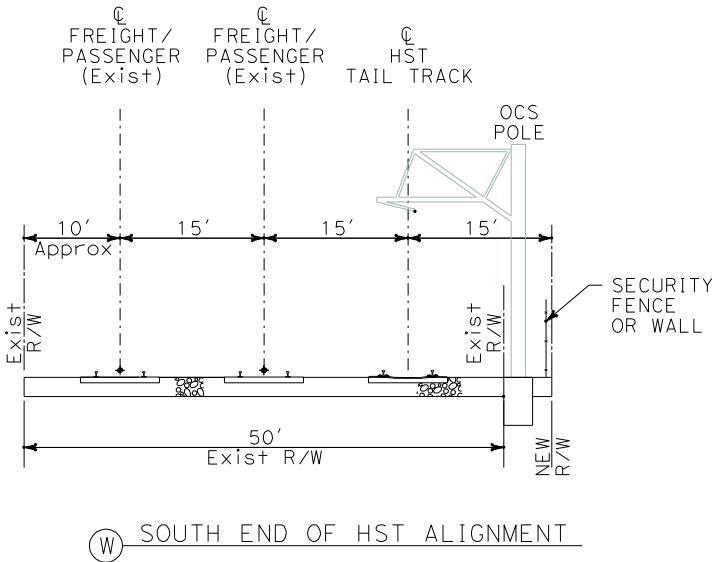
CONTRACT NO. HSR06-0005
DRAWING NO. T0306
SCALE 1" = 10'
SHEET NO. 13 OF 245

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\$\$\$\$\$\$SYTIME\$\$\$\$\$



NOTES:

1. ANAHEIM STATION AMENITIES, INCLUDING PEDESTRIAN ACCESS AND CANOPIES, ARE NOT SHOWN AND ARE CURRENTLY UNDER DEVELOPMENT.
2. TAIL TRACK SOUTH (EAST) OF ANAHEIM STATION IS A NON-REVENUE SERVICE TRACK. FINAL LENGTH AND CONFIGURATION OF TAIL TRACK IS CURRENTLY UNDER DEVELOPMENT AND IS SUBJECT TO THE LAYOUT AND LOCATION OF THE ANAHEIM LAYOVER YARD, WHICH IS ALSO CURRENTLY UNDER DEVELOPMENT.



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FOR INTERNAL USE ONLY



REV	DATE	BY	SUB	APP	DESCRIPTION

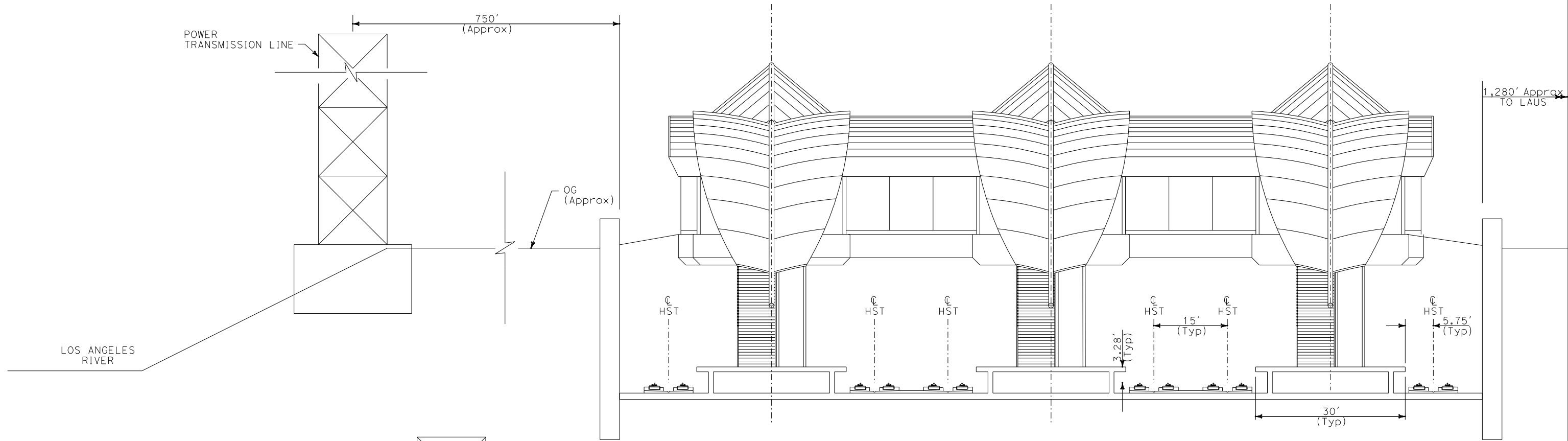
DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0307
SCALE 1" = 10'
SHEET NO. 14 OF 245

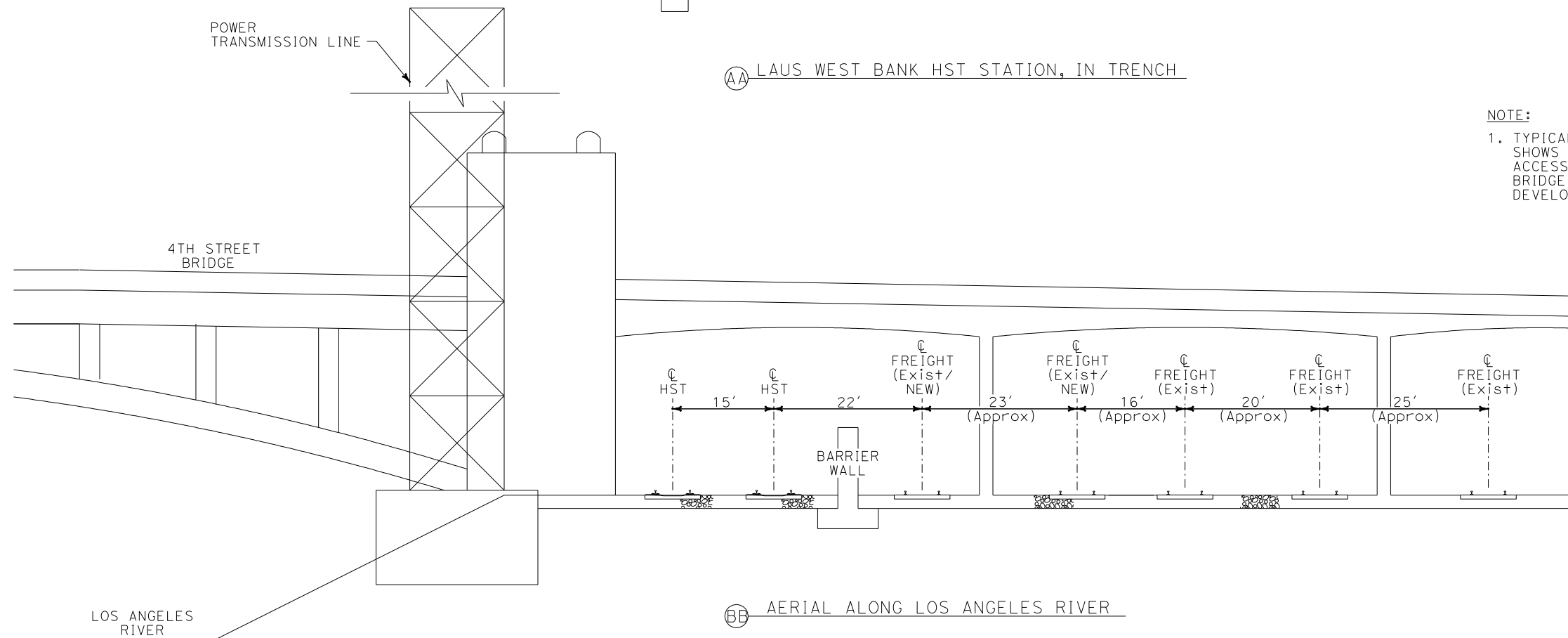
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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry



AA LAUS WEST BANK HST STATION, IN TRENCH

NOTE:

1. TYPICAL SECTION FOR LAUS WEST BANK STATION SHOWS A GENERIC CONCEPT FOR THE PEDESTRIAN ACCESS BRIDGE. ACTUAL PEDESTRIAN ACCESS BRIDGE AND CANOPIES ARE CURRENTLY UNDER DEVELOPMENT.



BB AERIAL ALONG LOS ANGELES RIVER

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

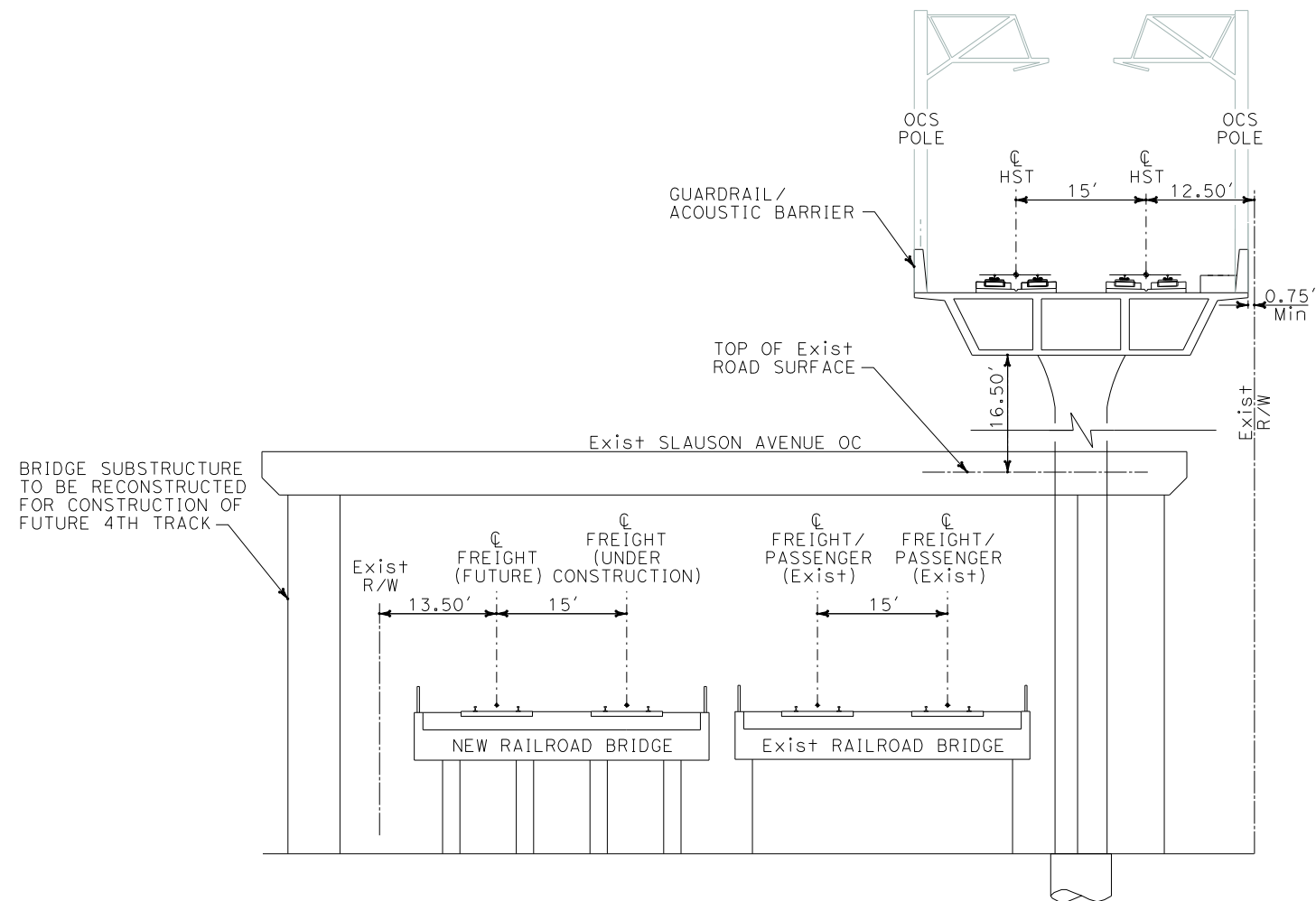


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
OPTIONAL ALIGNMENTS

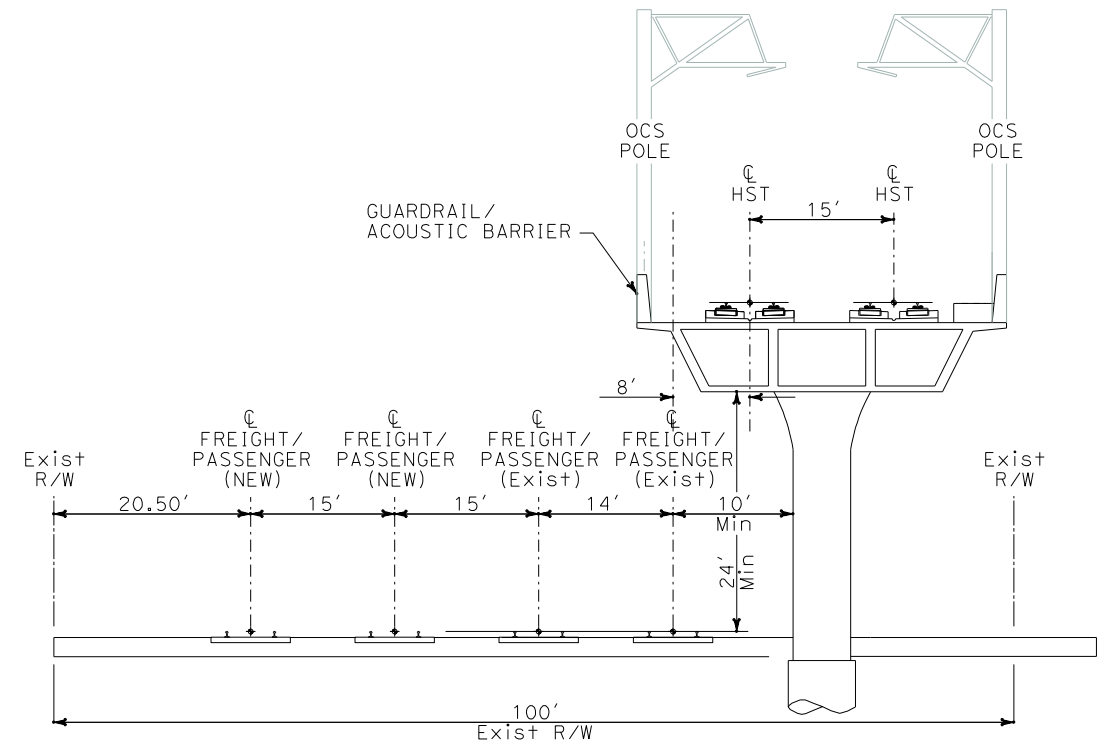
TYPICAL CROSS SECTIONS

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0308
SCALE
1" = 10'
SHEET NO.
15 OF 245

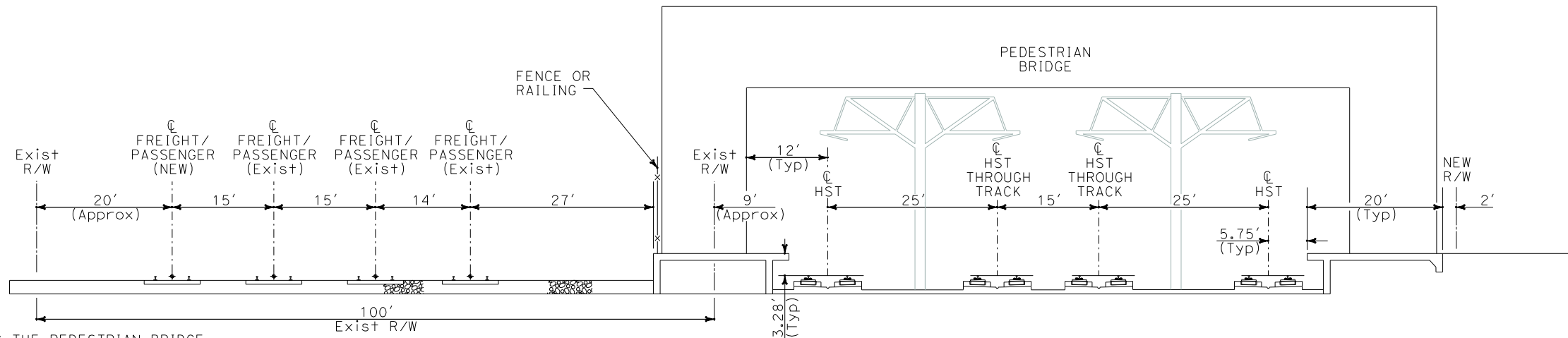
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CC AERIAL OVER SLAUSON AVENUE



DD AERIAL OVER LA HABRA SUBDIVISION TRACK



EE NORWALK/SANTA FE SPRINGS STATION

NOTES:

1. STATION AMENITIES, INCLUDING THE PEDESTRIAN BRIDGE ARE NOT DETAILED ON THE NORWALK/SANTA FE SPRINGS STATION SECTION, AND ARE CURRENTLY UNDER DEVELOPMENT.
2. NEW R/W LINE SHOWN NEAR EDGE OF STATION PLATFORM DOES NOT INCLUDE PROPERTY NEED FOR STATION ACCESS AND PARKING FACILITIES.

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DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

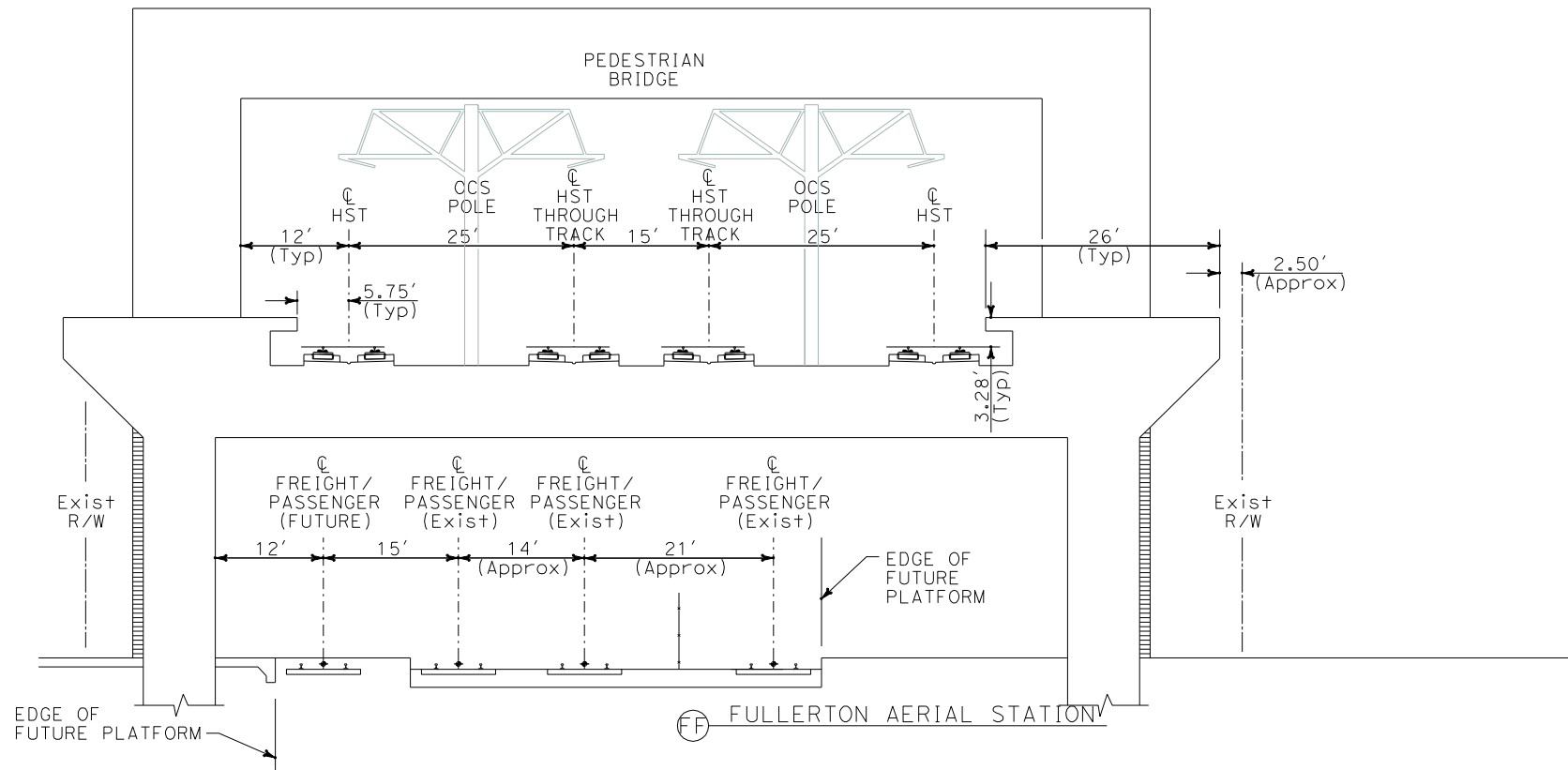
STV Incorporated
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
OPTIONAL ALIGNMENTS
TYPICAL CROSS SECTIONS

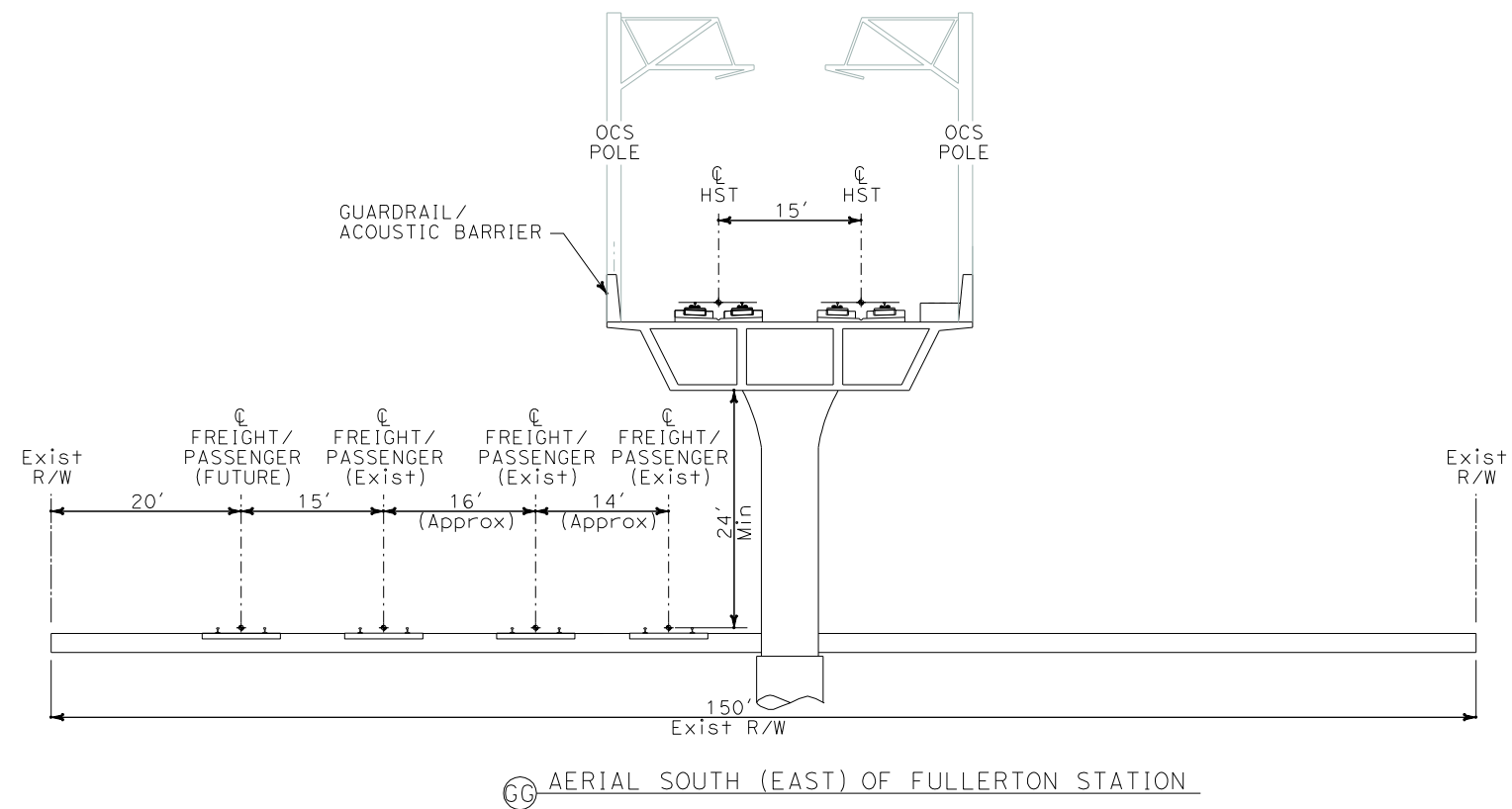
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DRAWING NO. T0309
SCALE 1" = 10'
SHEET NO. 16 OF 245

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NOTE:

1. DETAILS OF FULLERTON STATION AMENITIES, INCLUDING THE PEDESTRIAN BRIDGE ARE CURRENTLY UNDER DEVELOPMENT.




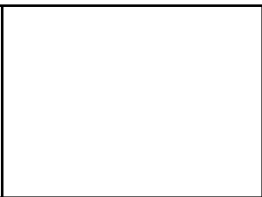
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DATE 02-13-09

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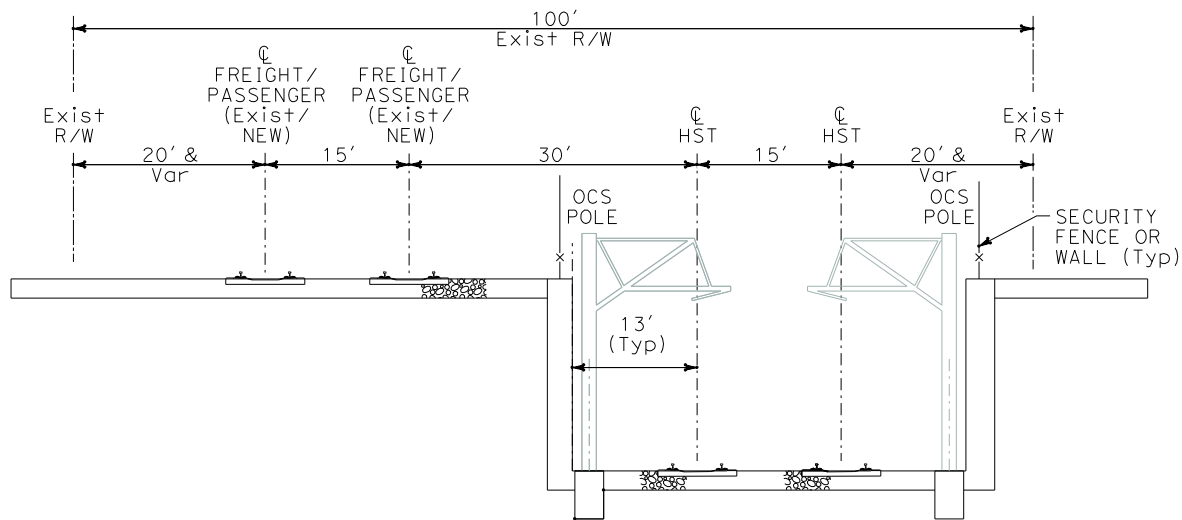


**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

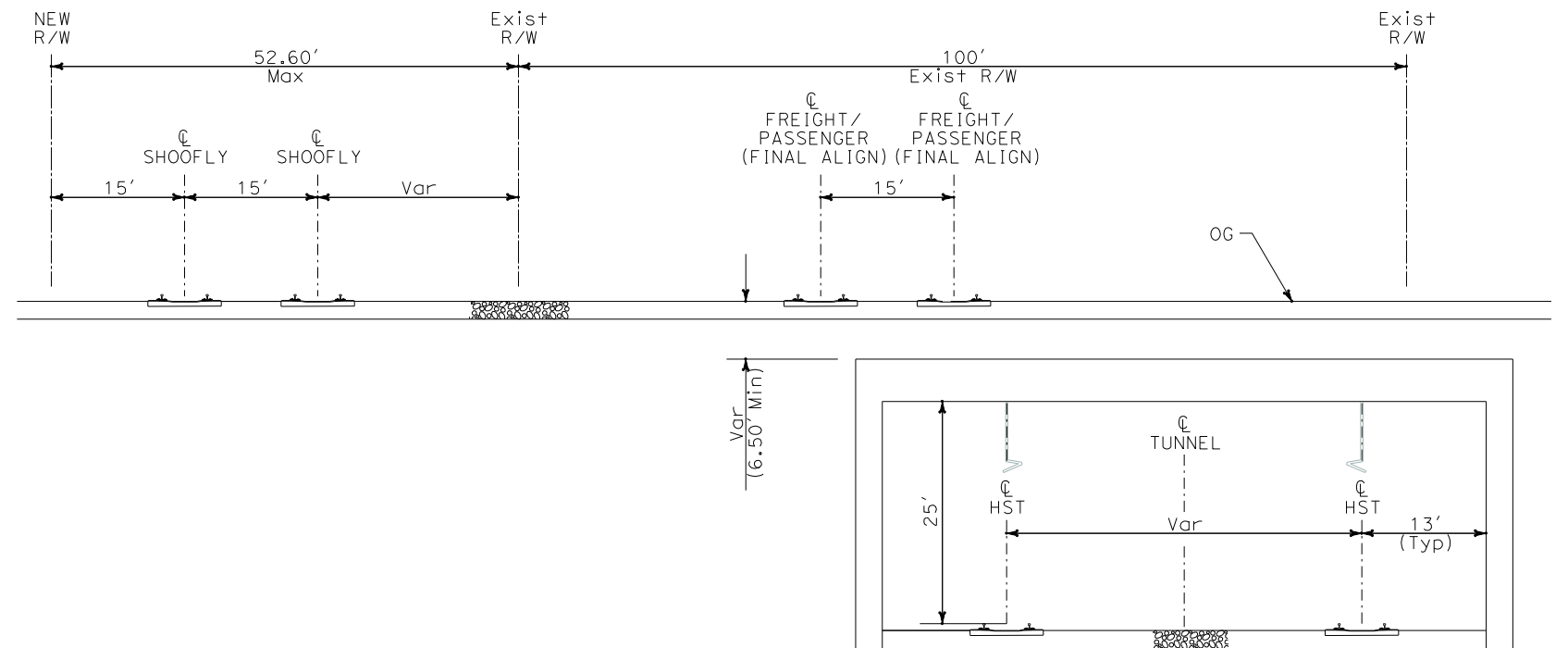
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ORANGE COUNTY TO LOS ANGELES
OPTIONAL ALIGNMENTS
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0310
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SHEET NO. 17 OF 245

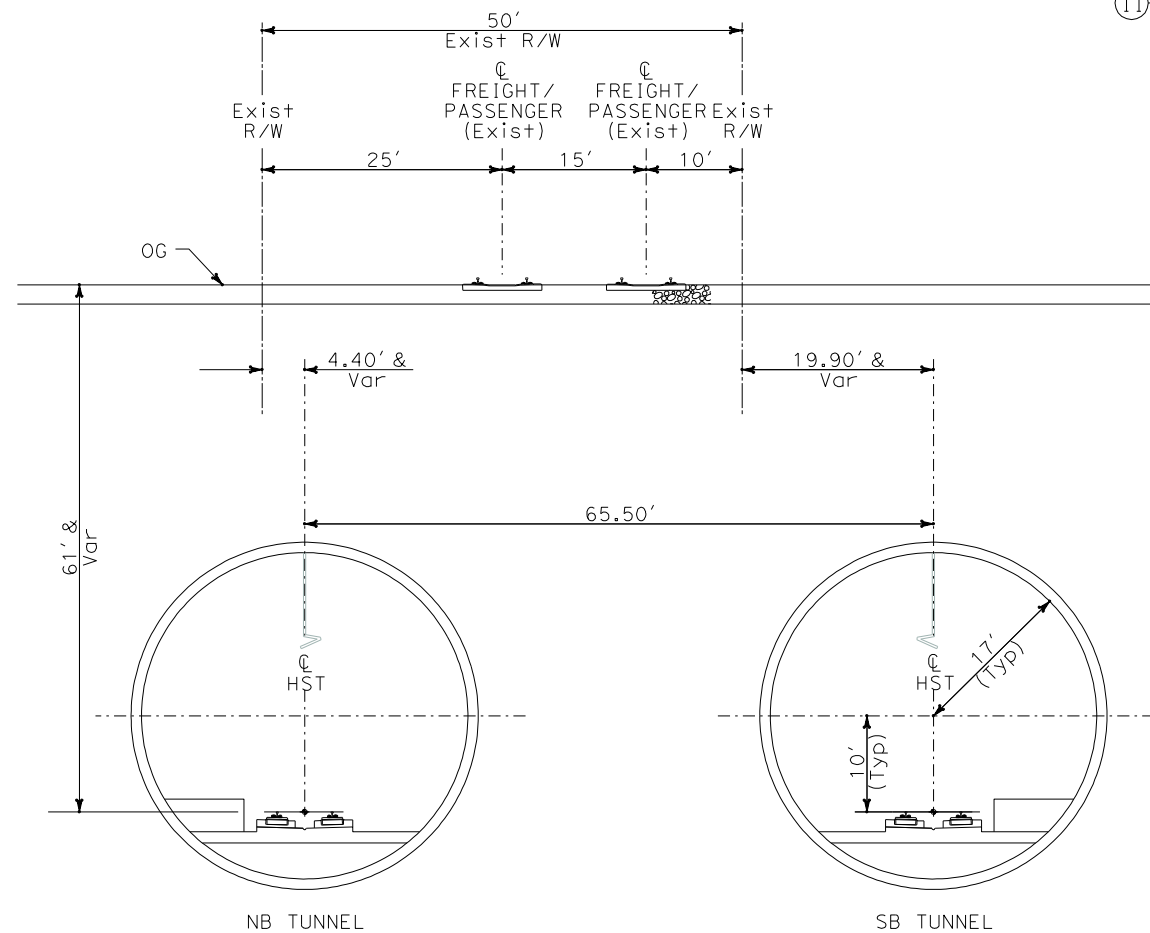
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HH ANAHEIM TUNNEL OPTION - TRENCH SECTION



II ANAHEIM TUNNEL OPTION - CUT AND COVER SECTION



JJ ANAHEIM TUNNEL OPTION - BORED TUNNEL SECTION

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CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
OPTIONAL ALIGNMENTS
TYPICAL CROSS SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. T0311
SCALE 1" = 10'
SHEET NO. 18 OF 245

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TRACK ALIGNMENT CURVE DATA SUMMARY TABLE - SOUTHBOUND TRACK (O-LA SB)

CURVE	DESIGN SPEED (V) (MPH)	RADIUS (R) (FT)	ACTUAL SUPERELEVATION (Ea) (IN)	UNBALANCED SUPERELEVATION (Eu) (IN)	SPIRAL LENGTH (Ls) (FT)	BEGIN STATION	END STATION
101	35	2,150	1.00	1.28	100	13+72.73	17+84.56
102	40	1,000	2.50	3.92	190	23+65.30	34+38.83
103	60	1,605	5.00	3.99	590	37+13.28	53+72.71
104	90	3,600	5.05	3.97	600	122+23.34	159+73.70
105	110	25,000	1.00	0.94	0	173+11.18	178+58.40
106	110	8,200	3.25	2.67	620	211+63.61	228+33.93
107	120	30,000	1.00	0.92	0	339+97.64	342+97.84
108	120	30,000	1.00	0.92	0	354+77.50	357+64.76
109	120	30,000	1.00	0.92	0	389+67.59	395+44.44
110	120	30,000	1.00	0.92	0	401+37.77	407+14.63
111	120	30,000	1.00	0.92	0	412+94.03	417+44.38
112	120	30,000	1.00	0.92	0	419+85.83	423+36.72
113	120	30,000	1.00	0.92	0	432+79.73	434+83.43
114	120	30,000	1.00	0.92	0	437+28.64	439+32.34
115	120	30,000	1.00	0.92	0	489+34.88	495+81.24
116	100	9,200	2.50	1.86	450	500+98.26	531+94.18
117	100	12,000	1.75	1.59	250	534+02.82	541+29.80
118	100	10,000	2.25	1.76	300	541+57.96	549+73.38
119	100	22,000	1.00	0.82	0	556+47.31	559+06.79
120	80	3,610	4.00	3.11	590	568+26.07	586+46.32
121	80	3,610	4.00	3.11	590	626+52.59	650+35.03
122	80	3,610	4.00	3.11	590	691+16.72	722+91.26
123	90	4,460	4.00	3.28	620	798+43.47	847+96.98
124	120	30,000	1.00	0.92	0	875+68.74	880+84.56
125	120	30,000	1.00	0.92	0	884+85.85	890+08.27
126	90	10,500	1.75	1.34	260	1050+06.04	1059+27.71
127	90	4,643.69	4.00	3.00	658.08 IN 749.01 OUT	1062+08.00	1090+20.21
128	110	25,000	1.00	0.94	0	1093+94.13	1098+93.35
129	110	25,000	1.00	0.94	0	1104+16.44	1111+72.71
130	110	25,000	1.00	0.94	0	1184+88.79	1188+90.59
131	110	25,000	1.00	0.94	0	1194+22.12	1198+23.93
132	100	6,000	3.75	2.93	700	1207+36.88	1228+20.52
133	110	25,000	1.00	0.94	0	1270+13.00	1275+40.76
134	110	25,000	1.00	0.94	0	1279+54.68	1284+75.40
135	60	1,805	4.50	3.50	440	1303+48.36	1336+22.99
136	100	5,700	4.00	3.04	520	1369+72.08	1389+37.57
137	100	22,000	1.00	0.82	0	1417+22.61	1421+48.12
138	100	22,000	1.00	0.82	0	1424+98.19	1429+23.70
139	100	22,000	1.00	0.82	0	1432+18.90	1435+93.43
140	100	22,000	1.00	0.82	0	1441+00.04	1444+74.57
141	100	5,750	4.00	2.97	700	1508+62.47	1570+23.82
142	50	8,000	0.50	0.75	0	1585+03.80	1587+02.89
143	40	5,000	0.50	0.78	0	1602+24.05	1605+56.29

TRACK ALIGNMENT CURVE DATA SUMMARY TABLE - NORTHBOUND TRACK (O-LA NB)

CURVE	DESIGN SPEED (V) (MPH)	RADIUS (R) (FT)	ACTUAL SUPERELEVATION (Ea) (IN)	UNBALANCED SUPERELEVATION (Eu) (IN)	SPIRAL LENGTH (Ls) (FT)	BEGIN STATION	END STATION
201	35	2,150	1.00	1.28	100	14+75.92	17+31.84
202	40	1,000	2.50	3.92	190	22+90.62	34+36.67
203	60	1,605	5.00	3.99	590	36+83.24	53+42.68
204	90	3,585	5.10	3.96	598.75	121+99.12	159+35.10
205A	80	5,400	2.50	2.25	300	174+63.56	183+44.19
205B	80	5,400	2.50	2.25	300	185+27.13	202+71.60
206	100	6,600	3.25	2.83	600	205+03.41	228+13.02
207	120	30,000	1.00	0.92	0	339+76.22	342+76.42
208	120	30,000	1.00	0.92	0	354+56.08	357+43.34
209	120	30,000	1.00	0.92	0	389+46.38	395+23.24
210	120	30,000	1.00	0.92	0	401+16.57	406+93.43
211	120	30,000	1.00	0.92	0	412+72.80	417+23.14
212	120	30,000	1.00	0.92	0	419+64.59	423+15.48
213	120	30,000	1.00	0.92	0	432+58.30	434+62.00
214	120	30,000	1.00	0.92	0	437+07.20	439+10.91
215	120	30,000	1.00	0.92	0	489+13.68	495+60.04
216	100	9,200	2.50	1.86	450	500+79.40	531+75.31
217	100	12,000	1.75	1.59	250	533+86.43	541+13.40
218	100	10,000	2.25	1.75	300	541+41.47	549+56.90
219	100	22,000	1.00	0.82	0	556+30.53	558+90.01
220	80	3,595	4.00	3.14	588.77	568+09.99	586+23.90
221	80	3,625	4.00	3.08	591.22	626+30.17	650+21.28
222	80	3,625	4.00	3.08	591.22	691+01.75	722+88.25
223	90	4,445	4.00	3.31	618.96	798+40.37	847+78.26
224	120	30,000	1.00	0.92	0	875+50.68	880+66.49
225	120	30,000	1.00	0.92	0	884+67.78	889+90.20
226	90	10,515	1.75	1.34	260.19	1049+87.75	1059+10.55
227	90	4,628.69	4.00	3.02	657.01 IN 747.80 OUT	1061+91.28	1089+95.54
228	110	25,000	1.00	0.94	0	1093+69.32	1098+69.13
229	110	25,000	1.00	0.94	0	1103+92.30	1111+48.58
230	110	25,000	1.00	0.94	0	1184+65.00	1188+66.81
231	110	25,000	1.00	0.94	0	1193+98.34	1198+00.14
232	100	6,000	3.75	2.93	700	1207+11.23	1227+94.87
233	110	25,000	1.00	0.94	0	1269+85.77	1275+13.54
234	110	25,000	1.00	0.94	0	1279+27.46	1284+48.18
235	60	1,820	4.50	3.43	441.82	1303+20.07	1336+20.08
236	100	5,700	4.00	3.04	520	1369+66.34	1389+31.84
237	100	22,000	1.00	0.82	0	1417+15.10	1421+40.61
238	100	22,000	1.00	0.82	0	1424+90.69	1429+16.19
239	100	22,000	1.00	0.82	0	1432+11.12	1435+85.65
240	100	22,000	1.00	0.82	0	1440+92.27	1444+66.80
241	100	5,750	4.00	2.97	700	1508+47.11	1570+08.46
242A	50	8,000	0.50	0.75	0	1577+18.29	1580+18.25
242B	50	8,000	0.50	0.75	0	1583+87.92	1584+88.78
243A	40	5,000	0.50	0.78	0	1602+46.57	1603+41.49
243B	40	5,000	0.50	0.78	0	1605+85.43	1606+50.00

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY S. MAGALLON
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



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FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

TRACK GEOMETRY DATA

CONTRACT NO. HSR06-0005
DRAWING NO. T0400
SCALE NO SCALE
SHEET NO. 19 OF 245

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TRACK ALIGNMENT CURVE DATA SUMMARY TABLE - SOUTHBOUND TRACK (O-LA SB)

CURVE	DESIGN SPEED (V) (MPH)	RADIUS (R) (FT)	ACTUAL SUPERELEVATION (Ed) (IN)	UNBALANCED SUPERELEVATION (Eu) (IN)	SPIRAL LENGTH (Ls) (FT)	BEGIN STATION	END STATION
301	35	5,000	0.45	0.53	140	18+65.95	23+68.54
302	50	2,000	2.25	2.76	320	36+04.56	45+04.03
303	100	22,000	1.00	0.82	0	553+59.73	561+45.09
304	90	4,450	4.00	3.30	650	564+08.66	586+81.55
305	80	3,610	4.00	3.11	590	626+09.64	649+92.09
306	120	30,000	1.00	0.92	0	748+69.80	754+98.12
307	100	22,000	1.00	0.82	0	772+33.62	776+94.38
308	80	3,270	4.00	3.85	703.31 IN 510.43 OUT	801+32.74	826+58.12
309	100	6,270	3.50	2.90	510.43 IN 699.69 OUT	826+58.12	851+66.33
310	100	6,000	3.75	2.93	700	1207+36.88	1228+20.52
311	120	30,000	1.00	0.92	0	1250+56.01	1257+36.99
312	100	22,000	1.00	0.82	0	1268+96.49	1273+95.65
313	65	1,900	5.00	3.92	700	1301+17.09	1338+01.46
314	100	5,700	4.00	3.04	520	1369+72.08	1389+37.57
315	100	22,000	1.00	0.82	0	1417+22.61	1421+48.12
316	100	22,000	1.00	0.82	0	1424+98.19	1429+23.70
317	100	22,000	1.00	0.82	0	1432+18.90	1435+93.43
318	100	22,000	1.00	0.82	0	1441+00.04	1444+74.57
319	100	5,750	4.00	2.97	700	1508+62.47	1570+23.82

TRACK ALIGNMENT CURVE DATA SUMMARY TABLE - NORTHBOUND TRACK (O-LA NB)

CURVE	DESIGN SPEED (V) (MPH)	RADIUS (R) (FT)	ACTUAL SUPERELEVATION (Ed) (IN)	UNBALANCED SUPERELEVATION (Eu) (IN)	SPIRAL LENGTH (Ls) (FT)	BEGIN STATION	END STATION
401A	35	10,000	0.25	0.24	110	19+32.15	22+26.61
401B	45	5,000	0.80	0.82	120	27+43.90	33+18.73
402	50	2,000	2.25	2.76	320	35+98.87	44+98.34
403	100	22,000	1.00	0.82	0	553+43.13	561+28.48
404	90	4,450	4.00	3.30	650	563+89.56	586+62.45
405	80	3,625	4.00	3.08	591.22	625+87.16	649+78.28
406	120	30,000	1.00	0.92	0	748+66.34	754+94.65
407	100	22,000	1.00	0.82	0	772+30.15	776+90.92
408	80	3,255	4.00	3.85	701.69 IN 508.65 OUT	801+29.93	826+44.80
409	100	6,255	3.50	2.90	508.65 IN 698.85 OUT	826+44.80	851+47.72
410	100	6,000	3.75	2.93	700	1207+35.14	1228+18.78
411	120	30,000	1.00	0.92	0	1250+52.36	1257+33.34
412	100	22,000	1.00	0.82	0	1268+92.84	1273+92.00
413	65	1,915	5.00	3.85	702.76	1301+12.23	1338+22.92
414A	100	5,700	4.00	3.04	520	1368+72.83	1390+03.46
414B	110	25,000	1.00	0.94	0	1392+32.17	1399+56.45
415	100	22,065.50	1.00	0.82	0	1417+12.24	1421+39.02
416	100	21,934.50	1.00	0.82	0	1424+89.09	1429+13.33
417	100	21,934.50	1.00	0.82	0	1432+08.53	1435+81.95
418A	100	22,065.50	1.00	0.82	0	1440+88.56	1444+64.20
418B	100	25,000	0.75	0.85	0	1477+93.83	1487+75.57
418C	100	25,000	0.75	0.85	0	1490+80.00	1500+61.75
419	100	5,750	4.00	2.97	700	1508+45.13	1570+06.48

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IN CHARGE D. THOMSON
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

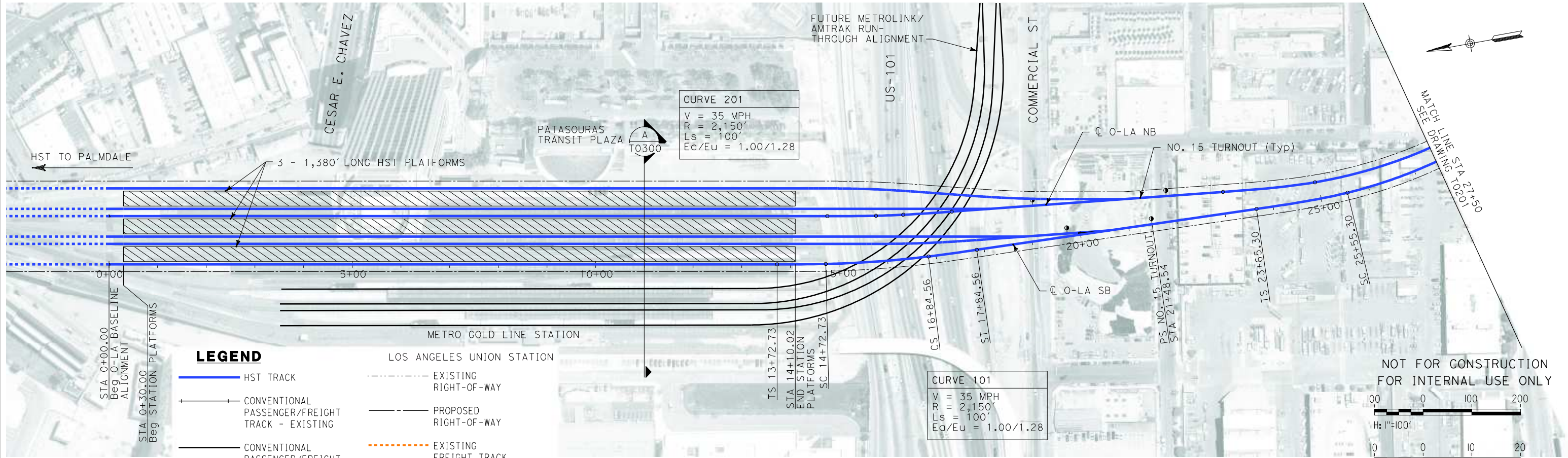


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FLY CALIFORNIA
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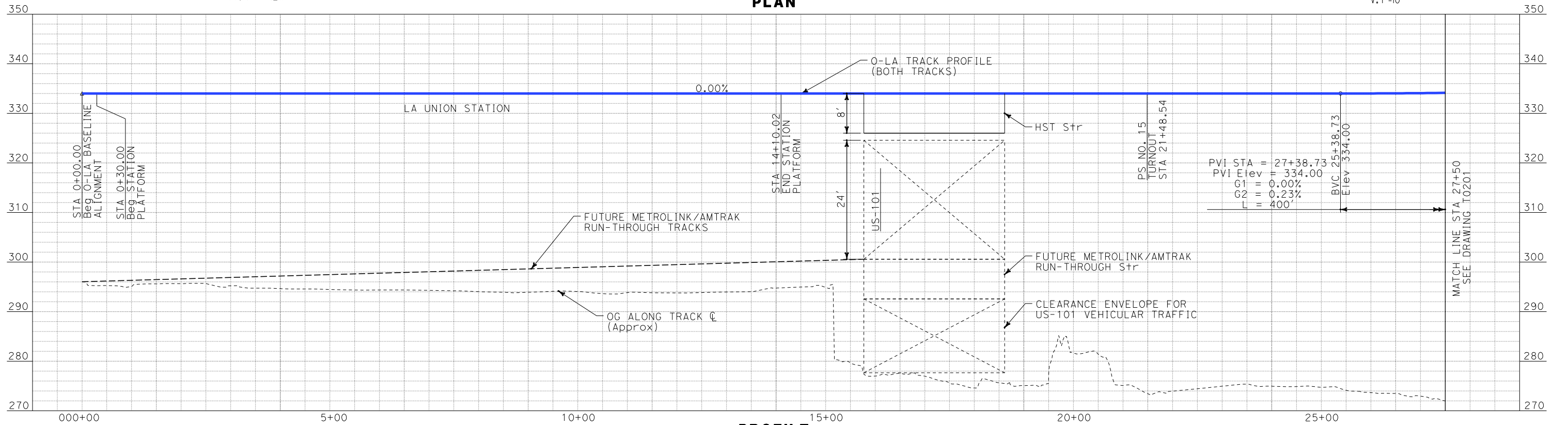
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
OPTIONAL ALIGNMENTS
TRACK GEOMETRY DATA

CONTRACT NO. HSR06-0005
DRAWING NO. T0401
SCALE NO SCALE
SHEET NO. 20 OF 245

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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

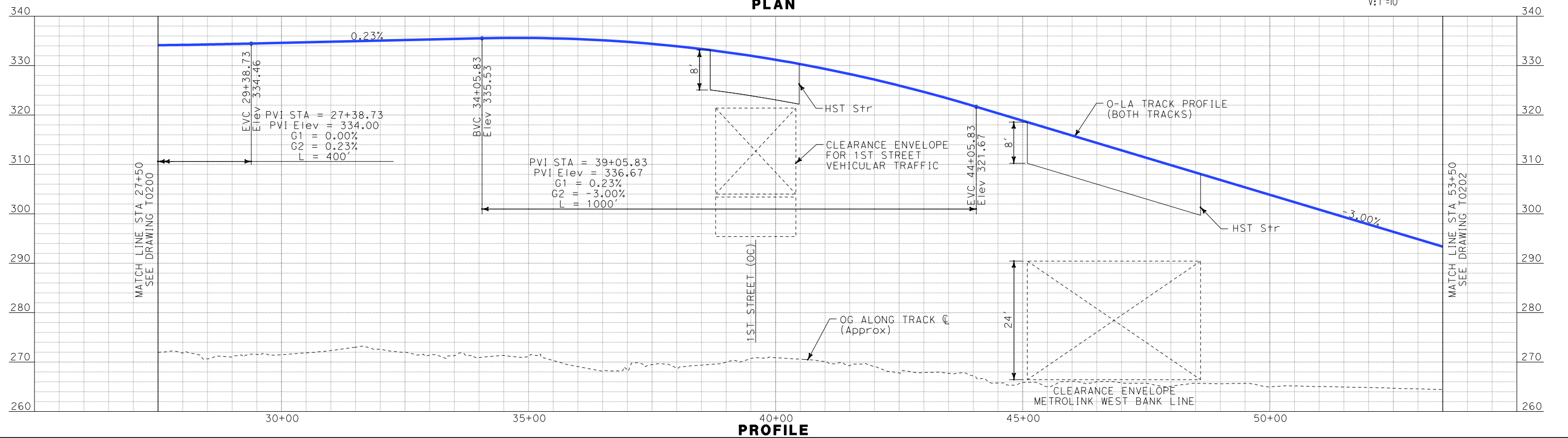
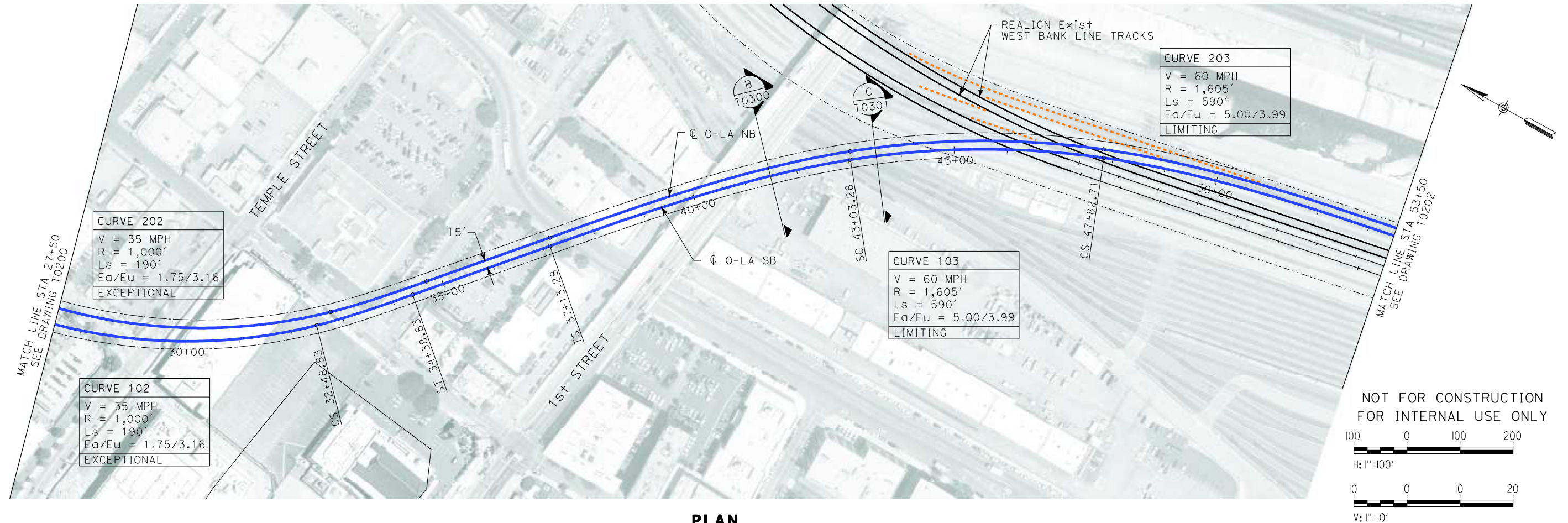
DESIGNED BY K. SPERRY
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IN CHARGE D. THOMSON
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LOS ANGELES, CA 90017-2556



**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0200
SCALE AS SHOWN
SHEET NO. 21 OF 245



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Sperry

REV	DATE	BY	SUB	APP	DESCRIPTION

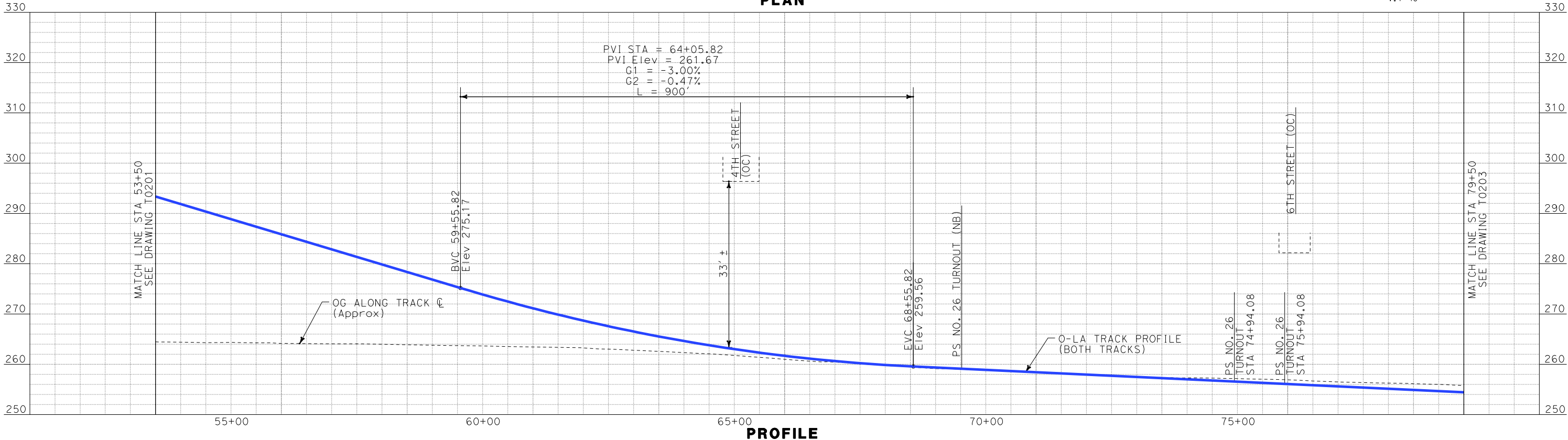
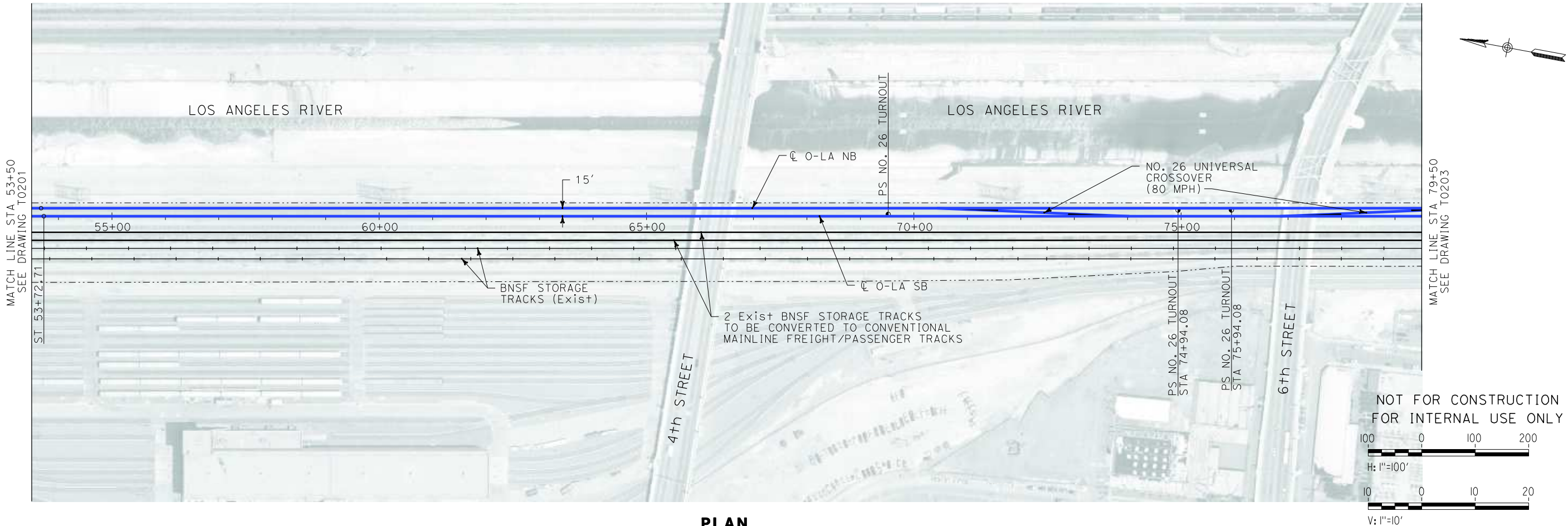
DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

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LOS ANGELES, CA 90017-2556



**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT
PLAN AND PROFILE


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DRAWING NO. T0201
SCALE AS SHOWN
SHEET NO. 22 OF 245



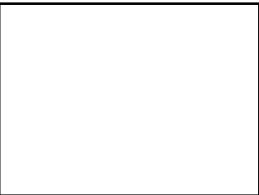
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Sperry\k

REV	DATE	BY	SUB	APP	DESCRIPTION

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DATE 02-13-09



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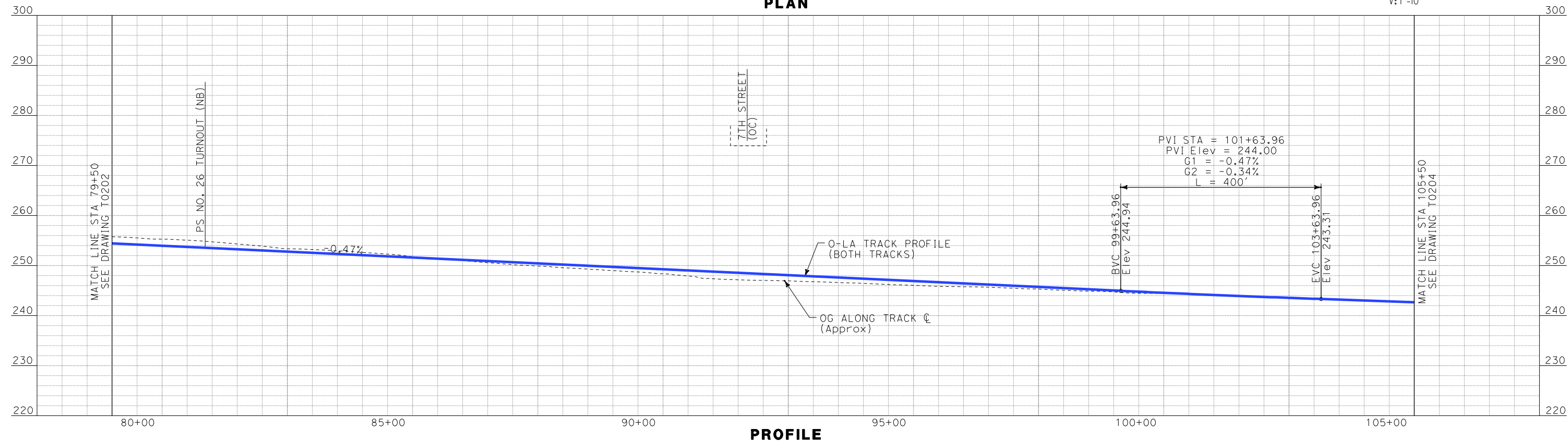
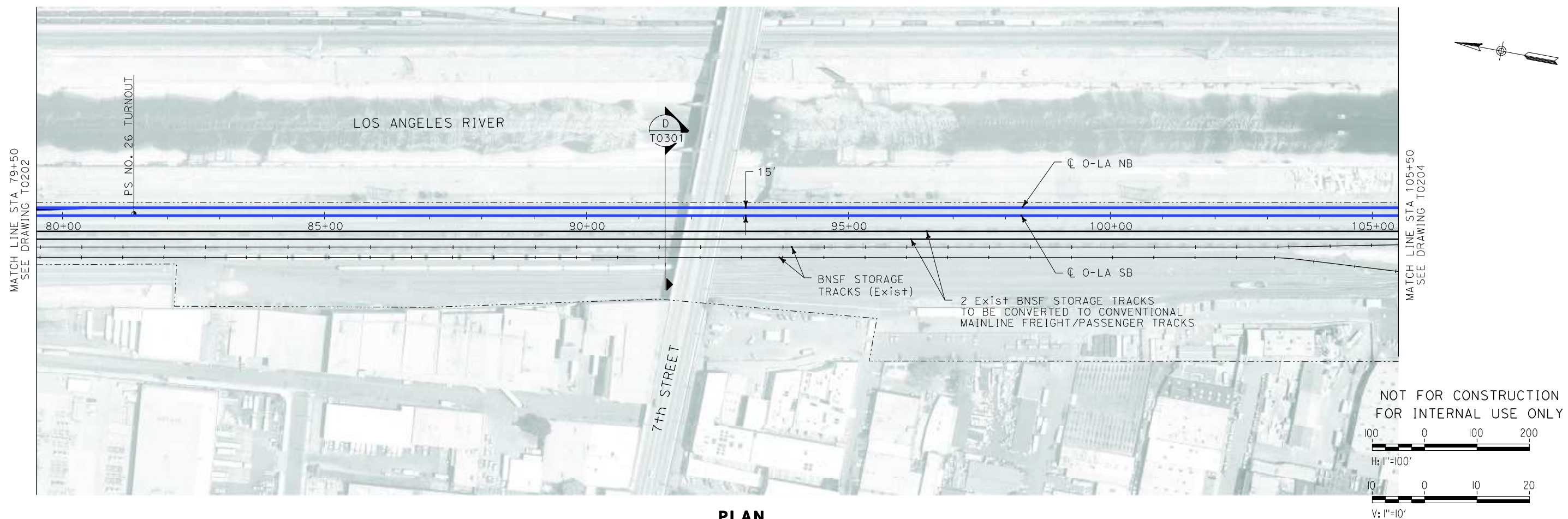


CALIFORNIA HIGH SPEED RAIL AUTHORITY
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0202
SCALE
AS SHOWN
SHEET NO.
23 OF 245

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LOS ANGELES, CA 90017-2556

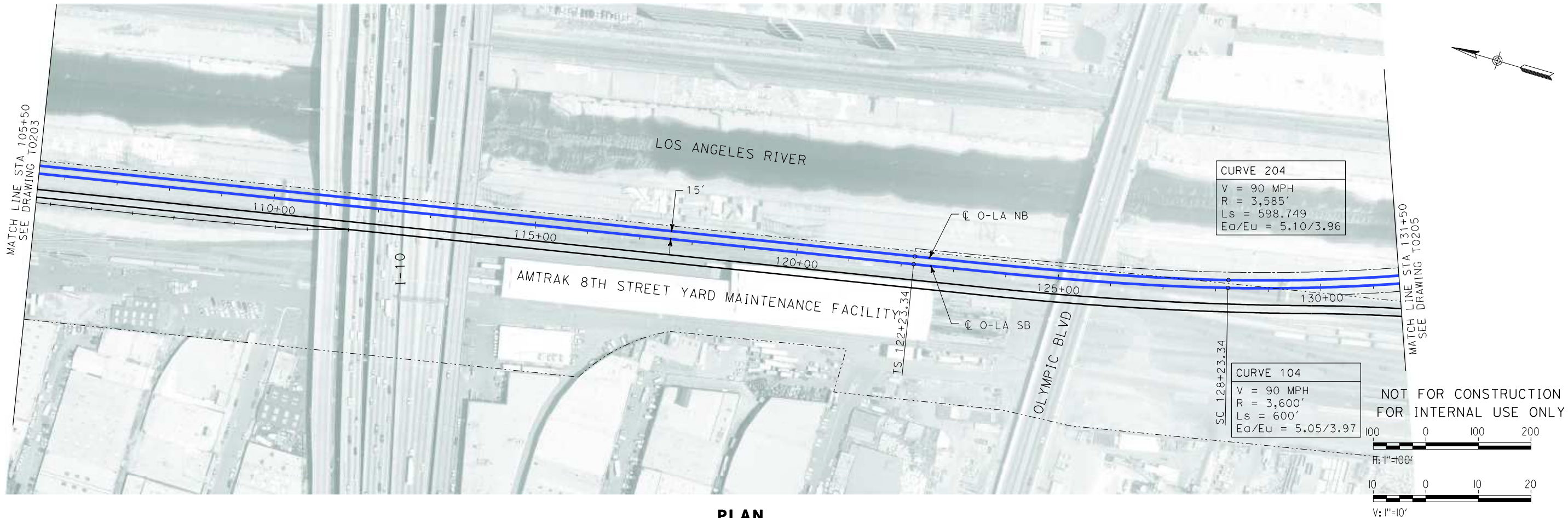
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

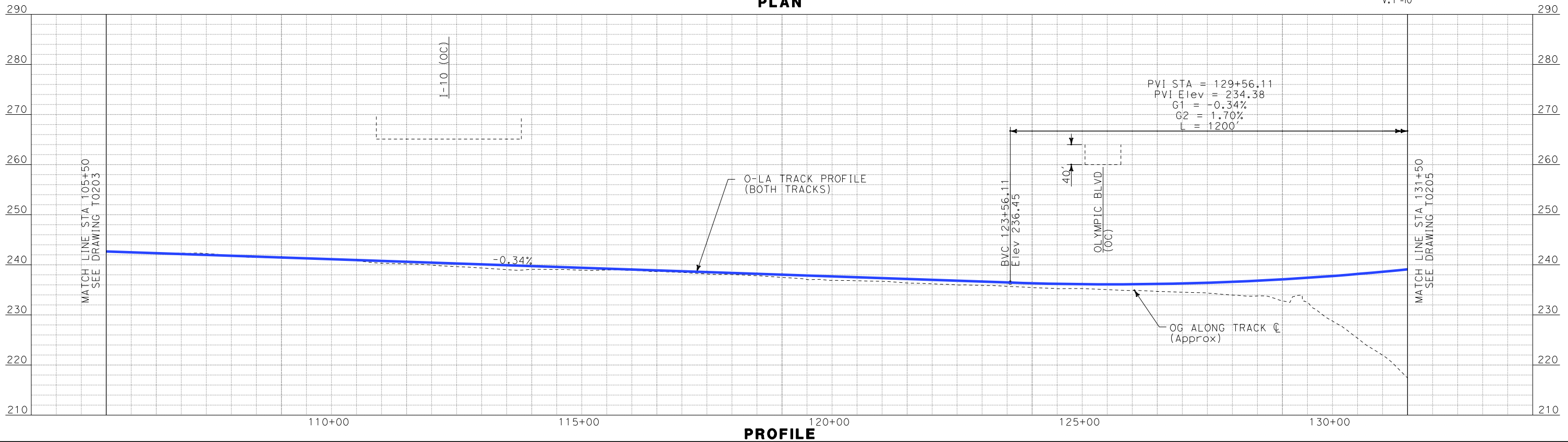
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CONTRACT NO. HSR06-0005
DRAWING NO. T0203
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SHEET NO. 24 OF 245

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


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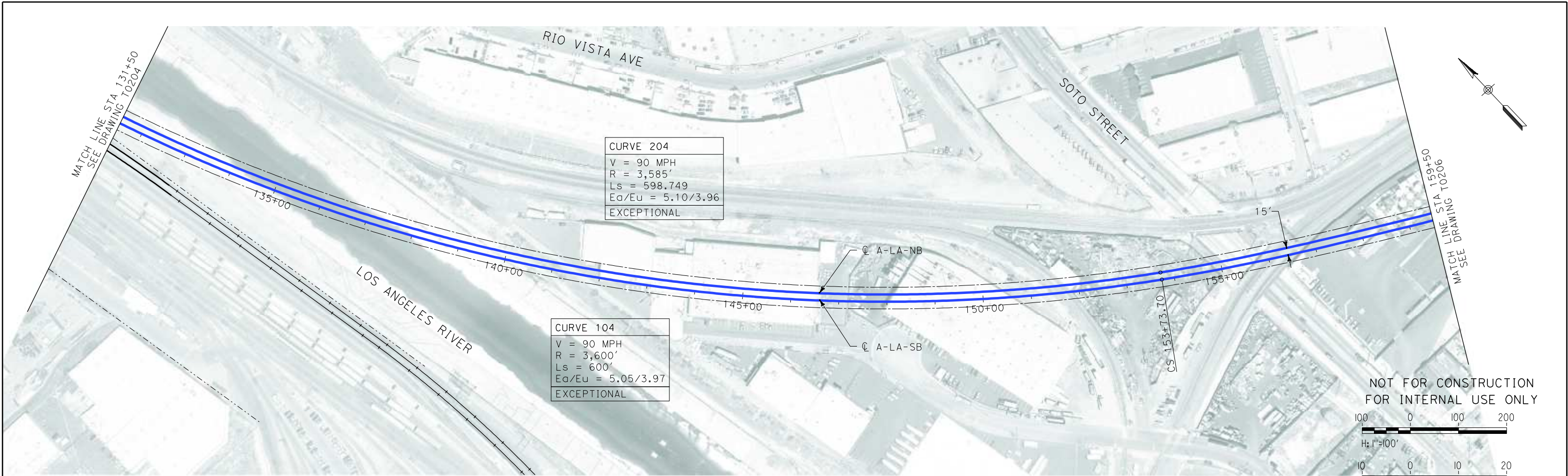
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

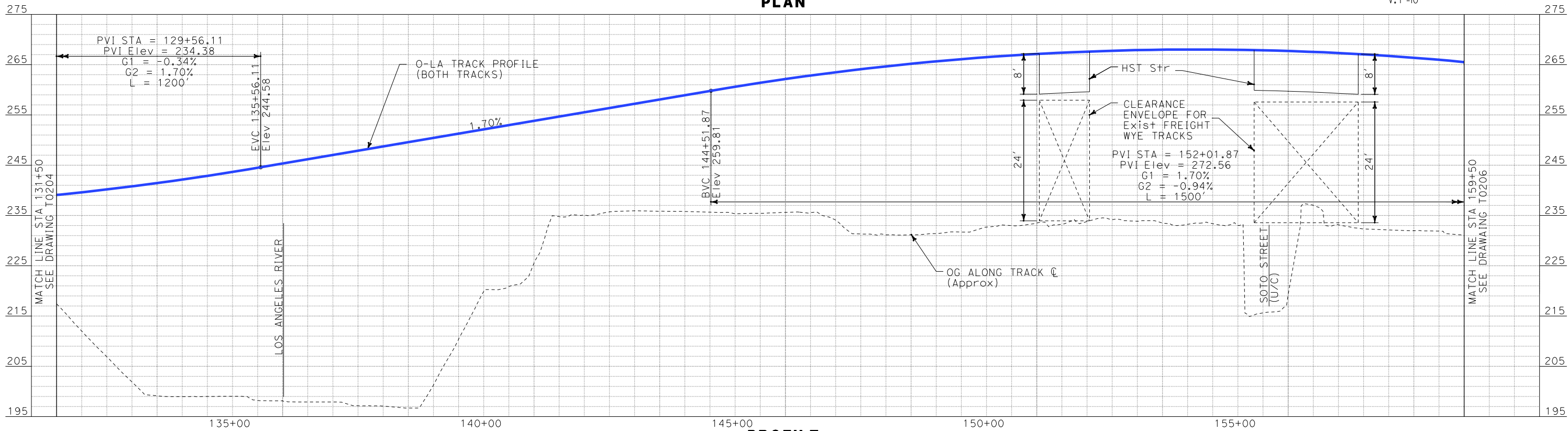
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CONTRACT NO. HSR06-0005
DRAWING NO. T0204
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SHEET NO. 25 OF 245

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LOS ANGELES, CA 90017-2556

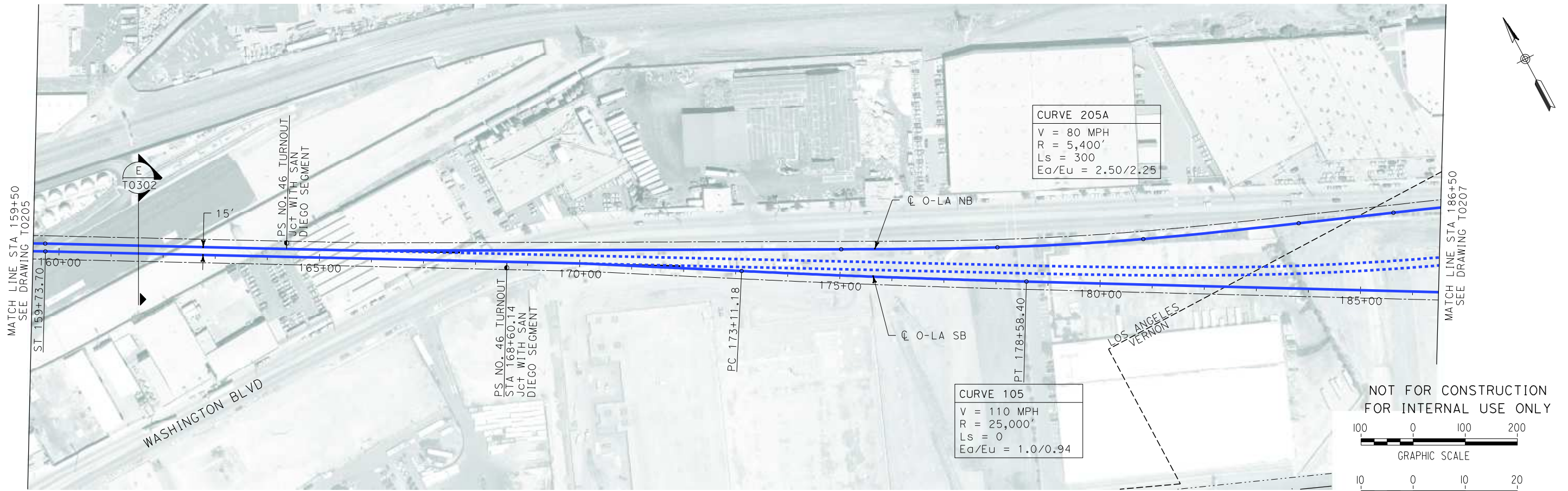


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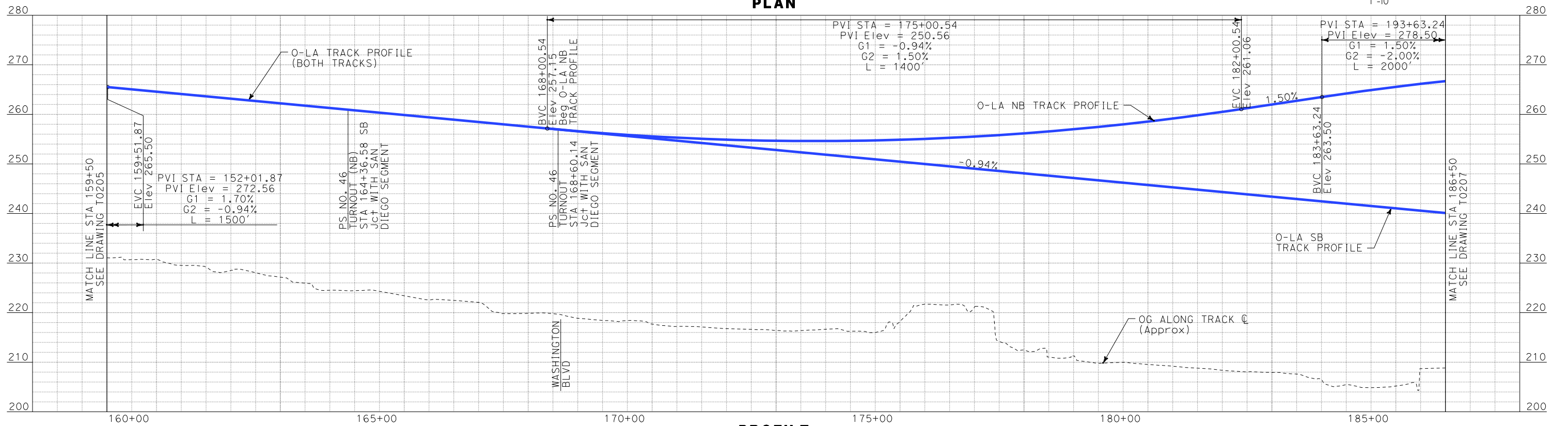
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0205
SCALE AS SHOWN
SHEET NO. 26 OF 245

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
PLAN



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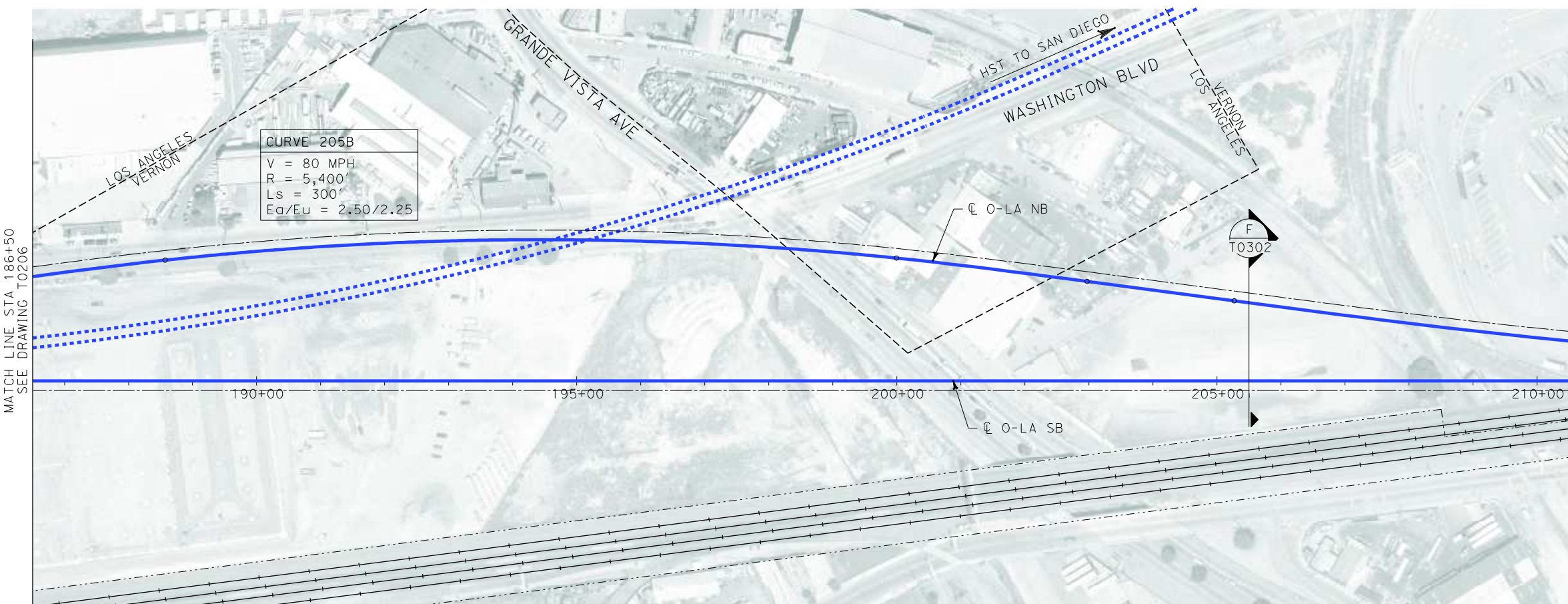
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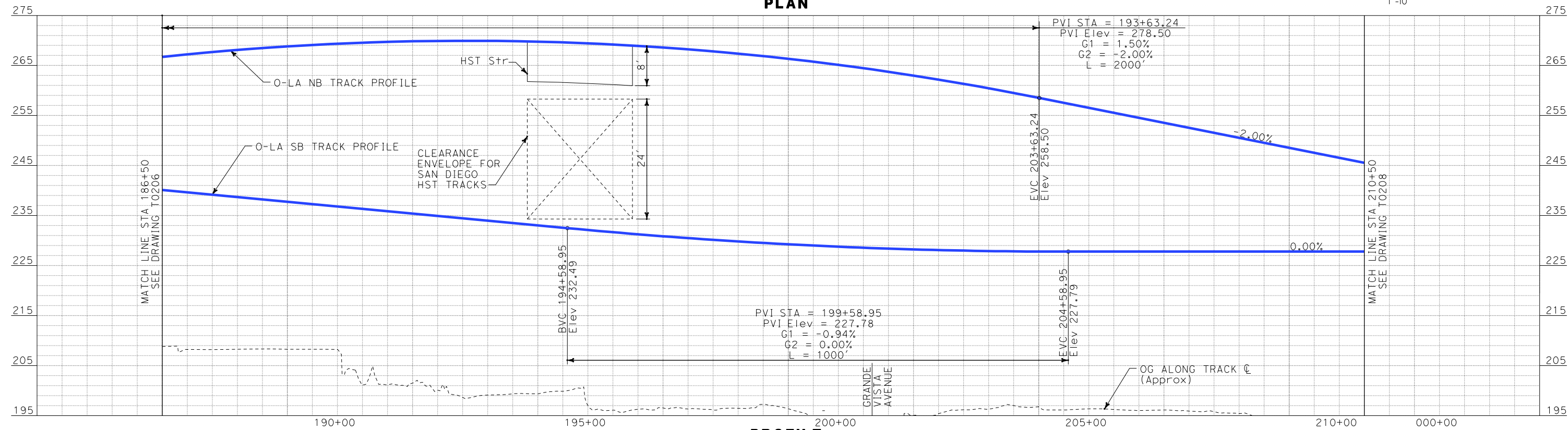
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0206
SCALE AS SHOWN
SHEET NO. 27 OF 245



PLAN




PROFILE

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DESIGNED BY
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DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

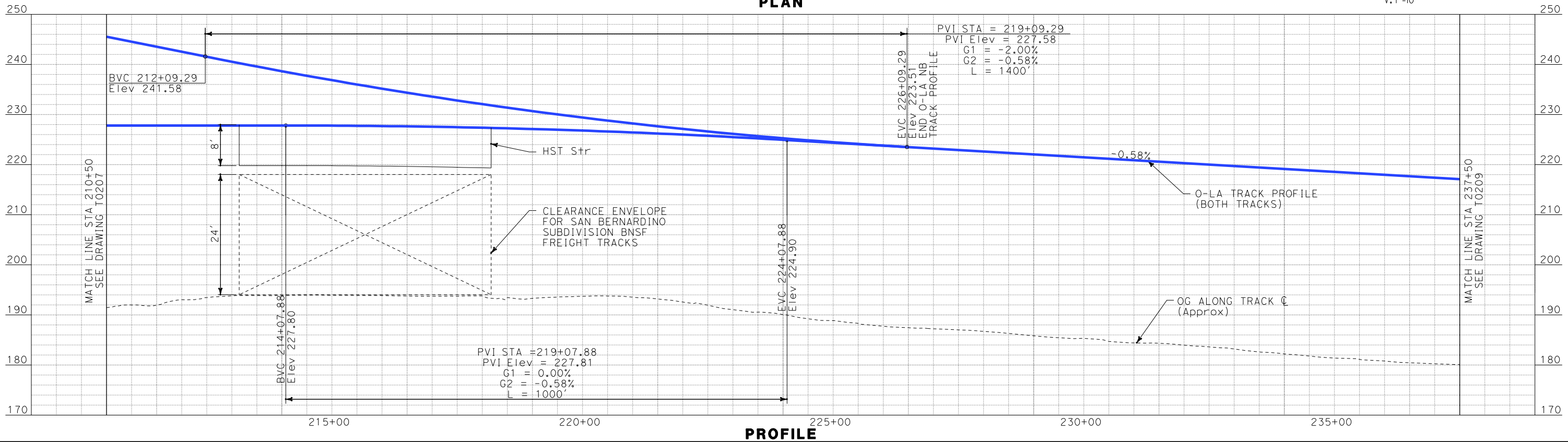
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0207
SCALE AS SHOWN
SHEET NO. 28 OF 245

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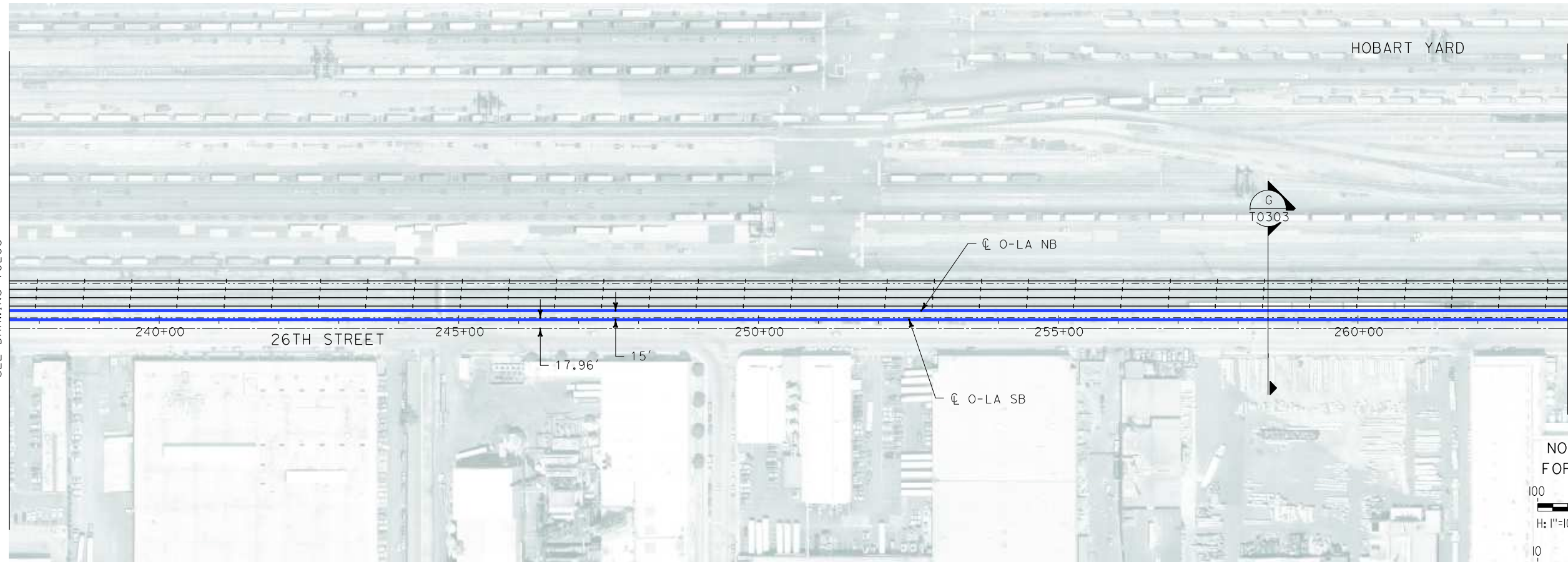
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT

PLAN AND PROFILE

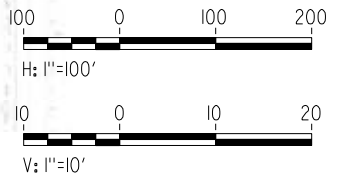
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SHEET NO. 29 OF 245

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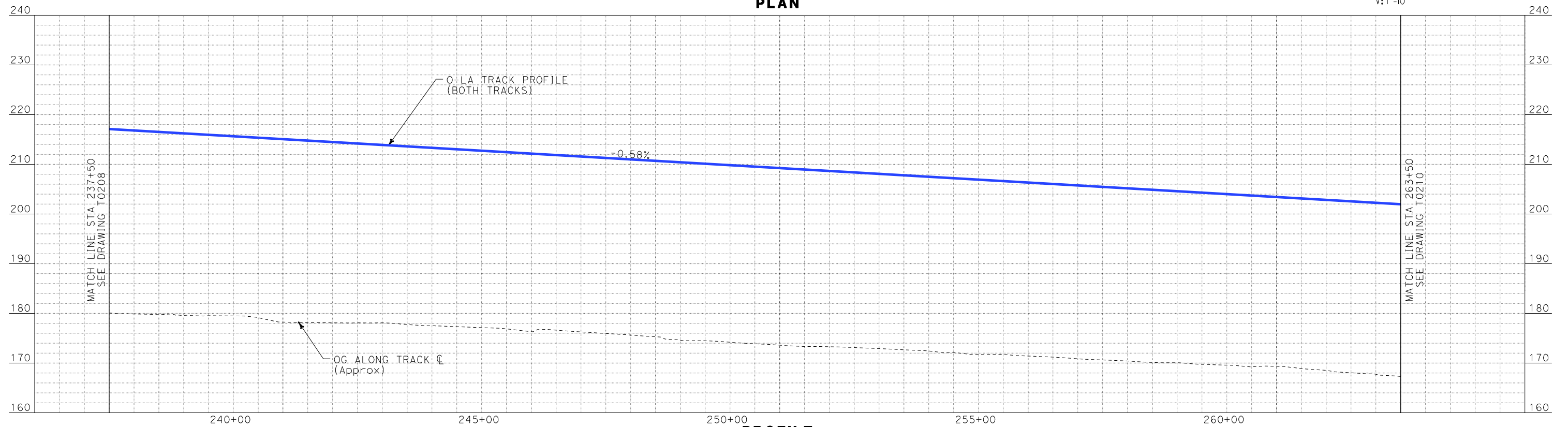
MATCH LINE STA 237+50
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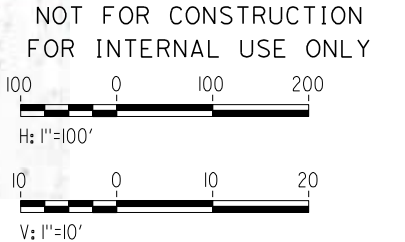
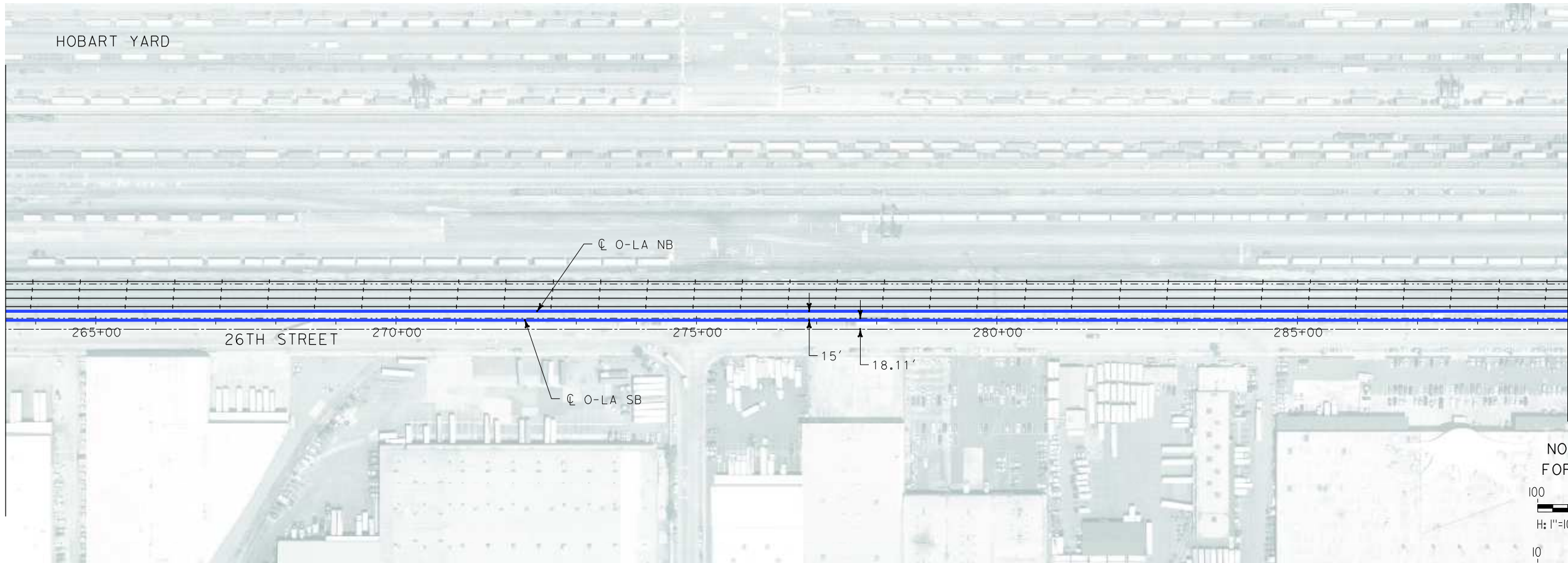


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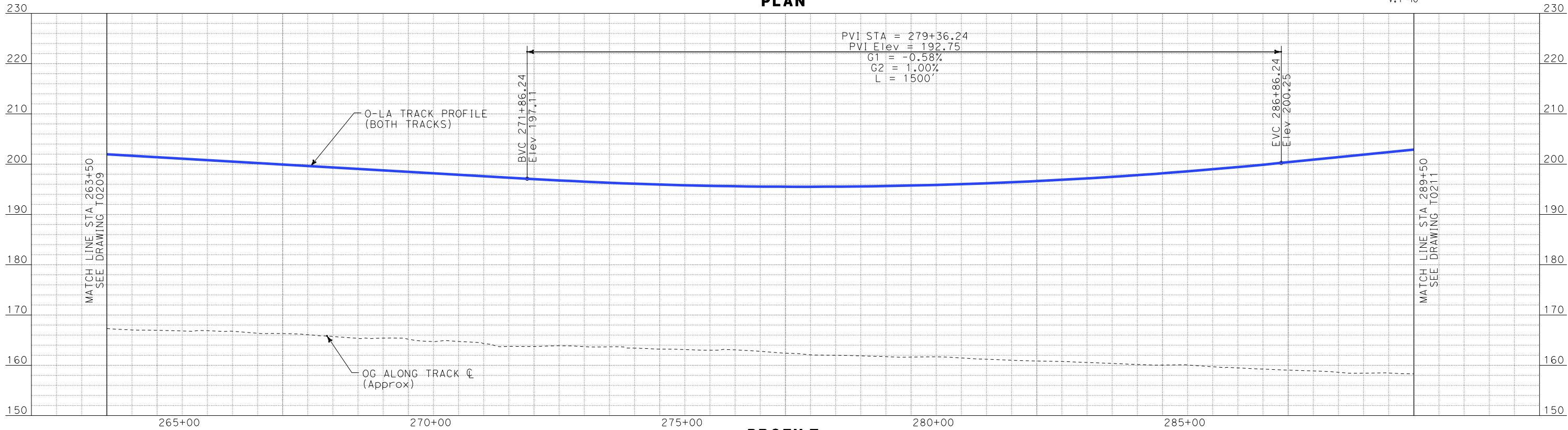
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0209
SCALE AS SHOWN
SHEET NO. 30 OF 245



PLAN



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LOS ANGELES, CA 90017-2556



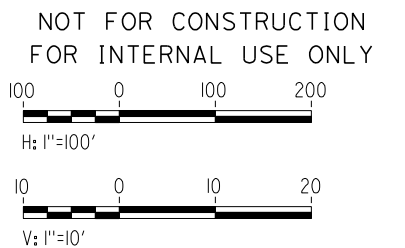
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

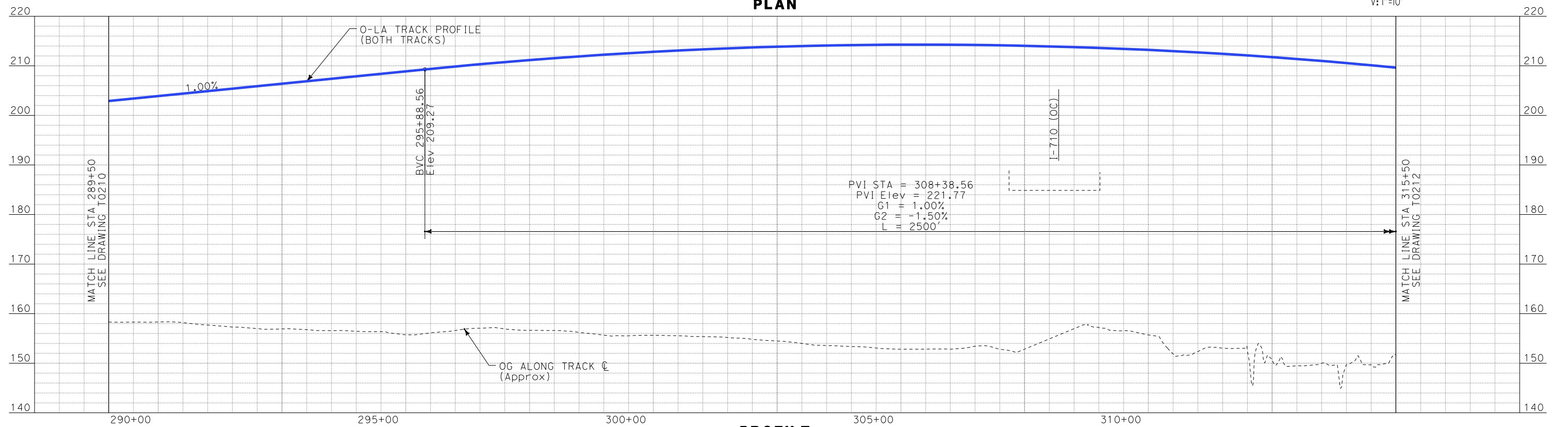
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0210
SCALE
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SHEET NO.
31 OF 245

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DRAWING NO.
T0211
SCALE
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SHEET NO.
32 OF 245

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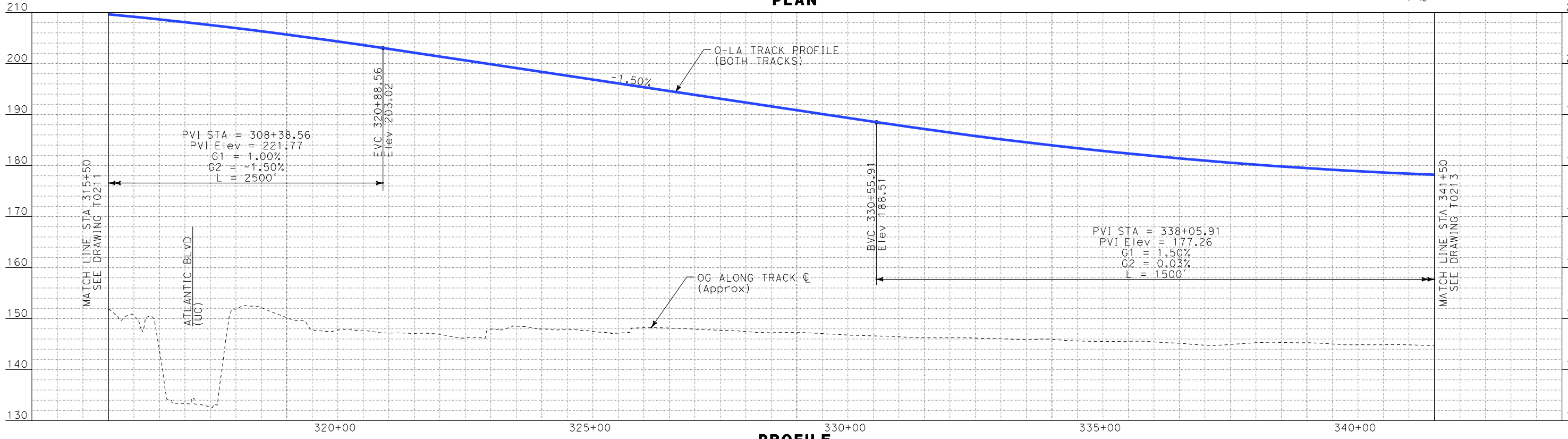
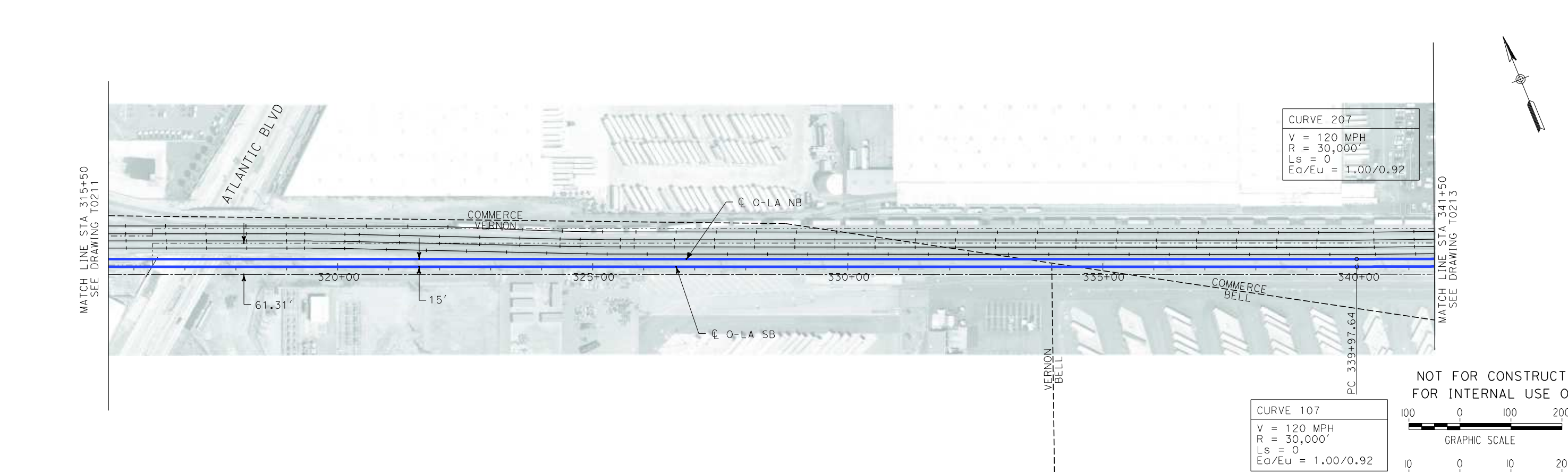


CALIFORNIA HIGH SPEED RAIL AUTHORITY
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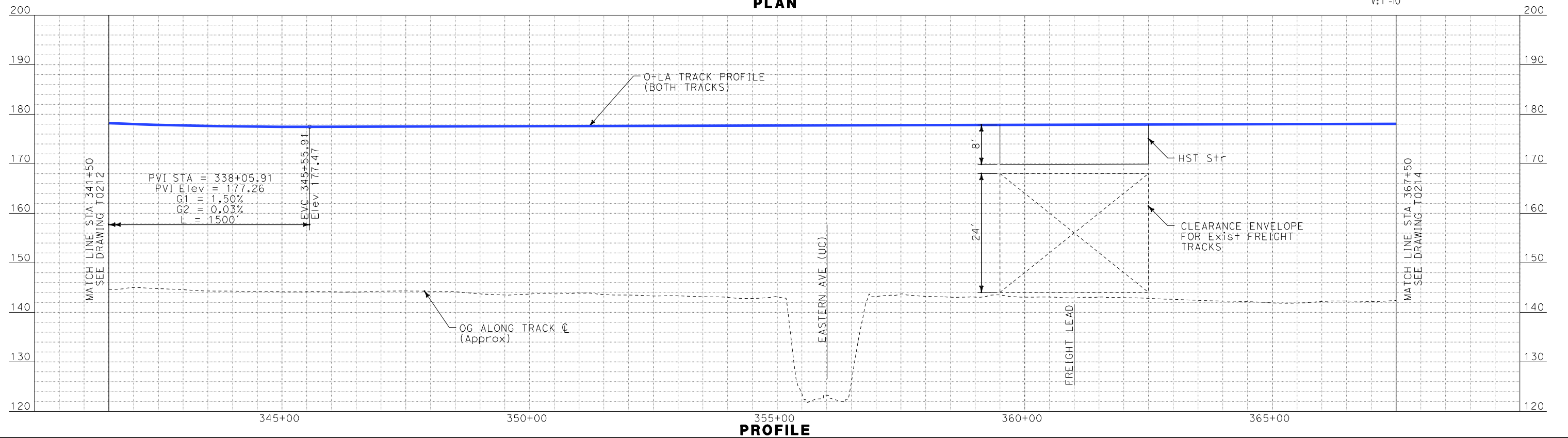
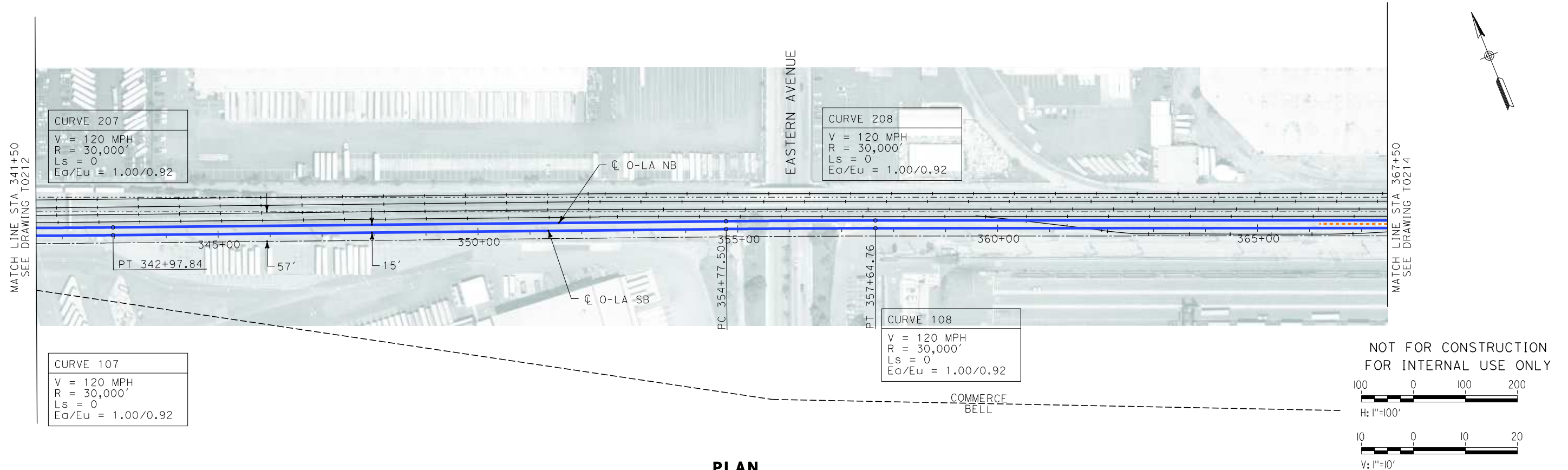
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CONTRACT NO.	HSR06-0005
DRAWING NO.	T0212
SCALE	AS SHOWN
SHEET NO.	33 OF 245



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DATE 02-13-09

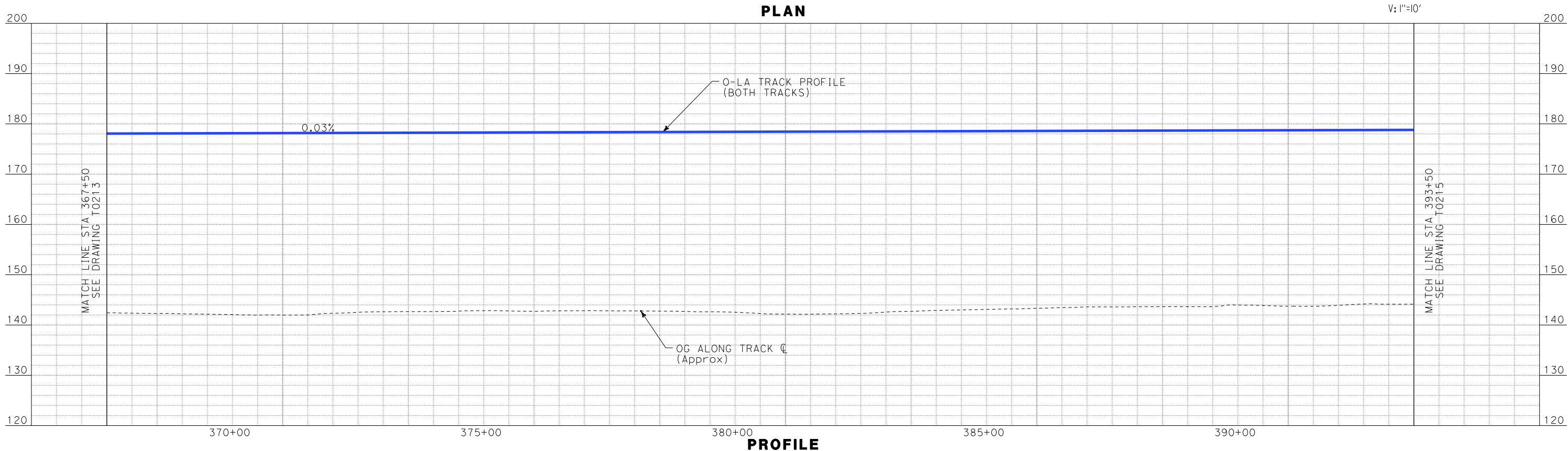
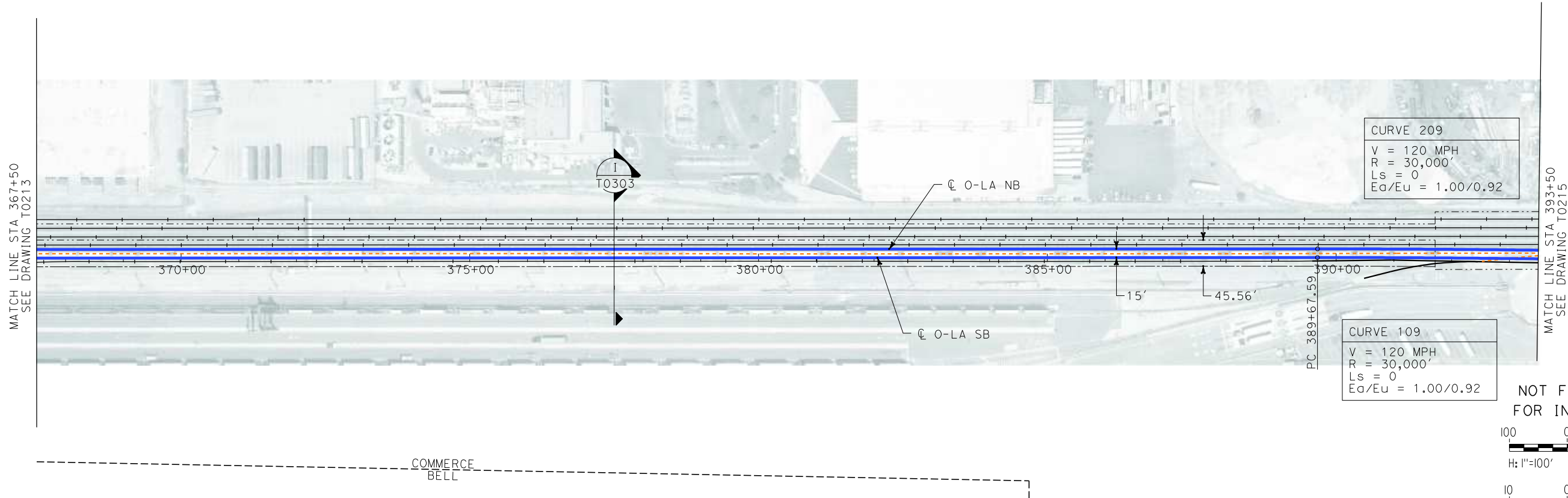
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LOS ANGELES, CA 90017-2556



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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE


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SHEET NO. 34 OF 245

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LOS ANGELES, CA 90017-2556

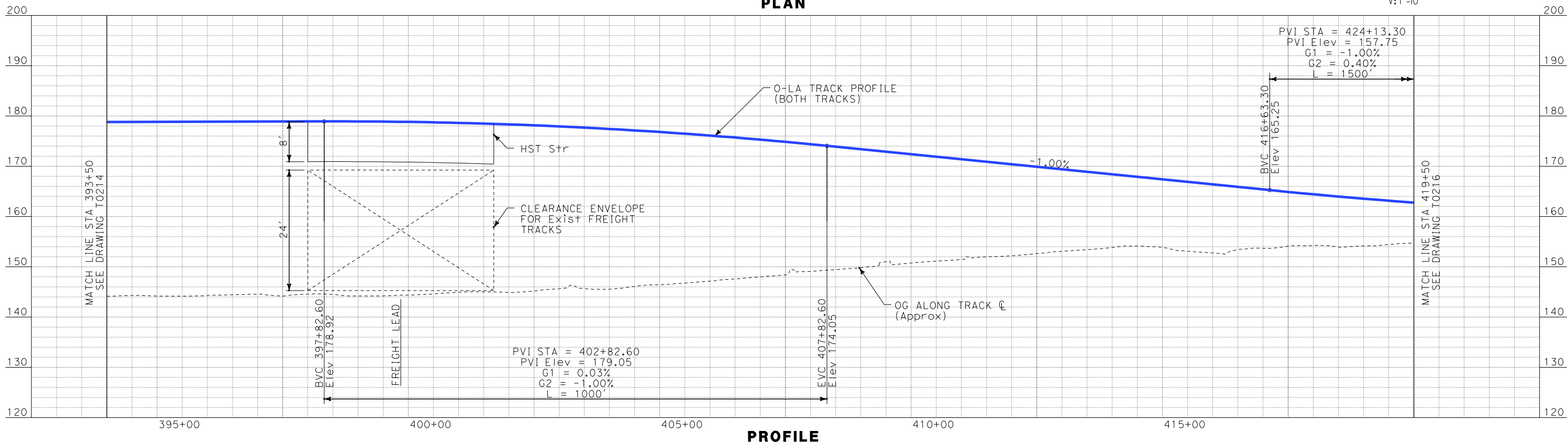
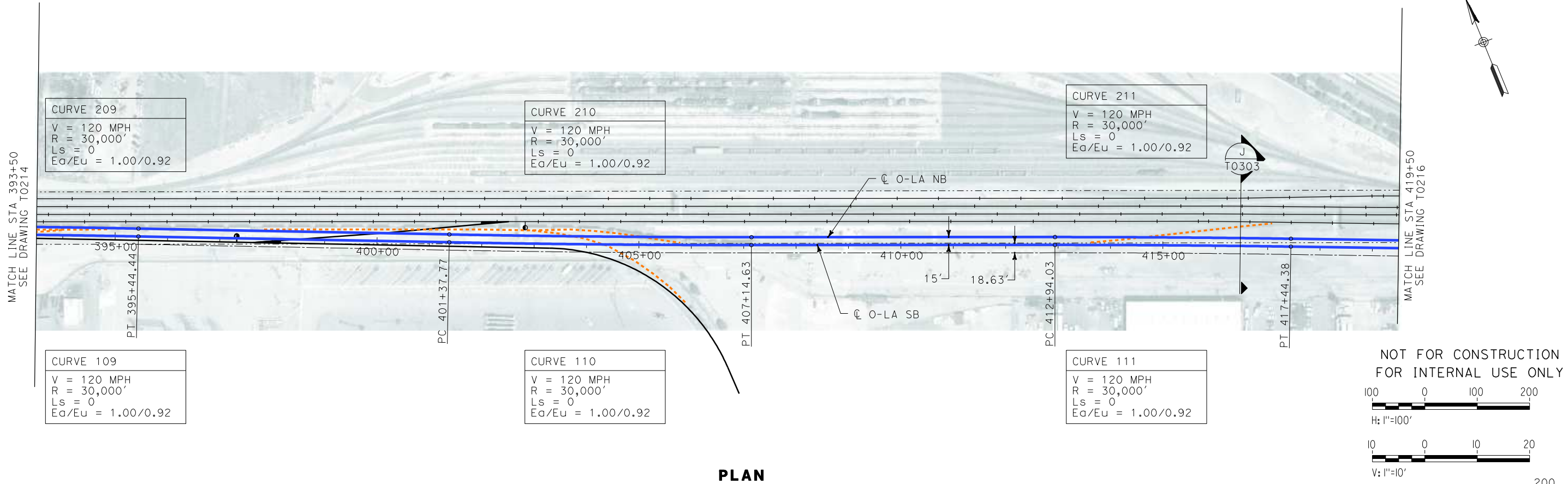
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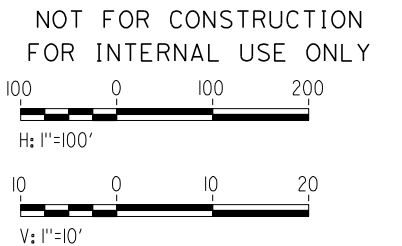
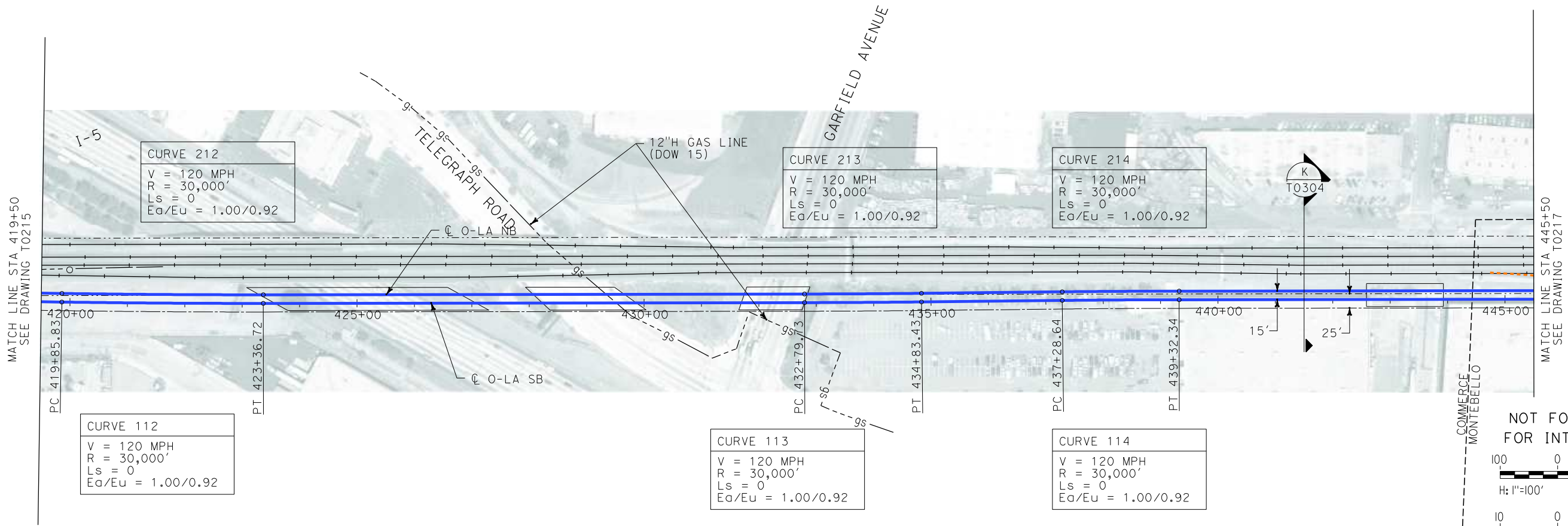
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SHEET NO. 35 OF 245

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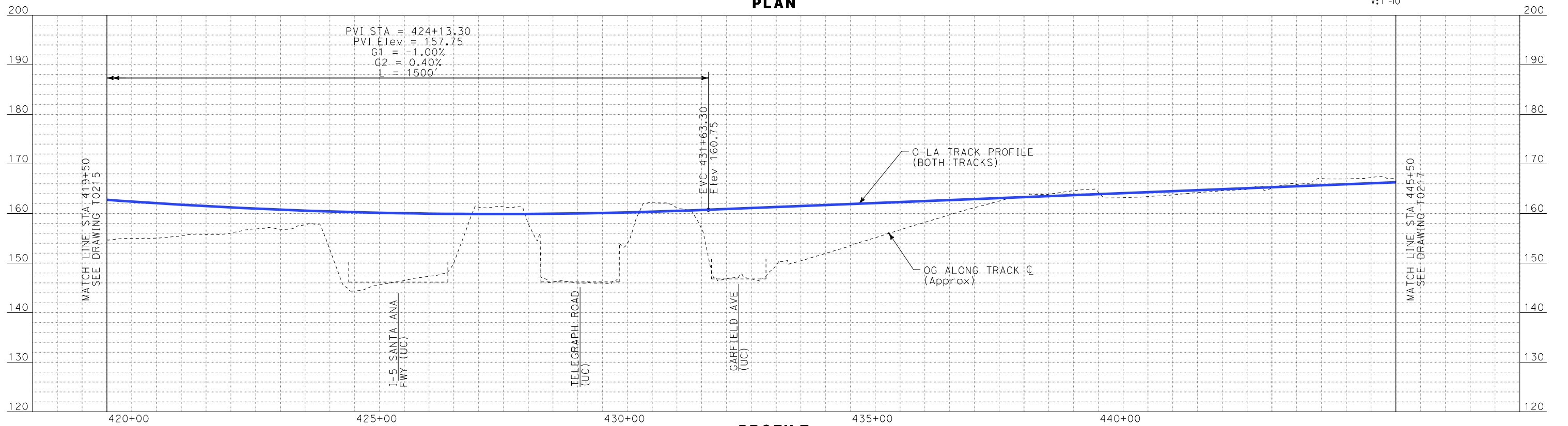


						DESIGNED BY K. SPERRY	 STV Incorporated <small>ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS 1055 WEST SEVENTH STREET, SUITE 3150 LOS ANGELES, CA 90017-2556</small>	 CALIFORNIA HIGH SPEED RAIL AUTHORITY FLY CALIFORNIA <i>Without ever leaving the ground.</i>	CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES BASELINE ALIGNMENT PLAN AND PROFILE		CONTRACT NO. HSR06-0005
						DRAWN BY K. SPERRY					DRAWING NO. T0215
						CHECKED BY A. SOKOL					SCALE AS SHOWN
						IN CHARGE D. THOMSON					SHEET NO. 36 OF 245
						DATE 02-13-09					
REV	DATE	BY	SUB	APP	DESCRIPTION						

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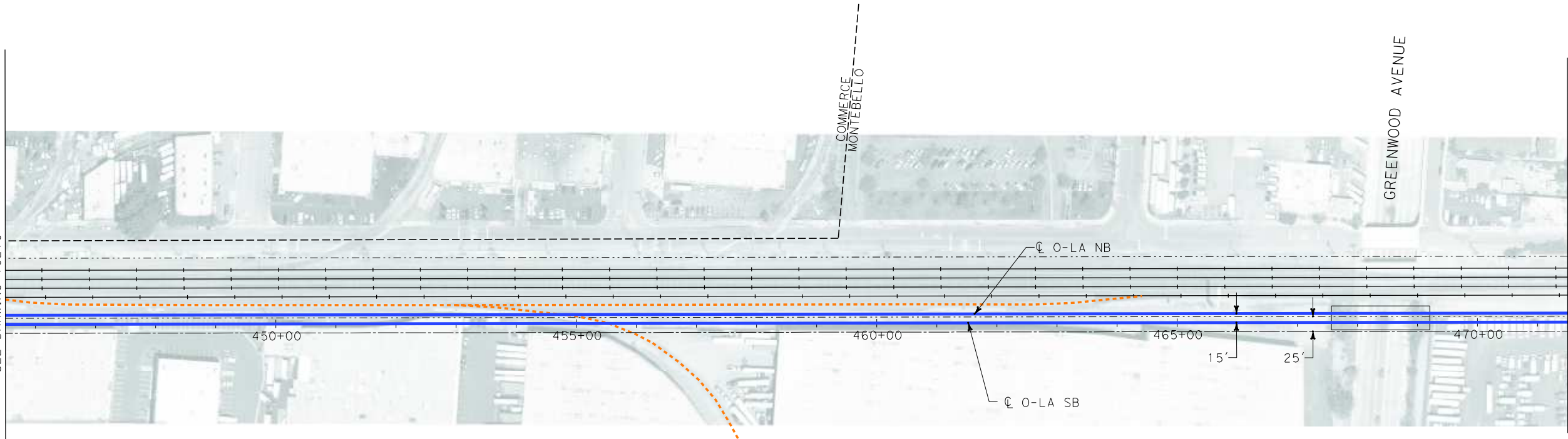
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CONTRACT NO.
HSR06-0005
DRAWING NO.
T0216
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AS SHOWN
SHEET NO.
37 OF 245

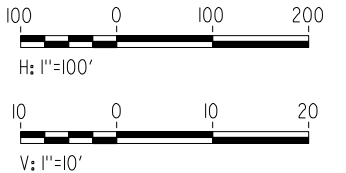
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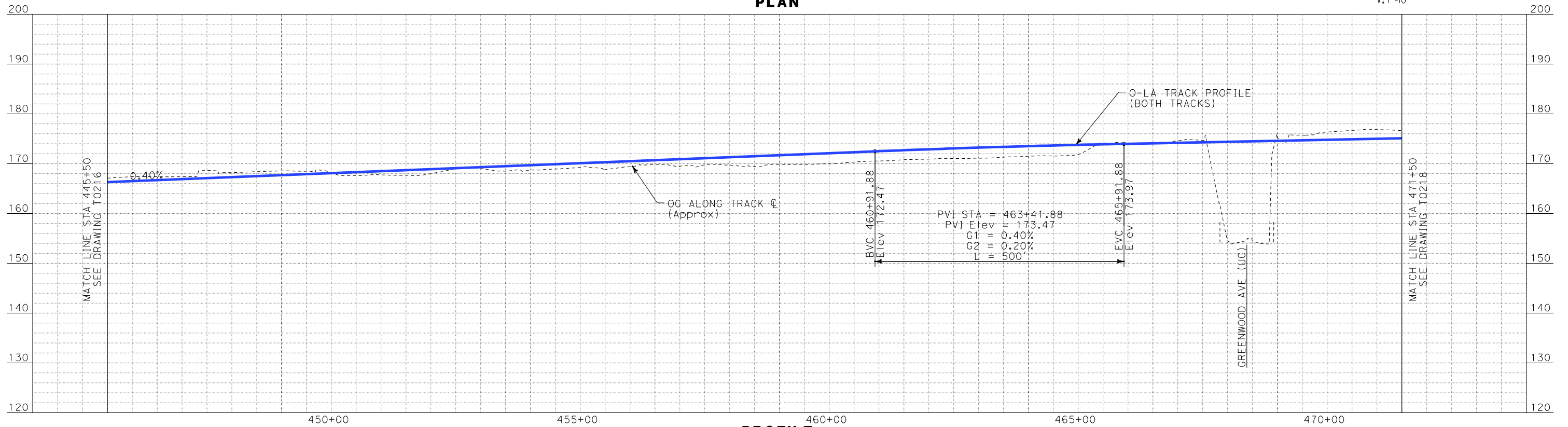
MATCH LINE STA 445+50
SEE DRAWING T0216



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
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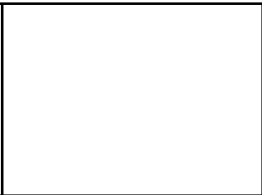
PROFILE

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LOS ANGELES, CA 90017-2556



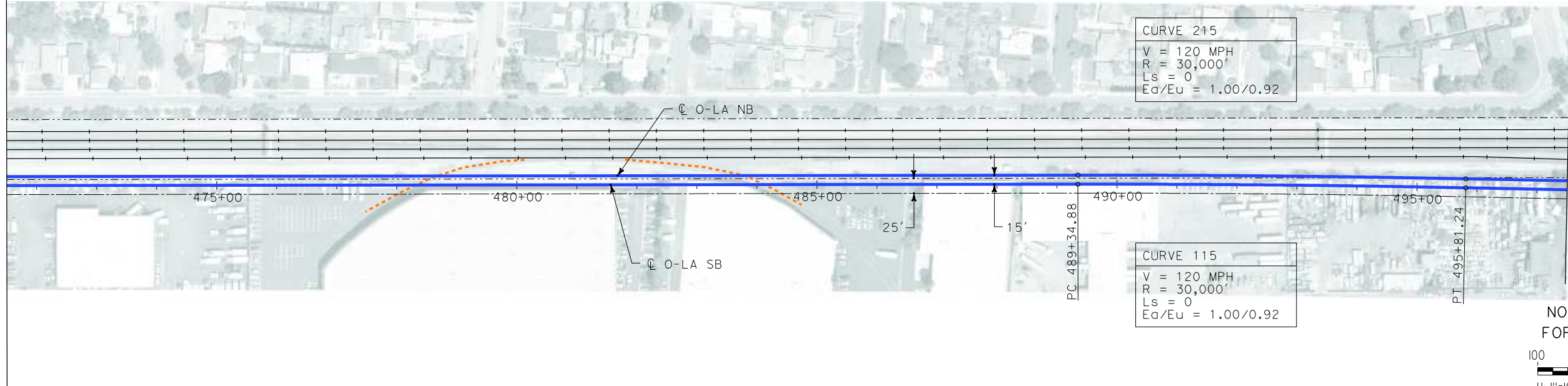
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

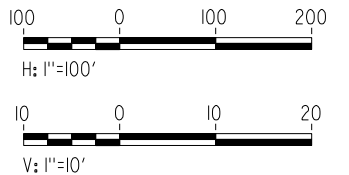
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DRAWING NO. T0217
SCALE AS SHOWN
SHEET NO. 38 OF 245

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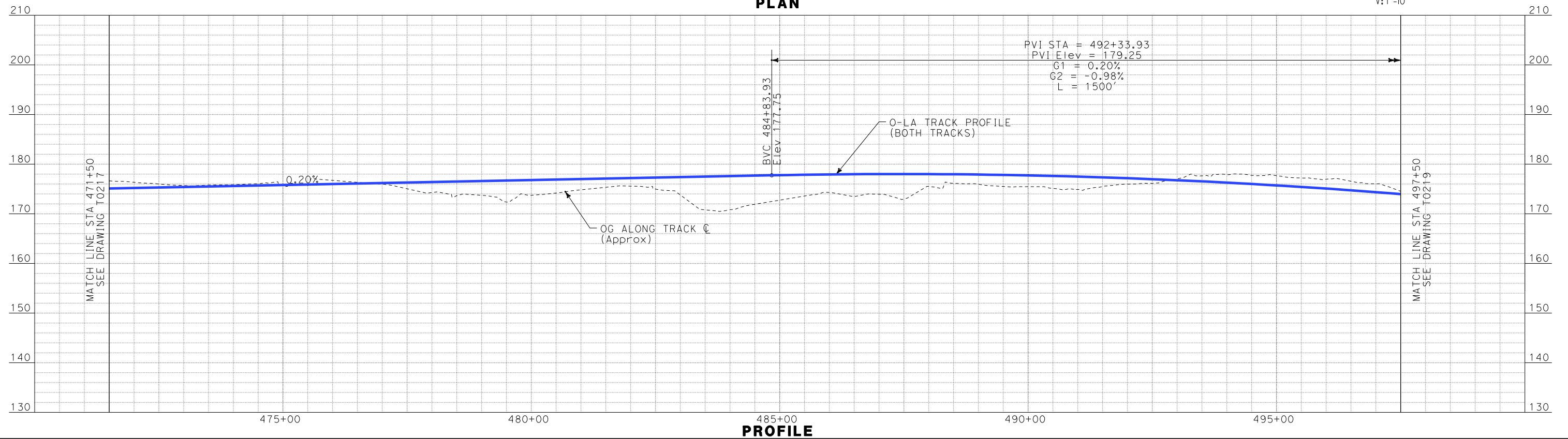
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
PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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CHECKED BY A. SOKOL
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DATE 02-13-09

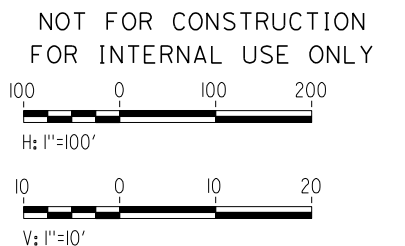
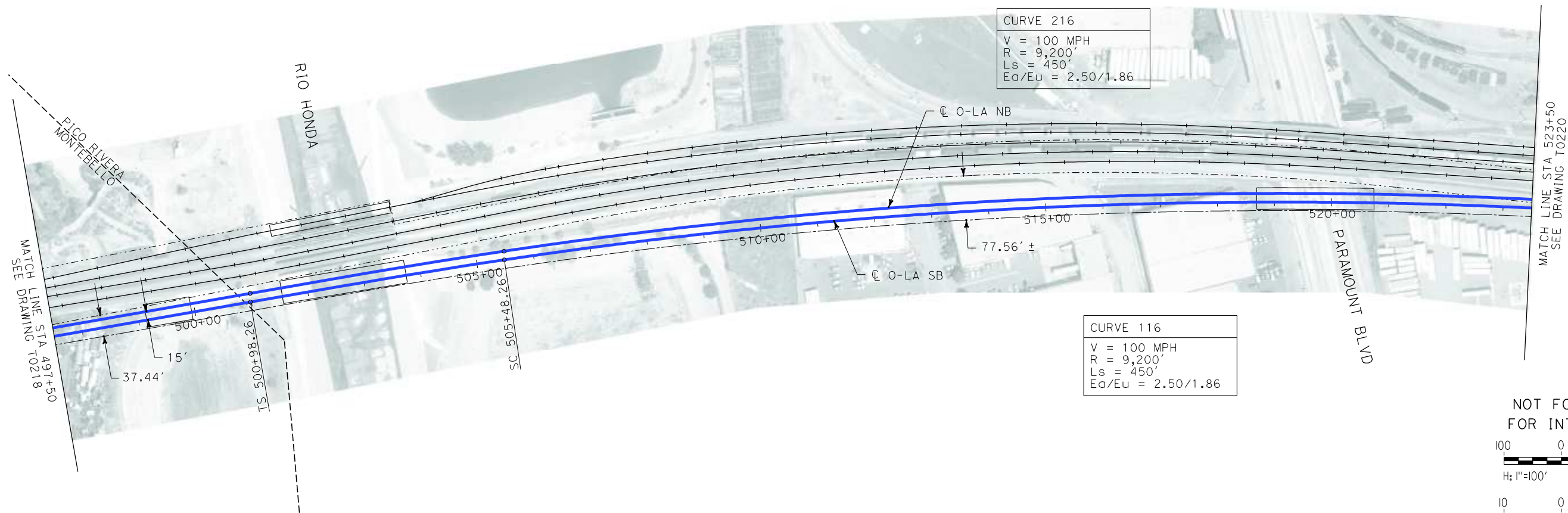
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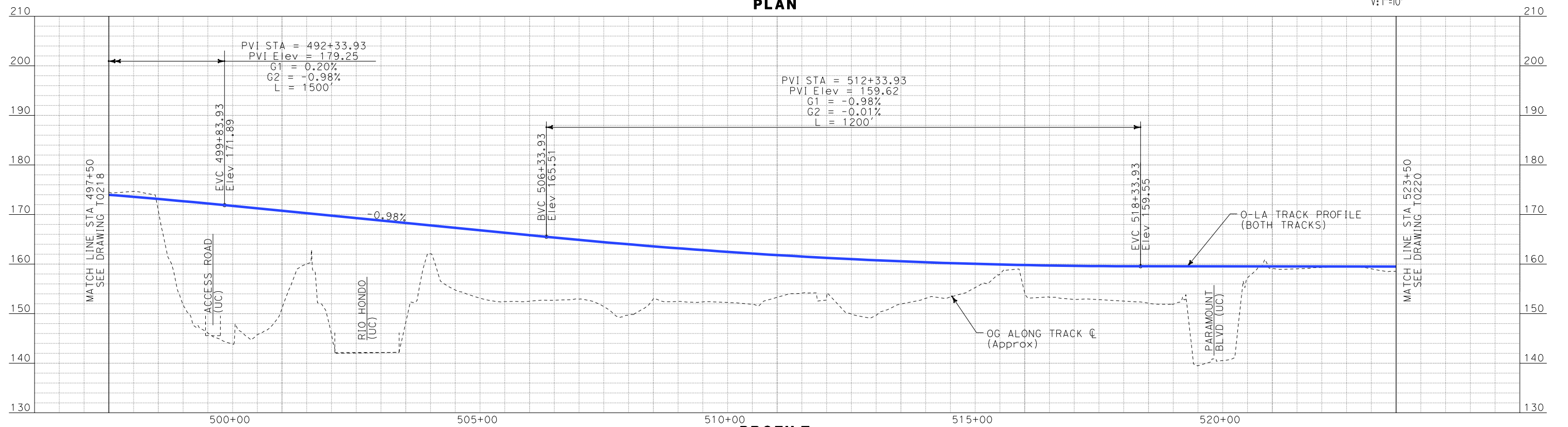
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CONTRACT NO. HSR06-0005
DRAWING NO. T0218
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SHEET NO. 39 OF 245

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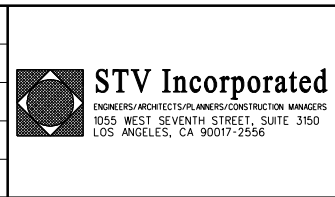
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REV	DATE	BY	SUB	APP	DESCRIPTION

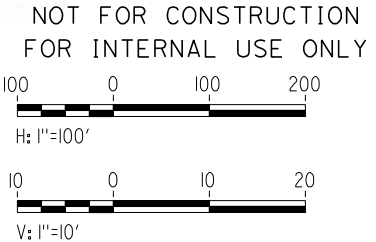
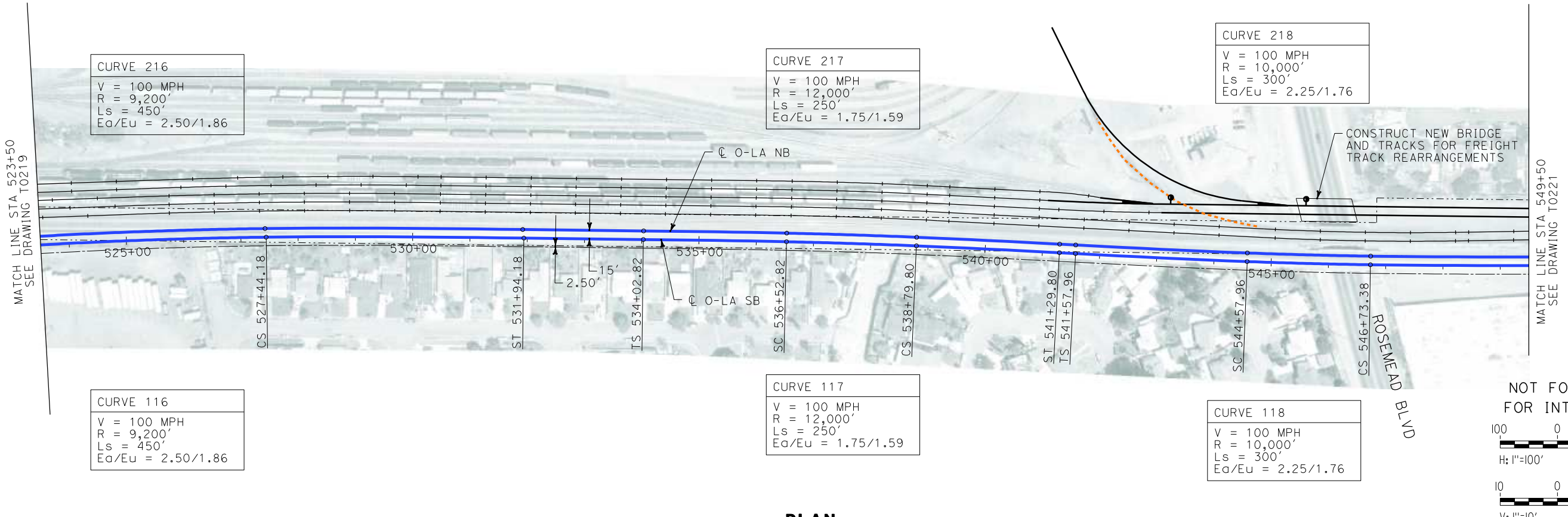
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IN CHARGE
D. THOMSON
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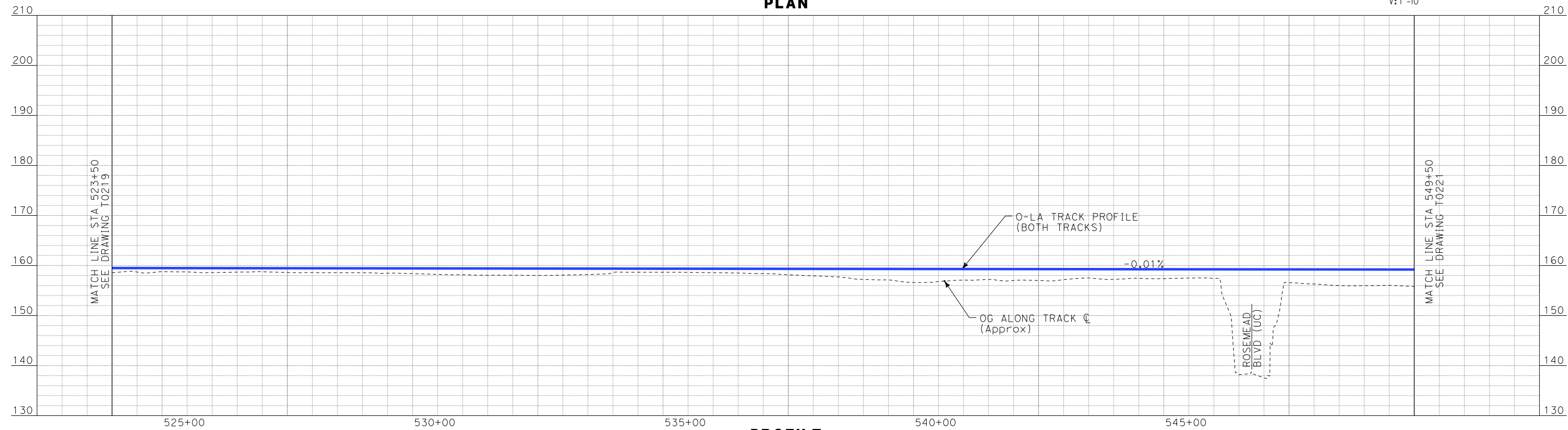
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CONTRACT NO. HSR06-0005
DRAWING NO. T0219
SCALE AS SHOWN
SHEET NO. 40 OF 245

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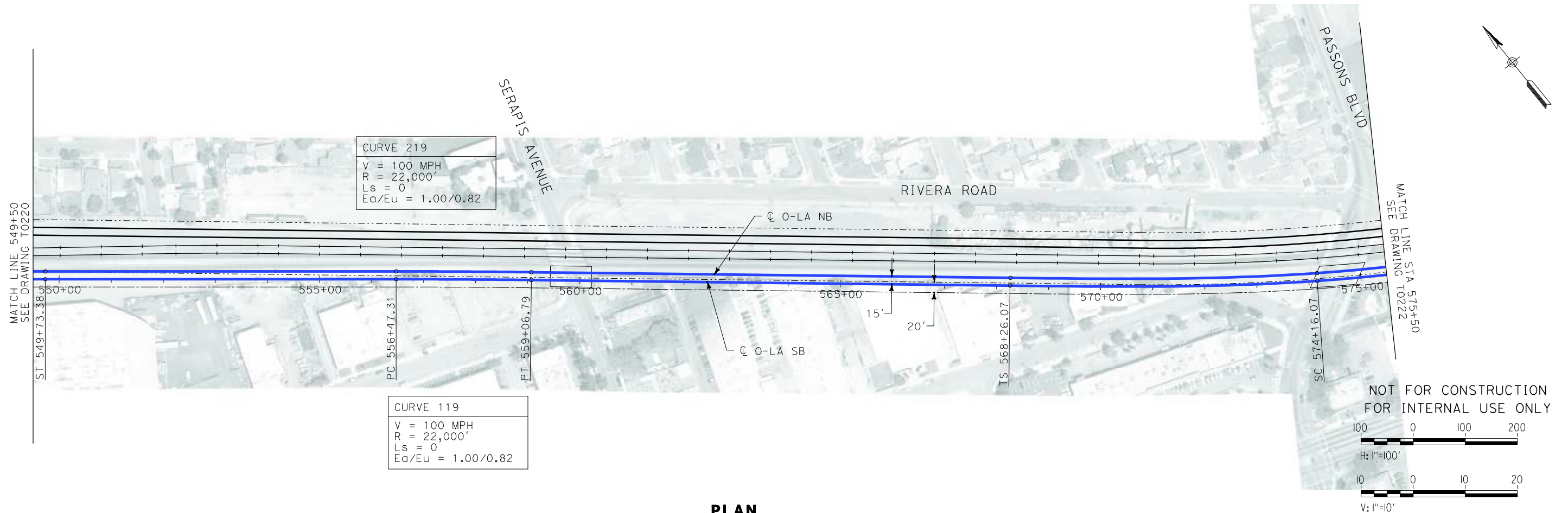
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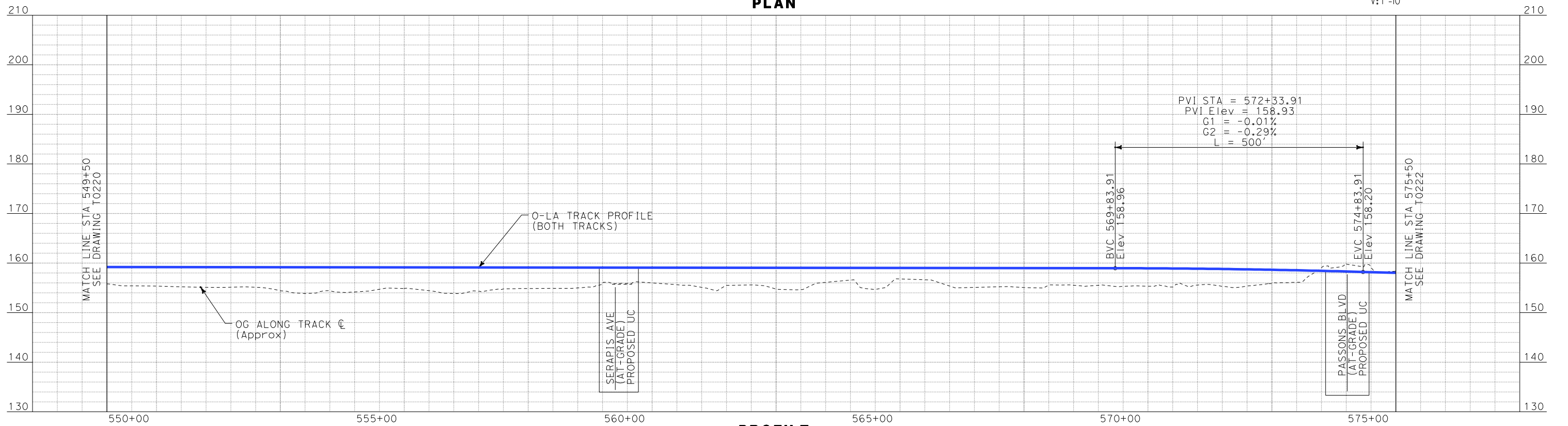
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CONTRACT NO. HSR06-0005
DRAWING NO. T0220
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SHEET NO. 41 OF 245

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
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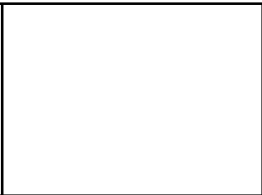
PROFILE

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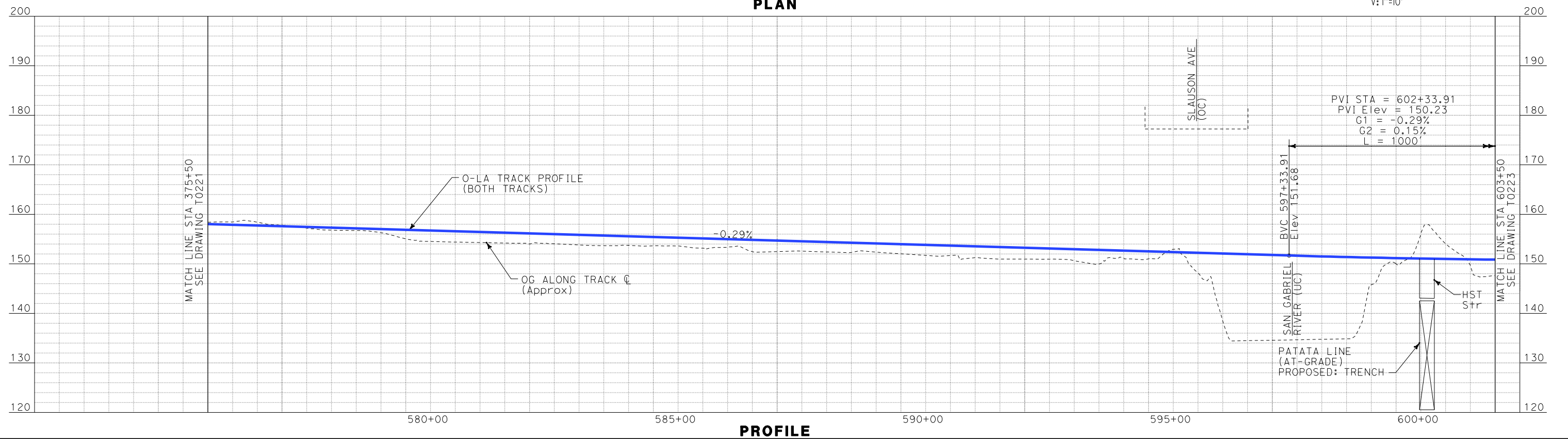
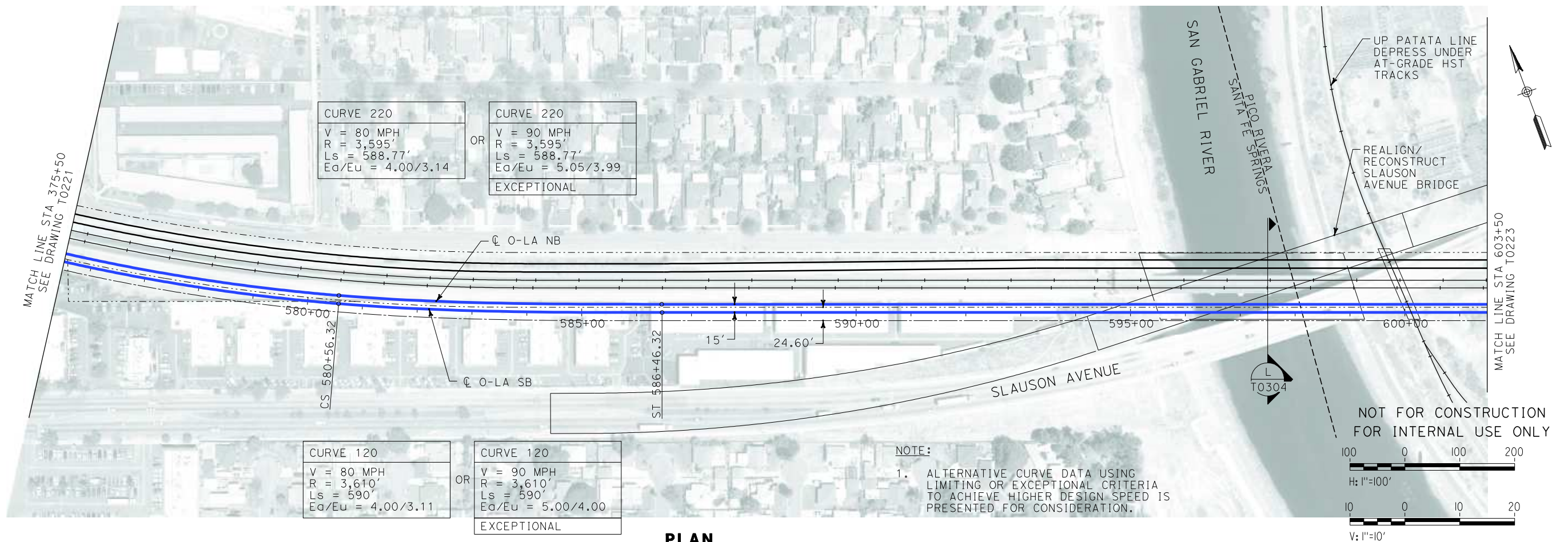


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CONTRACT NO. HSR06-0005
DRAWING NO. T0221
SCALE AS SHOWN
SHEET NO. 42 OF 245

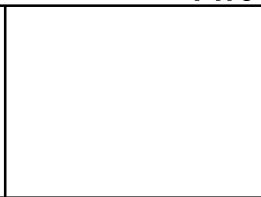
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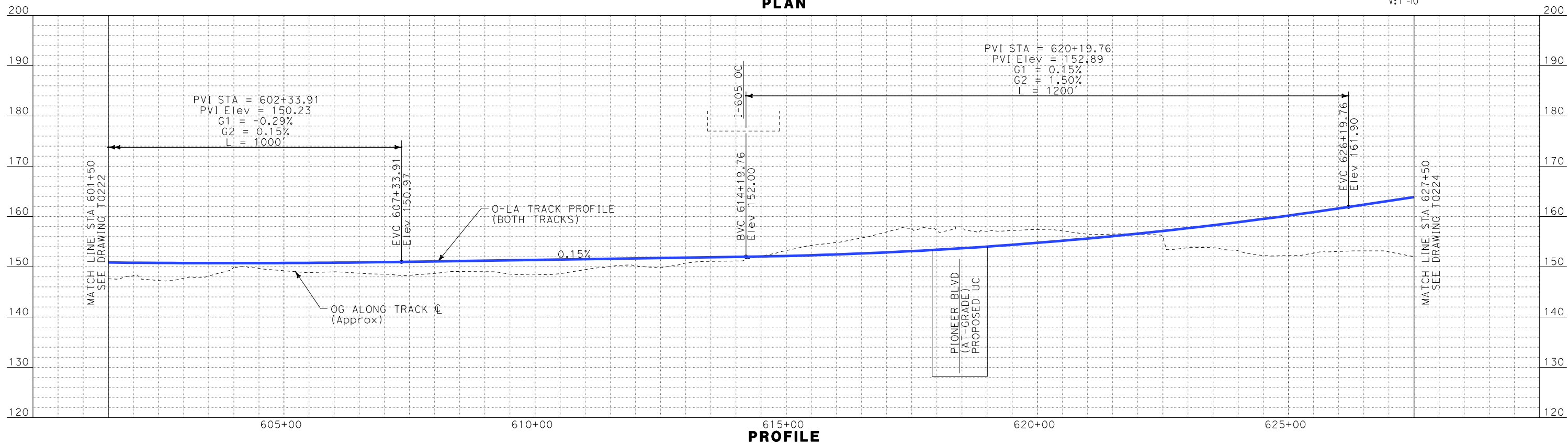
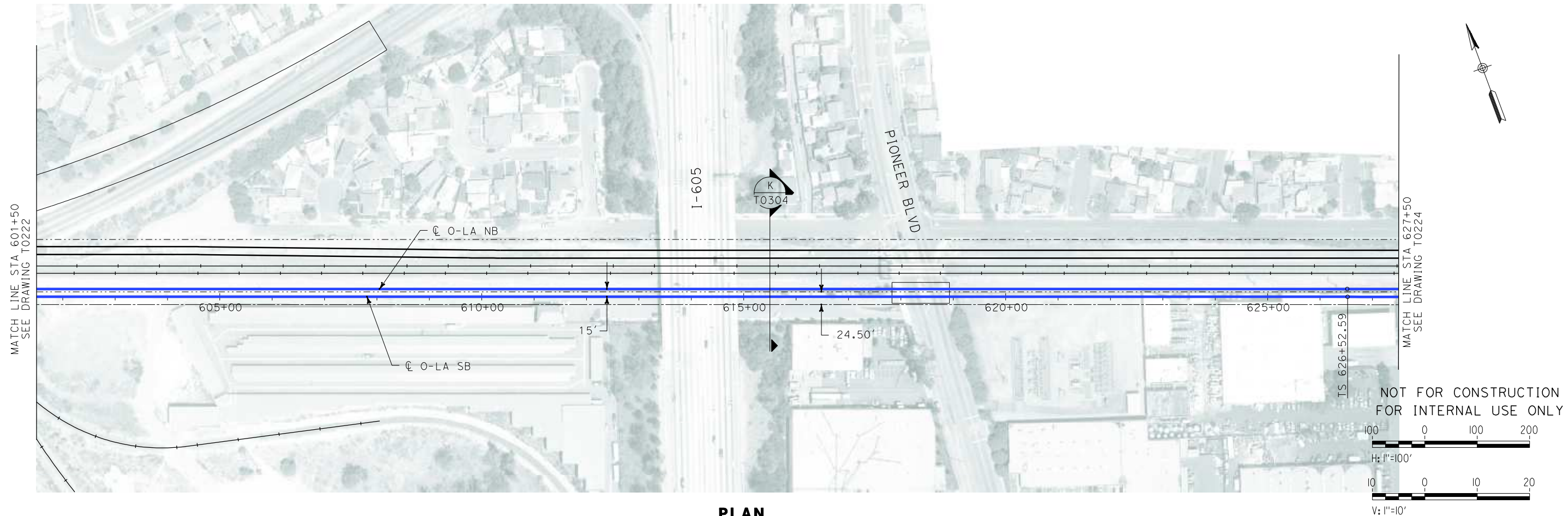


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
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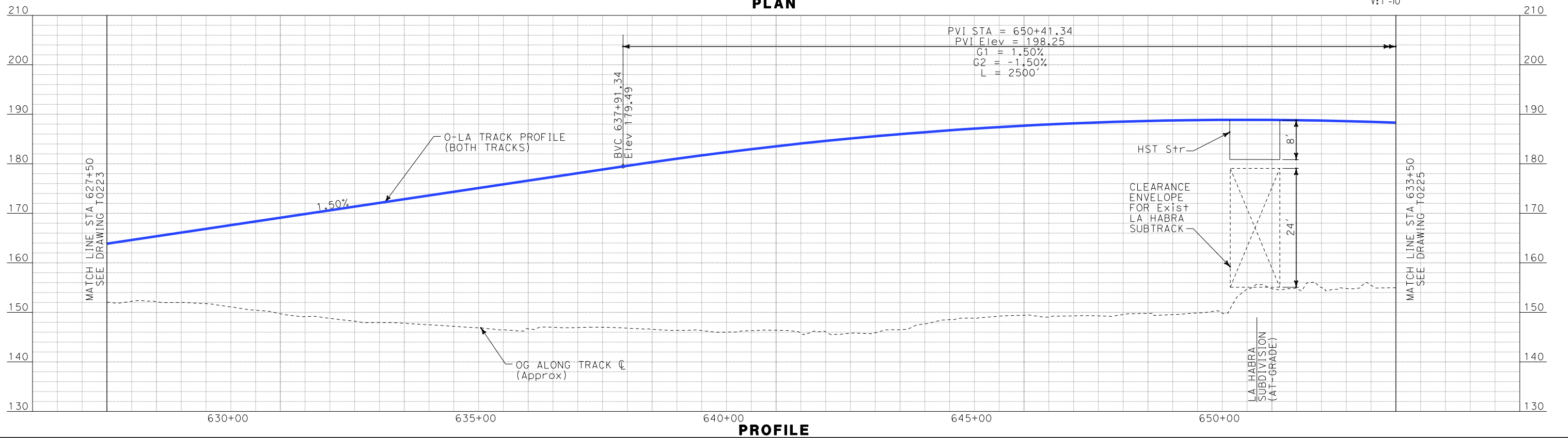
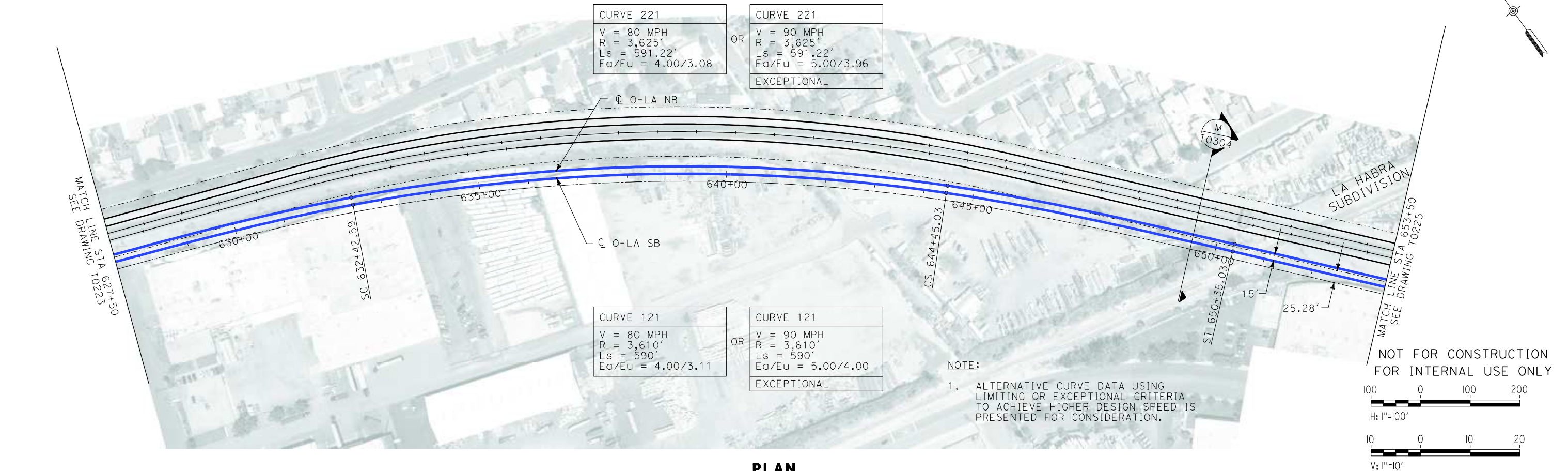


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DRAWING NO. T0223
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SHEET NO. 44 OF 245

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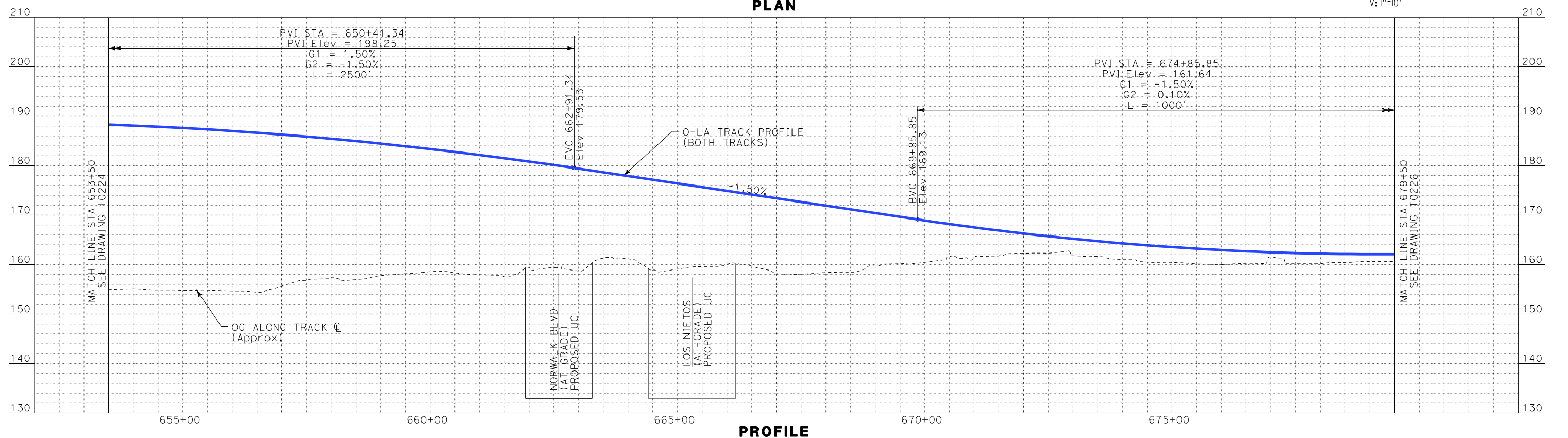
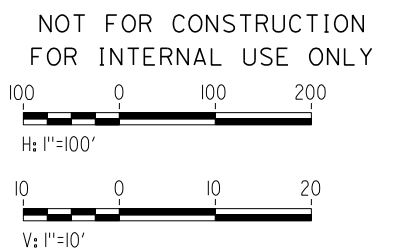
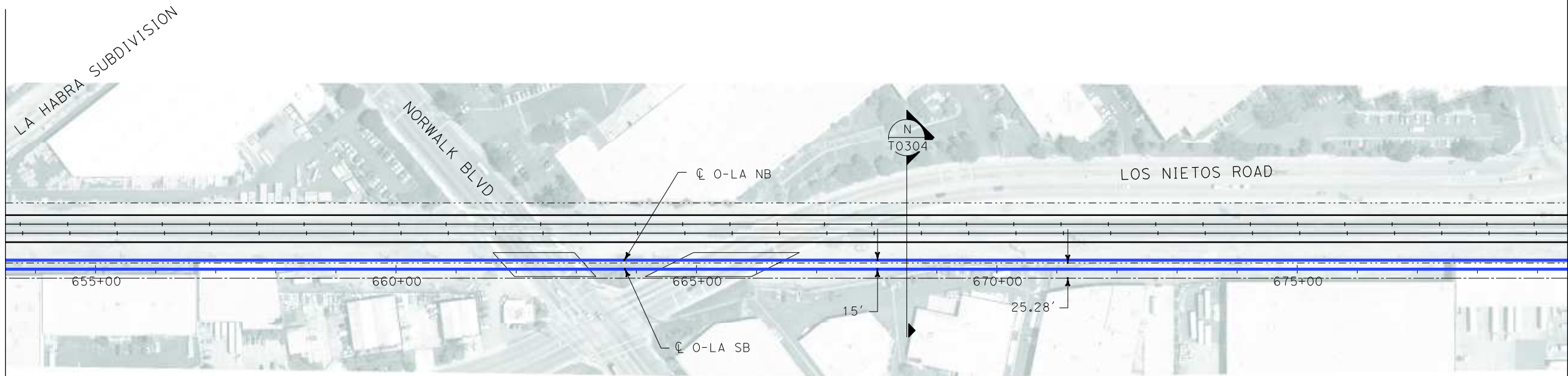
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
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HSR06-0005
DRAWING NO.
T0224
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45 OF 245



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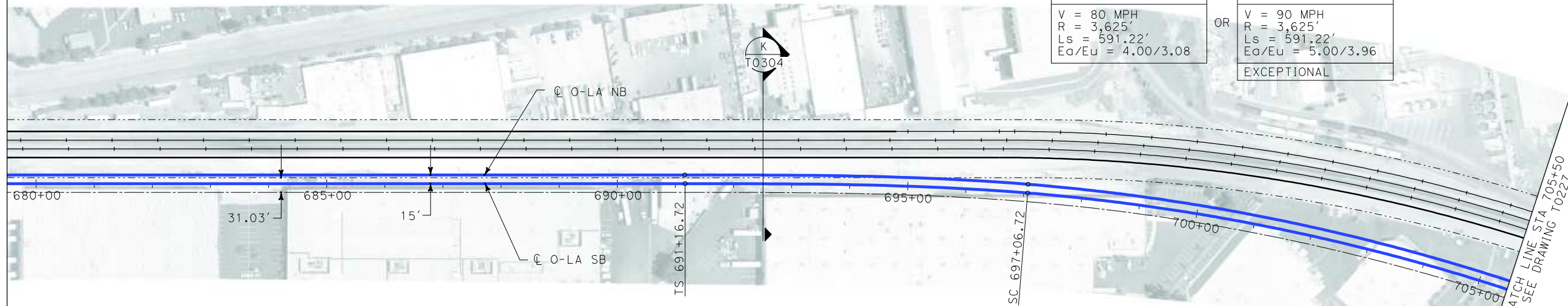
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CONTRACT NO.	HSR06-0005
DRAWING NO.	T0225
SCALE	AS SHOWN
SHEET NO.	46 OF 245

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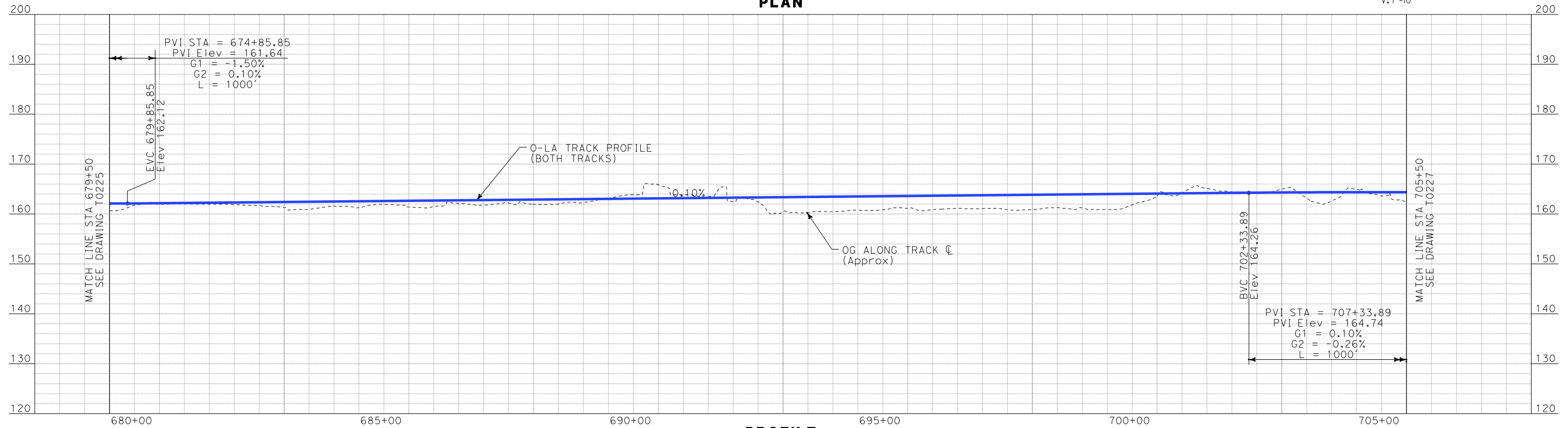
MATCH LINE STA 679+50
SEE DRAWING T0225



NOTE:

1. ALTERNATIVE CURVE DATA USING LIMITING OR EXCEPTIONAL CRITERIA TO ACHIEVE HIGHER DESIGN SPEED IS PRESENTED FOR CONSIDERATION.

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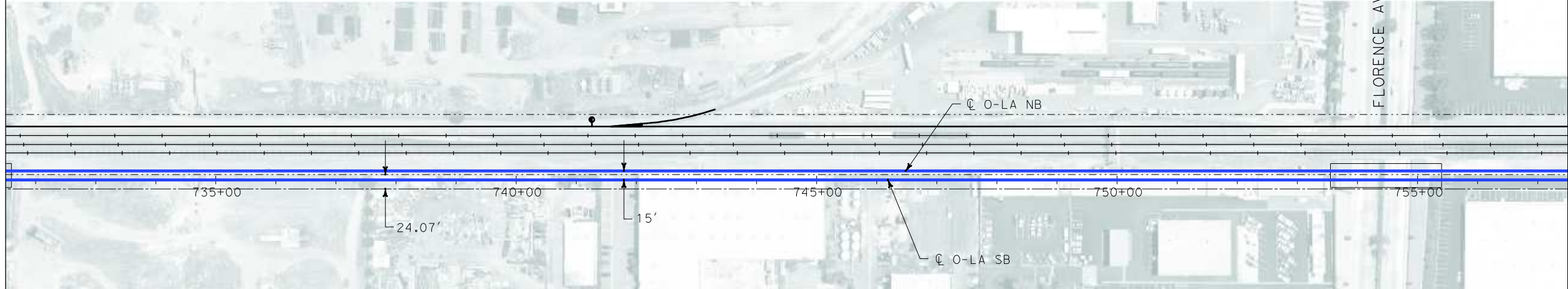


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DRAWING NO. T0226
SCALE AS SHOWN
SHEET NO. 47 OF 245

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MATCH LINE STA 731+50
SEE DRAWING T0227



MATCH LINE STA 757+50
SEE DRAWING T0229

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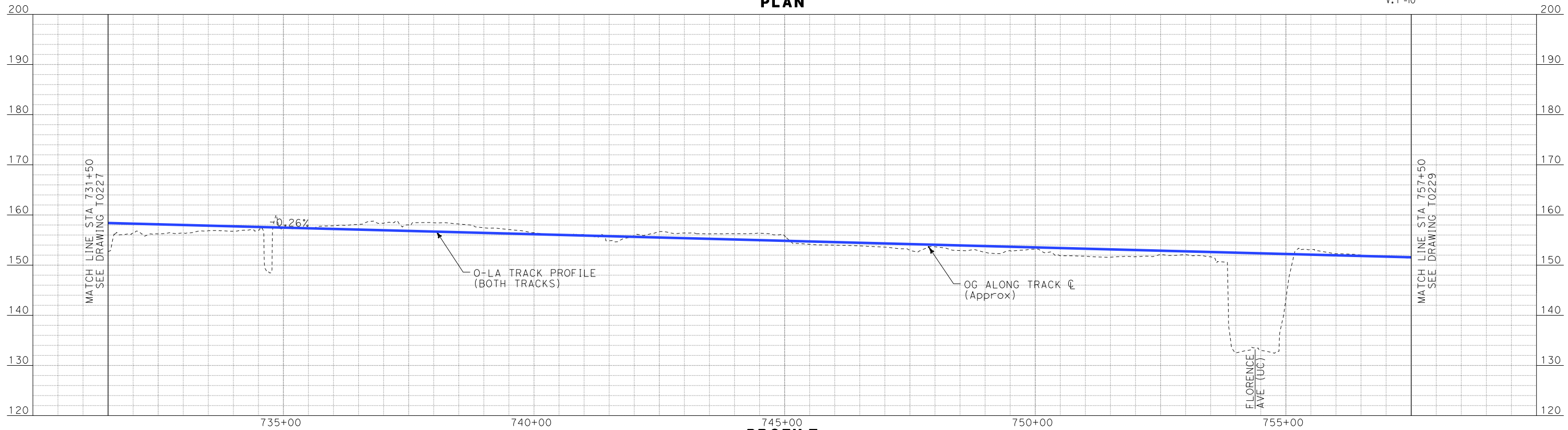


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V: 1"=10'


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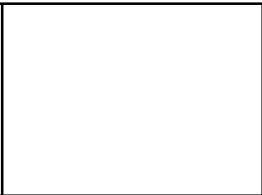
PROFILE

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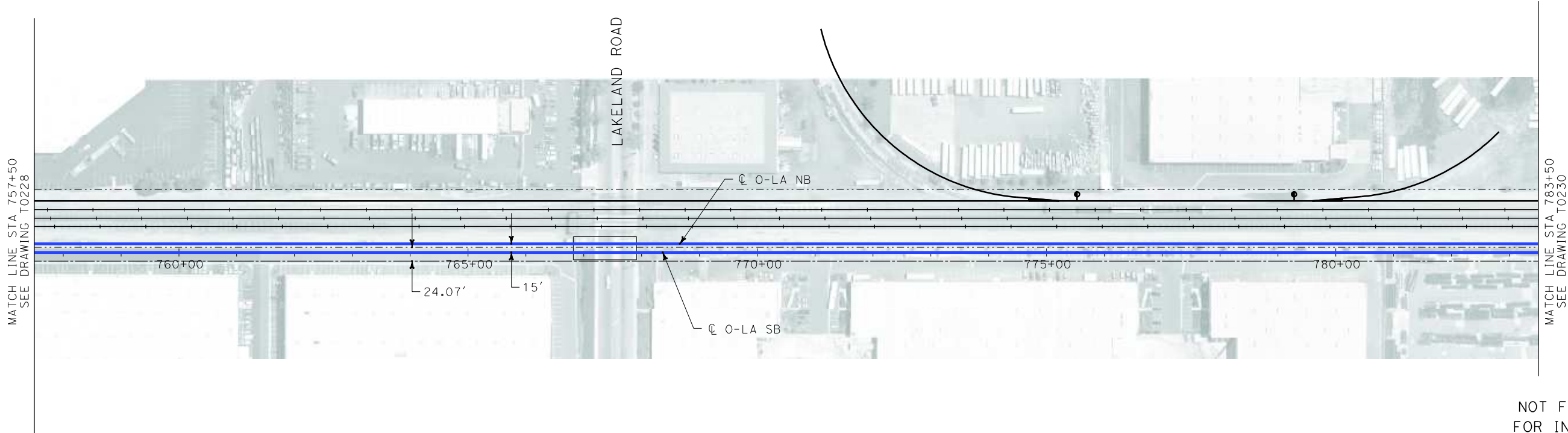


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ORANGE COUNTY TO LOS ANGELES
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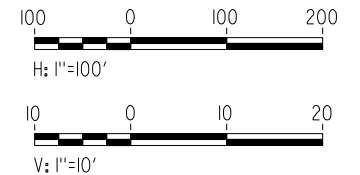
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0228
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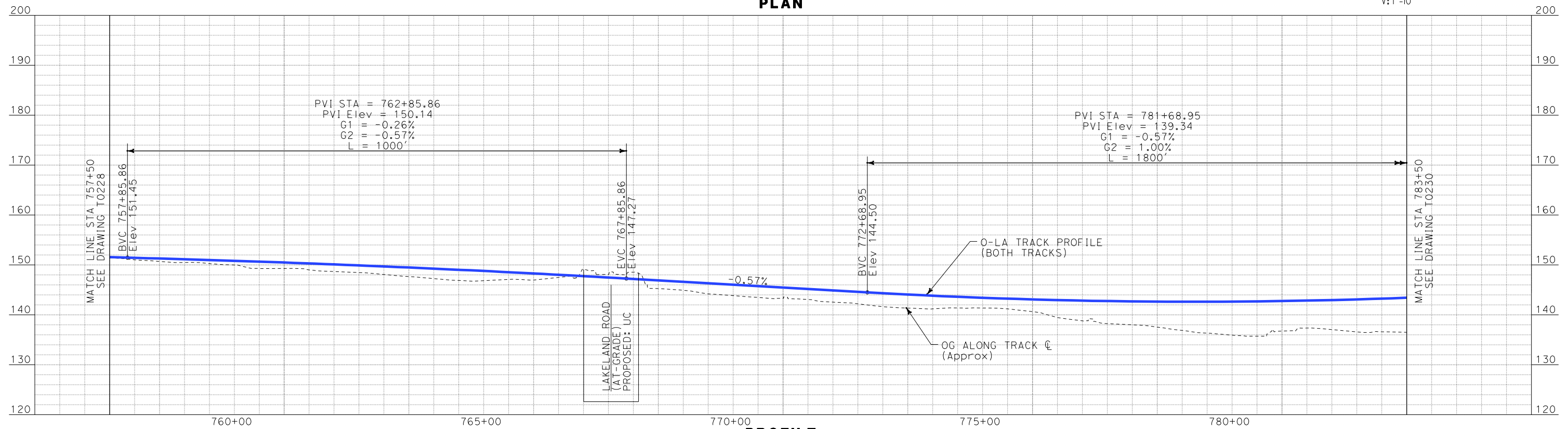
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
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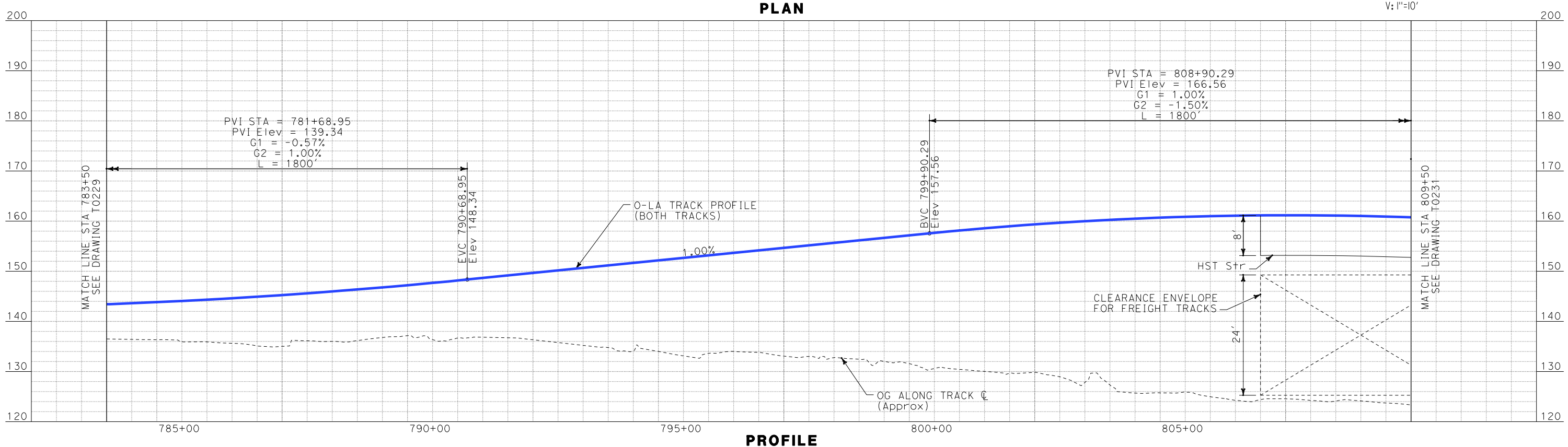
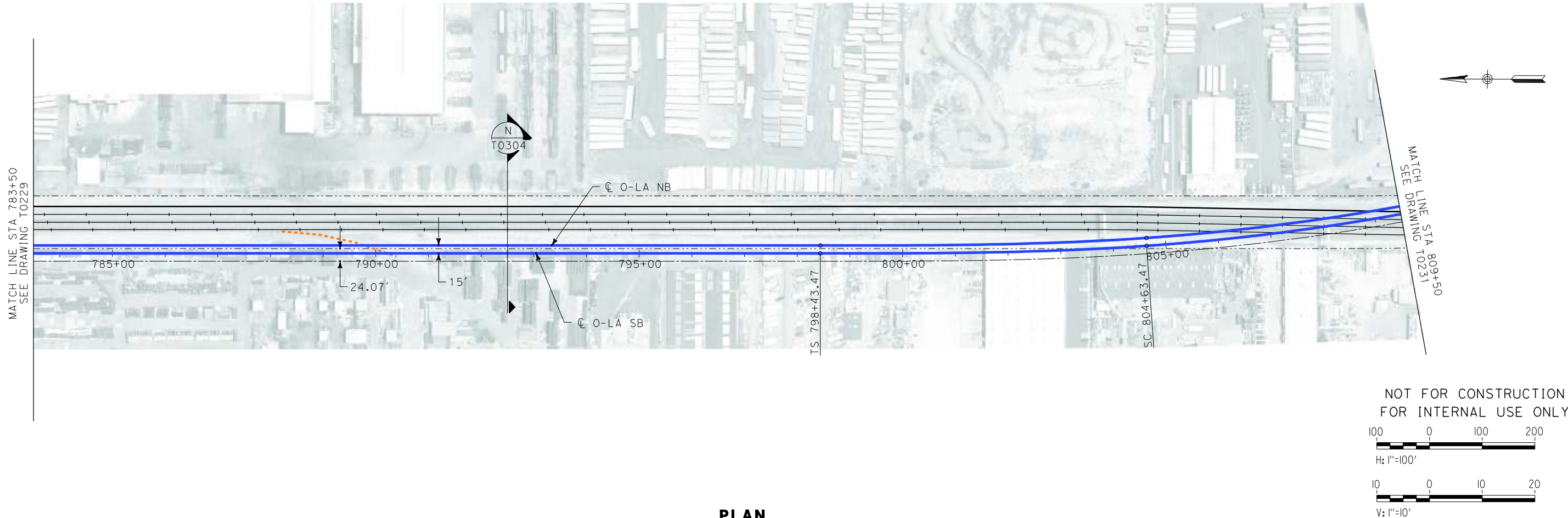
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SHEET NO. 50 OF 245

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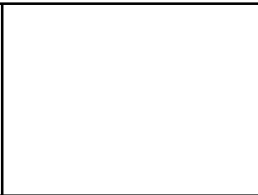


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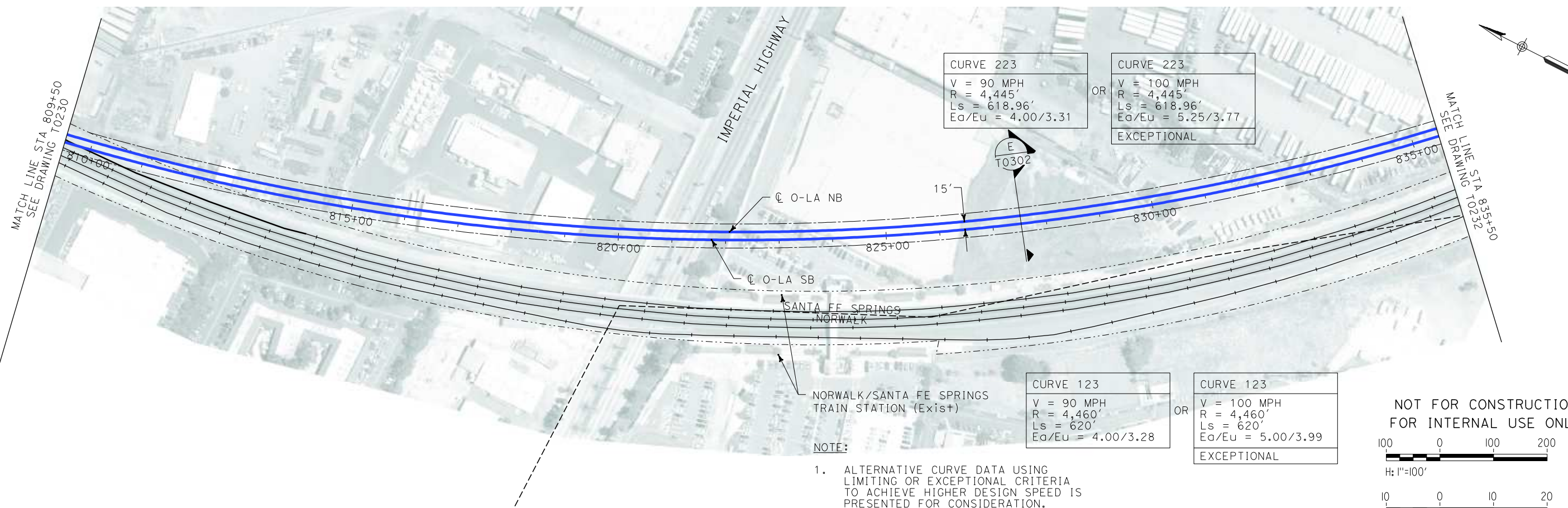




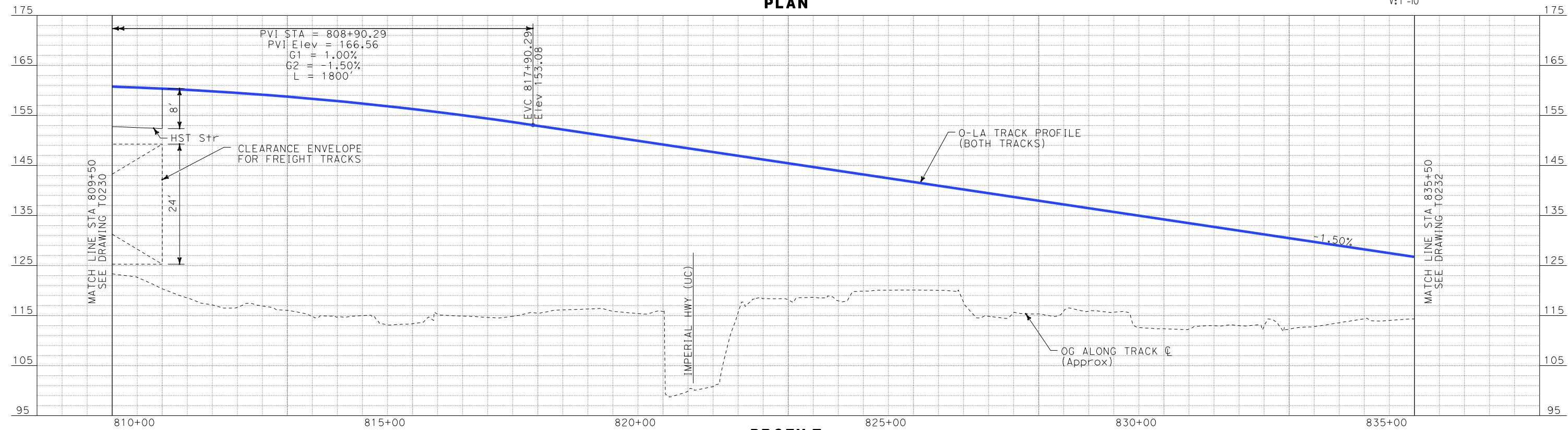
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DRAWING NO.
T0230
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51 OF 245



PLAN




PROFILE

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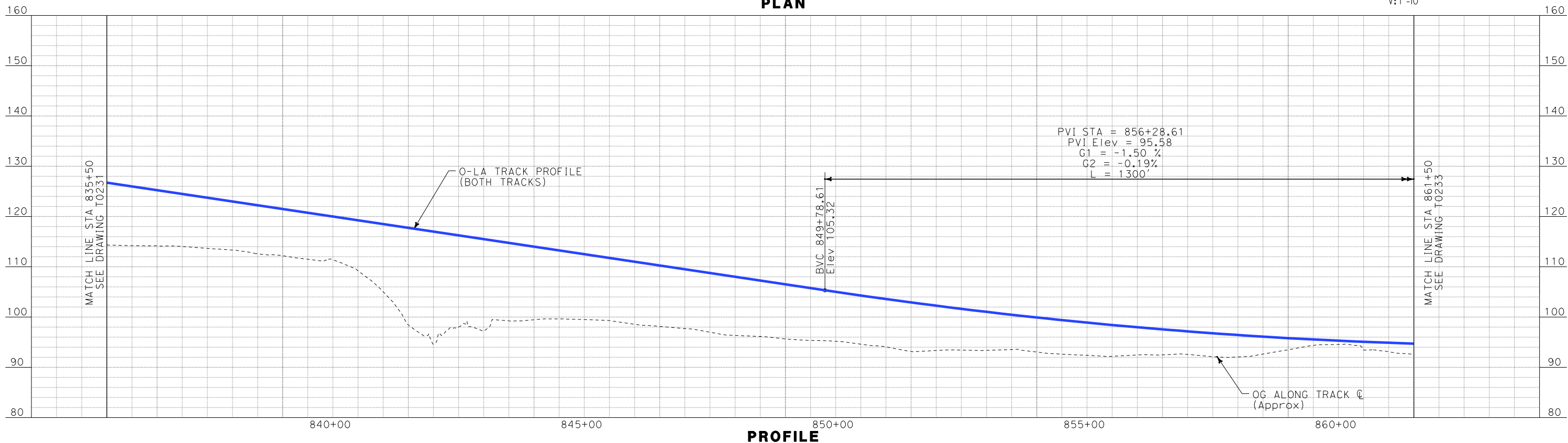
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52 OF 245

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
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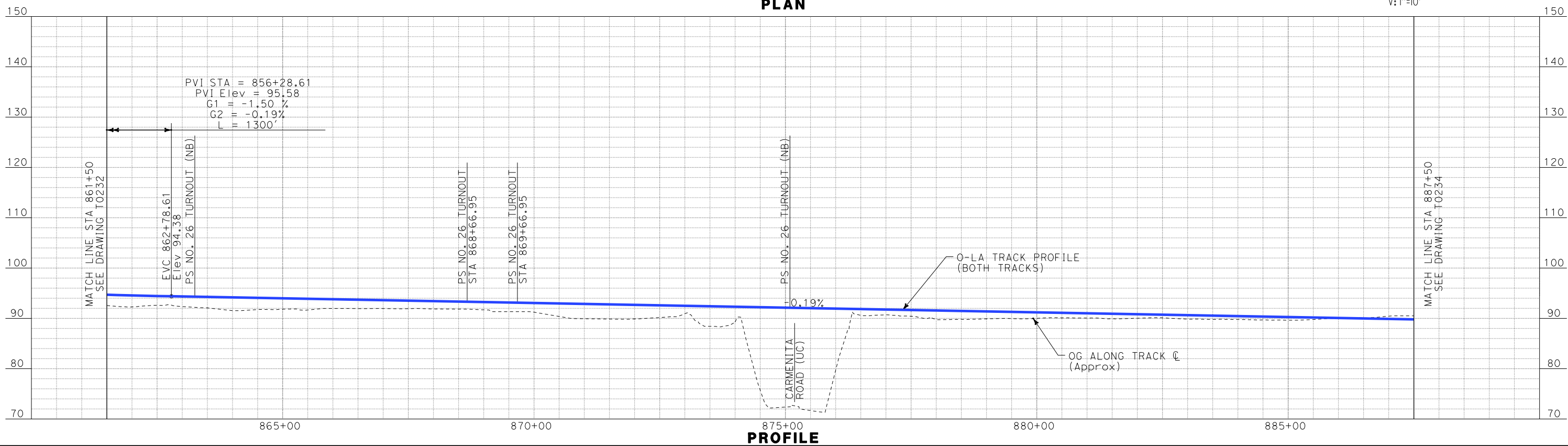
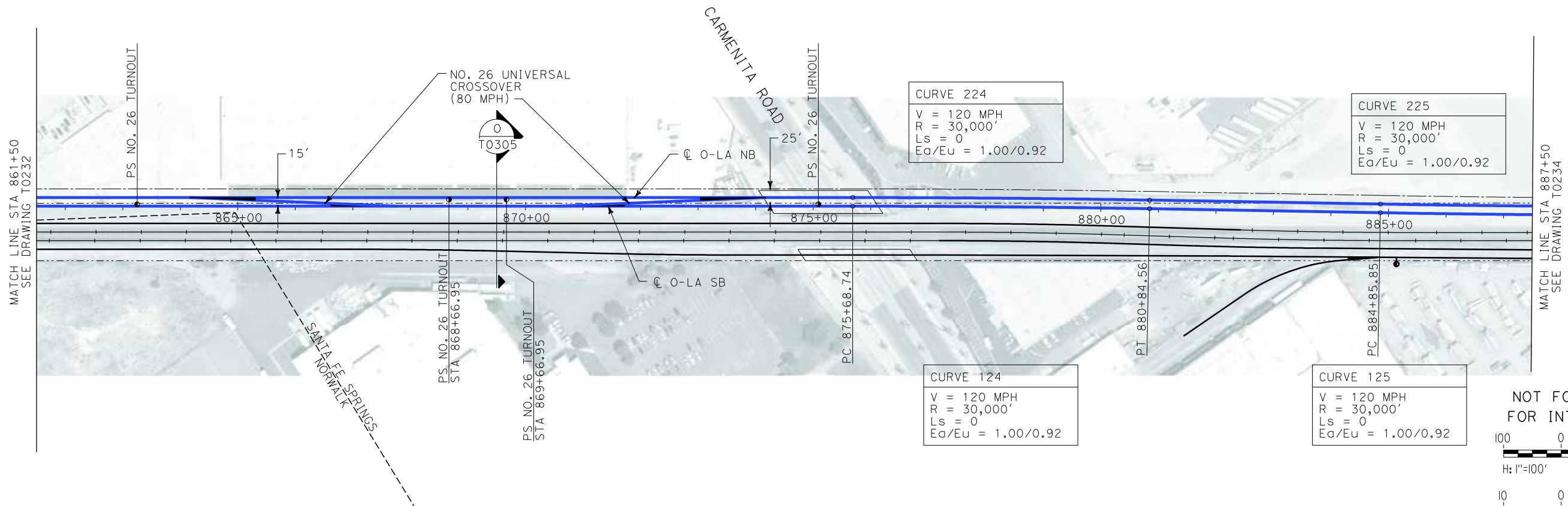
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
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53 OF 245

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IN CHARGE D. THOMSON
DATE 02-13-09

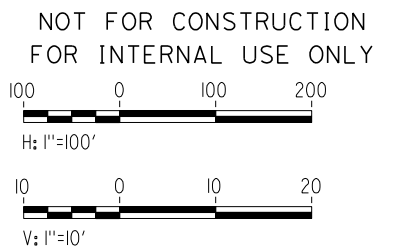
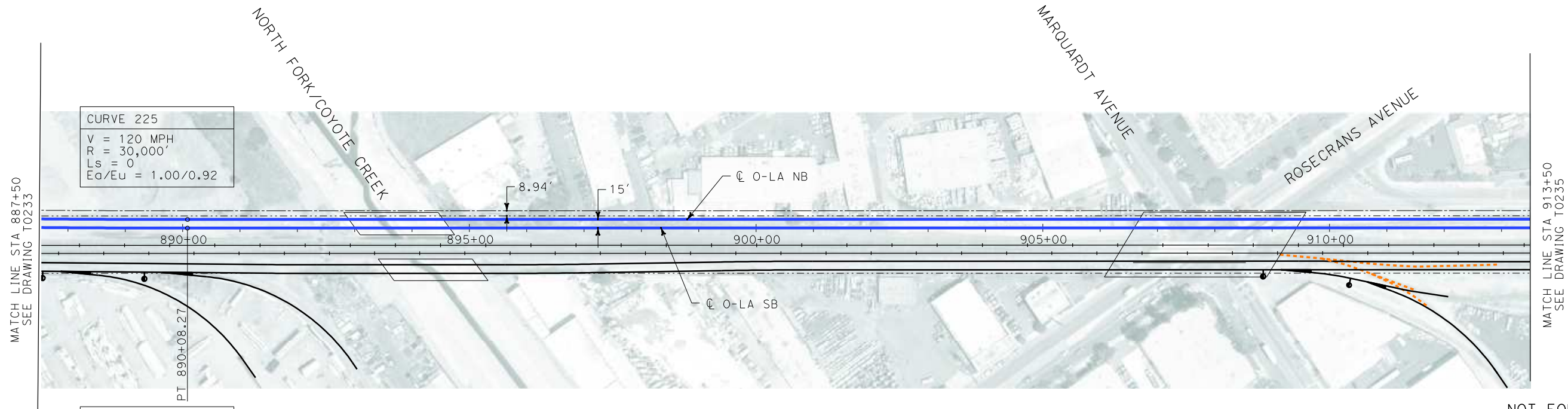
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

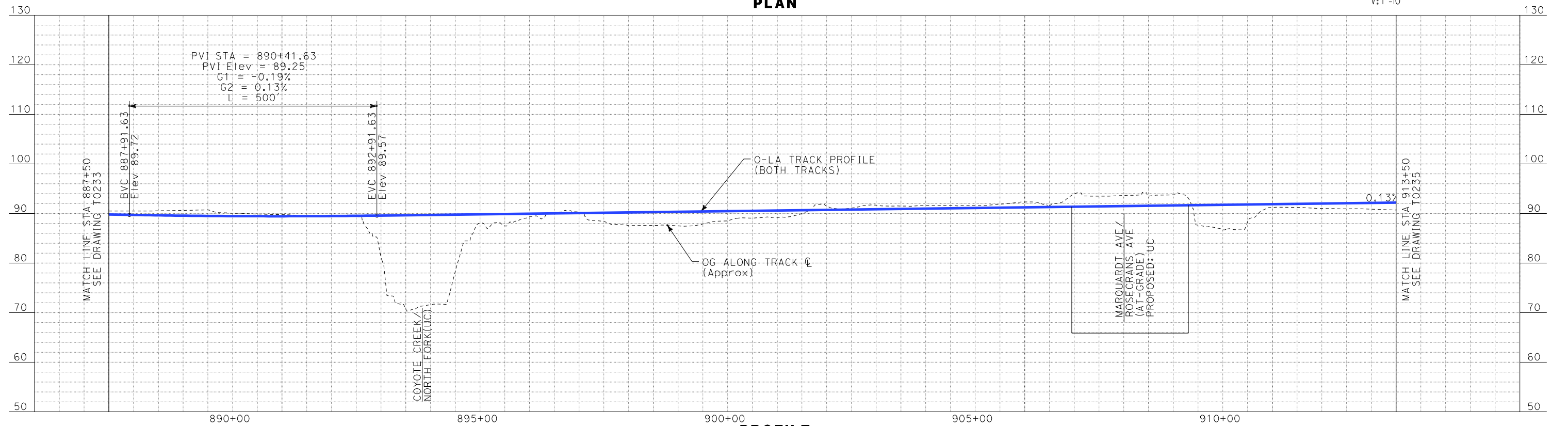
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0233
SCALE AS SHOWN
SHEET NO. 54 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry




PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



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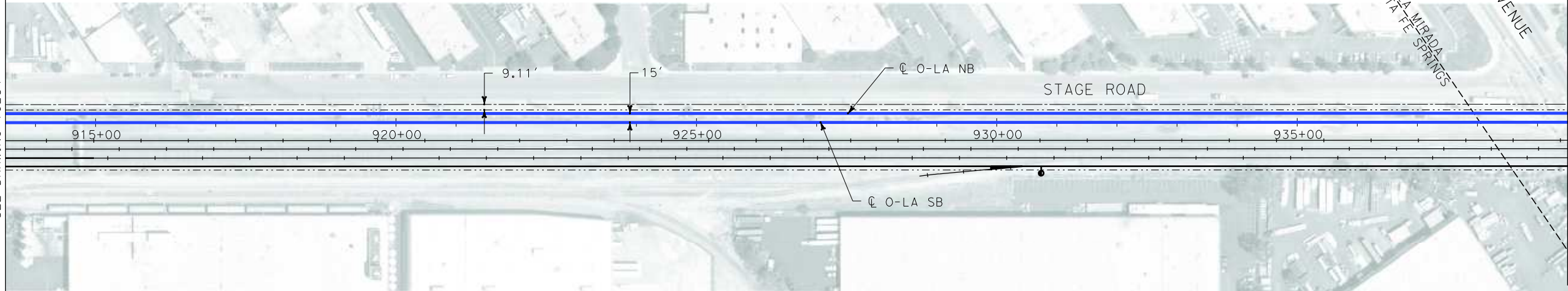
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0234
SCALE
AS SHOWN
SHEET NO.
55 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km

MATCH LINE STA 913+50
SEE DRAWING T0234



MATCH LINE STA 939+50
SEE DRAWING T0236

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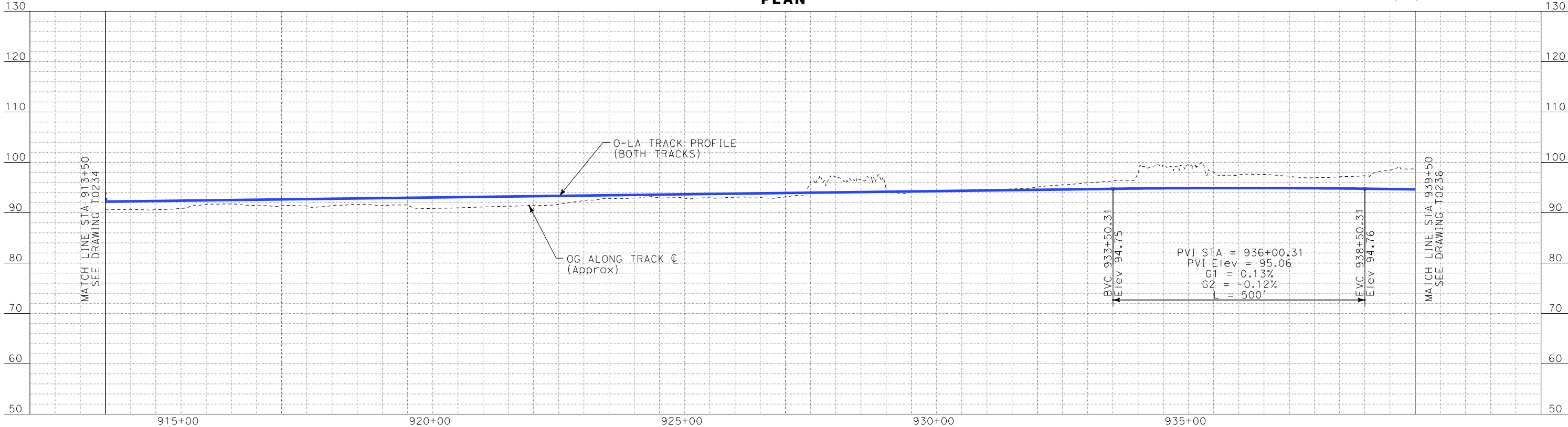
100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'

PLAN



PROFILE

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

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ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT

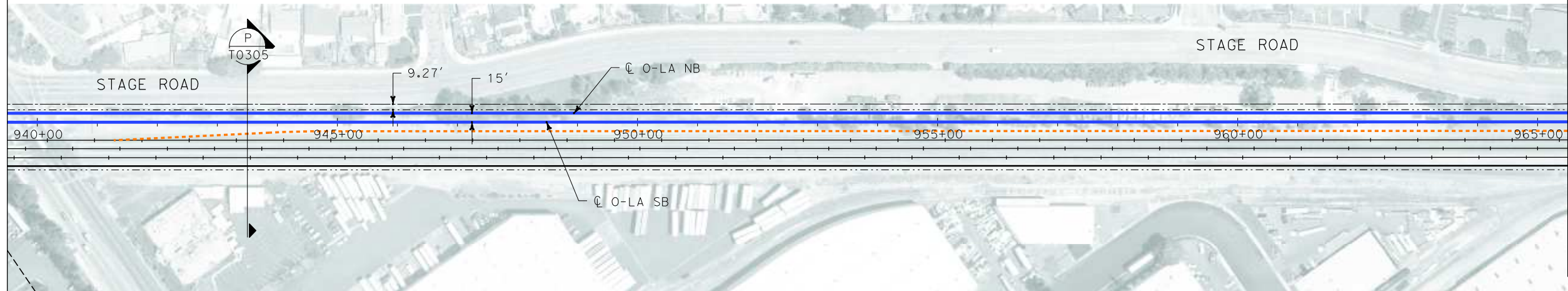
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0235
SCALE
AS SHOWN
SHEET NO.
56 OF 245

REV	DATE	BY	SUB	APP	DESCRIPTION

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\$\$\$\$\$SYTIME\$\$\$\$\$

MATCH LINE STA 939+50
SEE DRAWING T0235



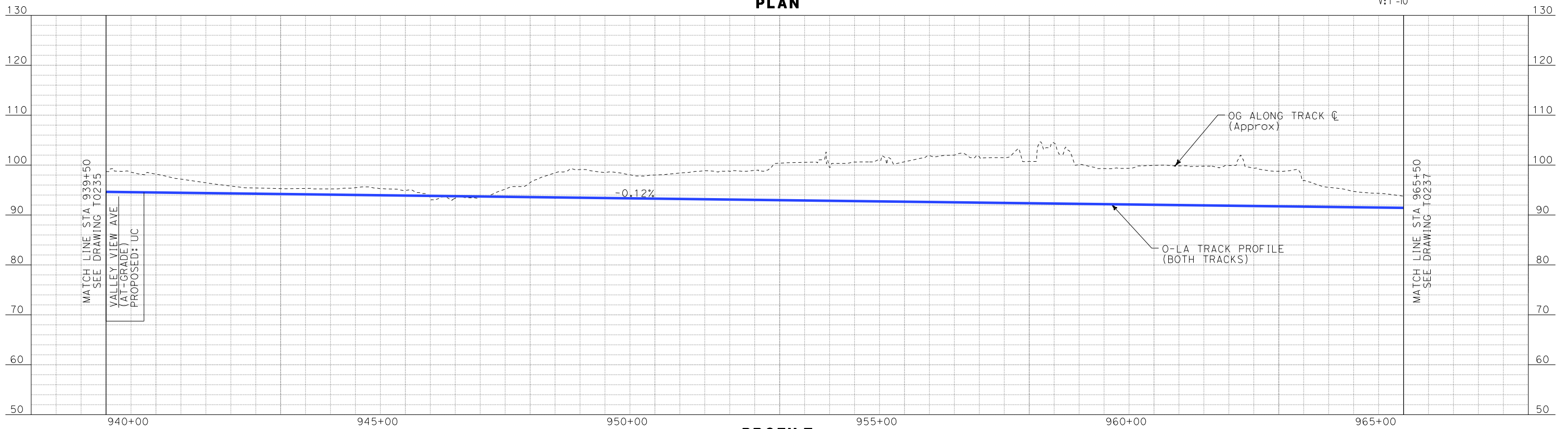
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100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0236
SCALE
AS SHOWN
SHEET NO.
57 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$

MATCH LINE STA 965+50
SEE DRAWING T0236

LA MIRADA CREEK

9.44'
15'
CL O-LA NB
CL O-LA SB

STAGE ROAD

970+00 975+00 980+00 985+00 990+00

MATCH LINE STA 991+50
SEE DRAWING T0238

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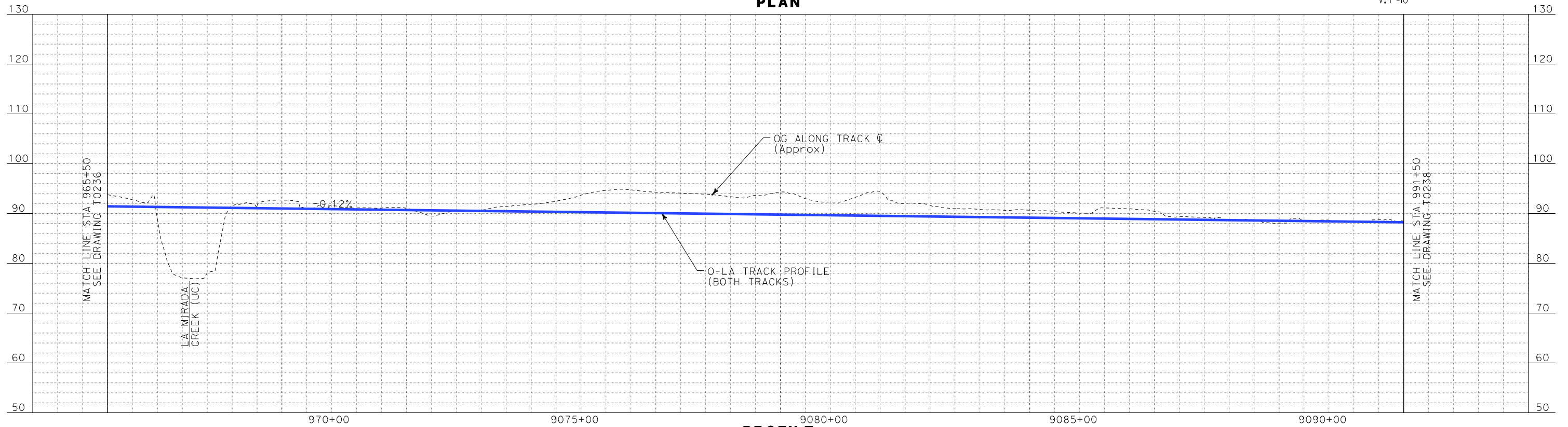
100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'

PLAN



PROFILE

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

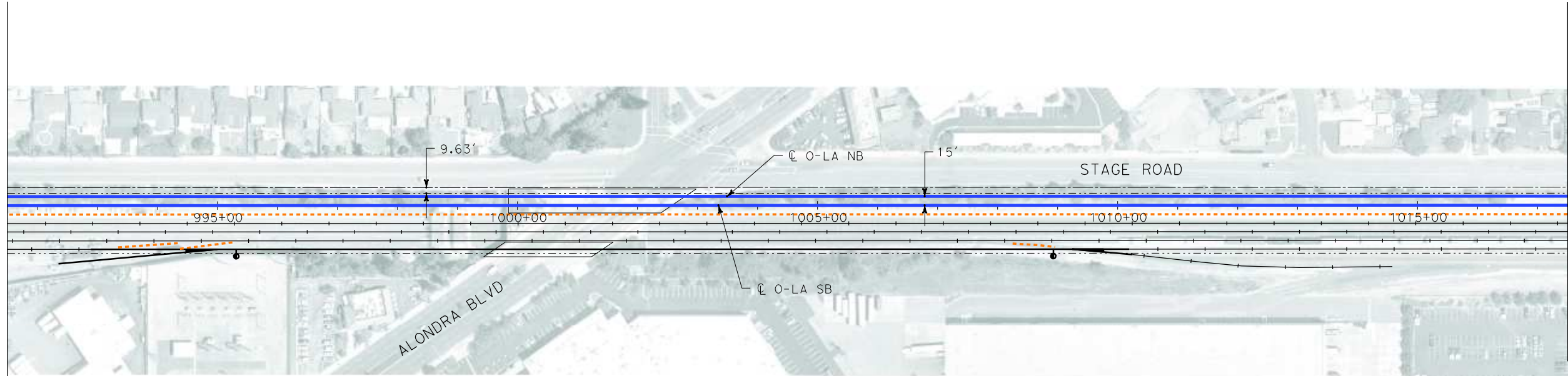
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0237
SCALE AS SHOWN
SHEET NO. 58 OF 245

REV	DATE	BY	SUB	APP	DESCRIPTION

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k

MATCH LINE STA 991+50
SEE DRAWING T0237



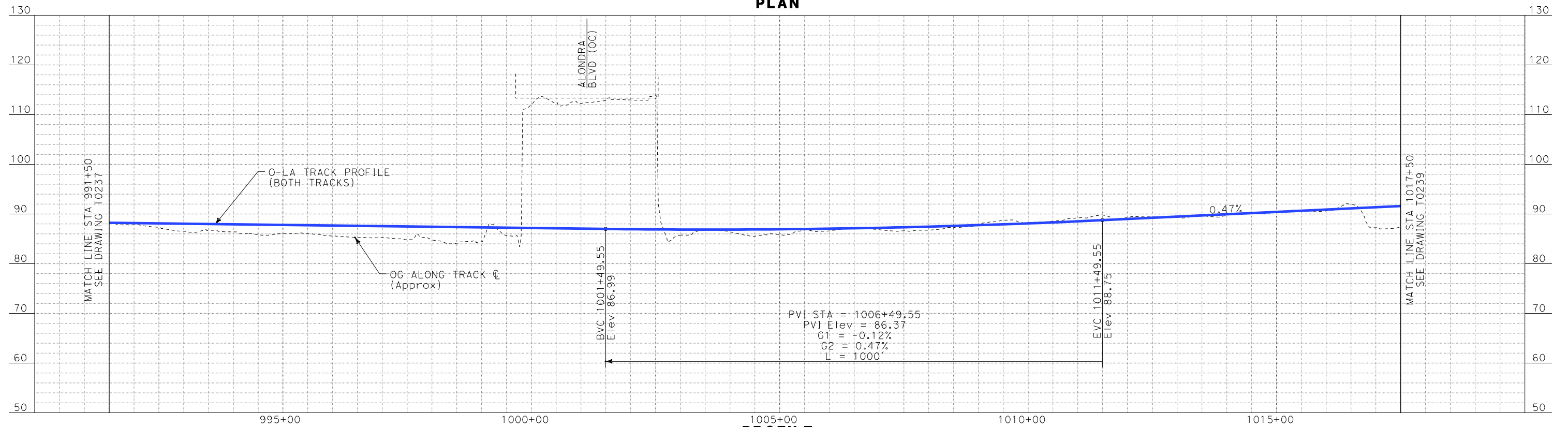
MATCH LINE STA 1017+50
SEE DRAWING T0239

NOT FOR CONSTRUCTION
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H: 1"=100'

V: 1"=10'

PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

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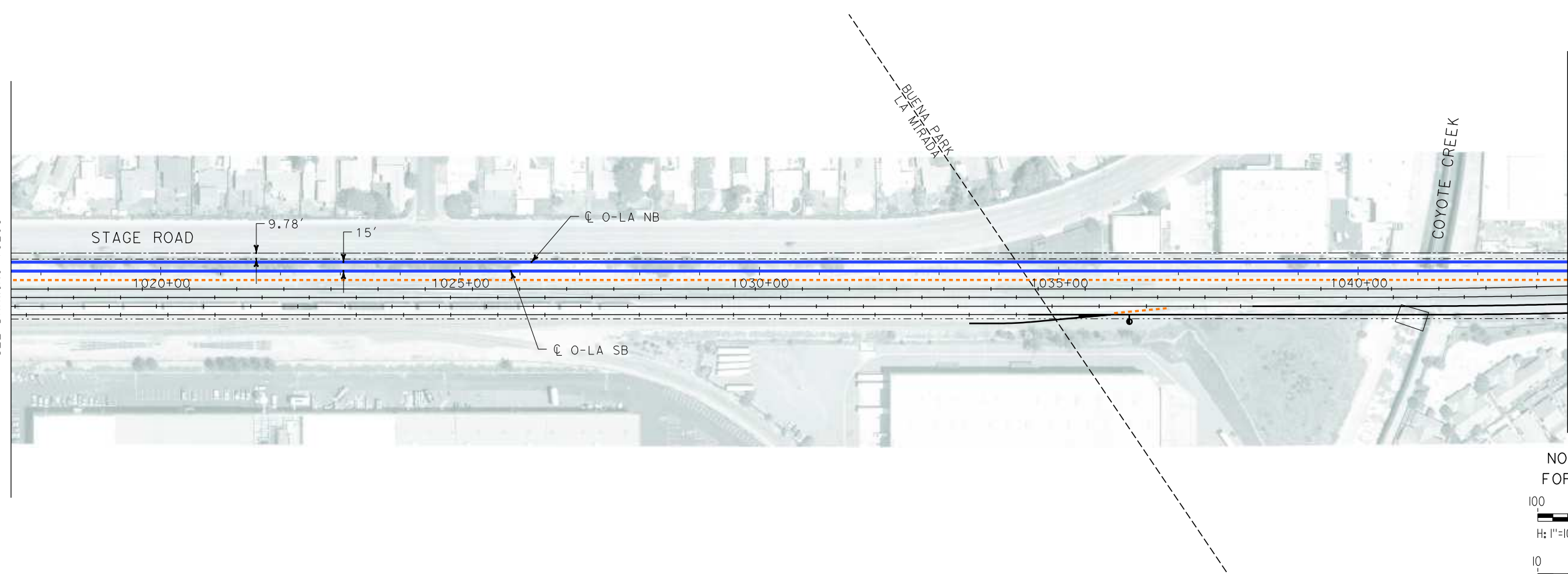


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

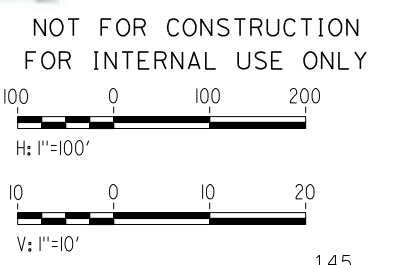
CONTRACT NO. HSR06-0005
DRAWING NO. T0238
SCALE AS SHOWN
SHEET NO. 59 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km

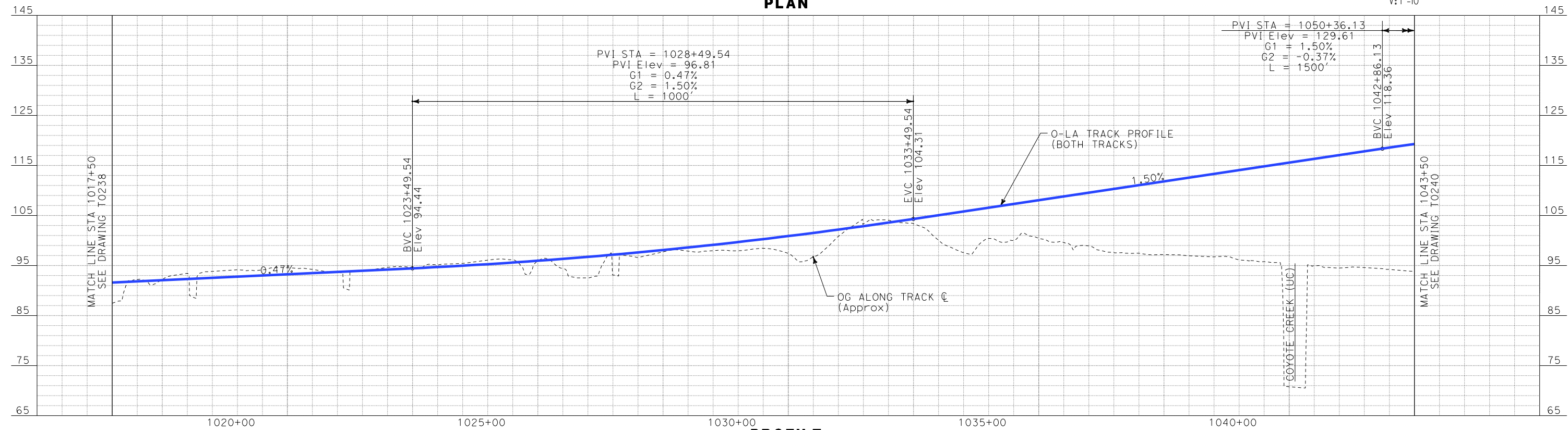
MATCH LINE STA 1017+50
SEE DRAWING T0238



MATCH LINE STA 1043+50
SEE DRAWING T0240




PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
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IN CHARGE
D. THOMSON
DATE
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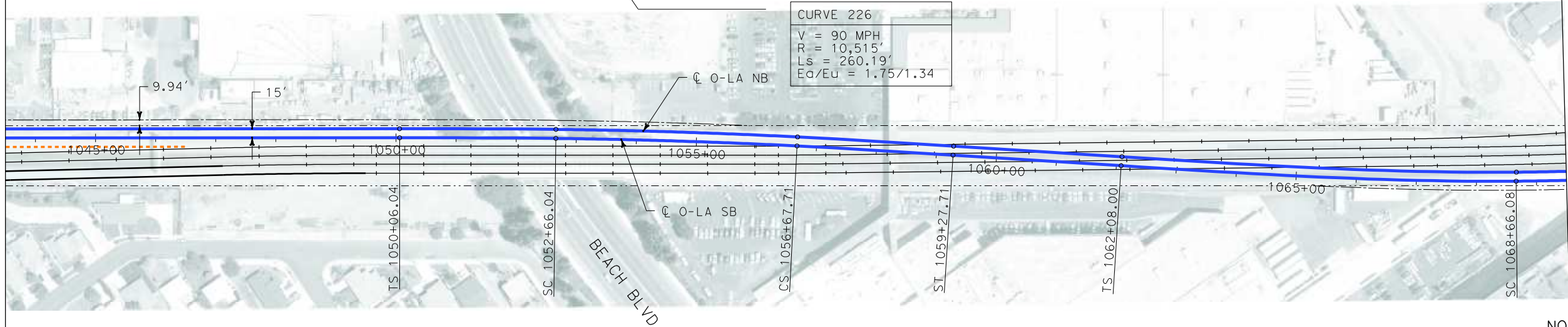
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CONTRACT NO. HSR06-0005
DRAWING NO. T0239
SCALE AS SHOWN
SHEET NO. 60 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

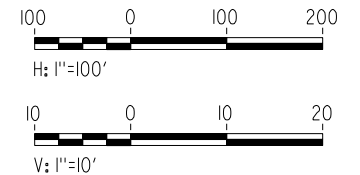
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SEE DRAWING T0239



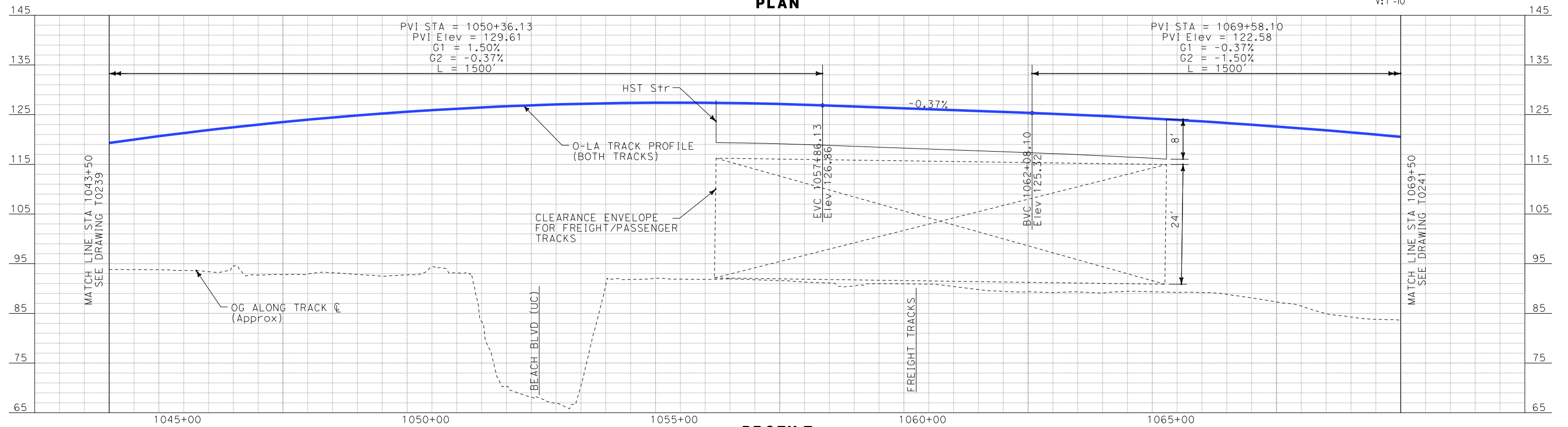
CURVE 126
V = 90 MPH
R = 10,500'
Ls = 260'
Ea/Eu = 1.75/1.34

CURVE 226
V = 90 MPH
R = 10,515'
Ls = 260.19'
Ea/Eu = 1.75/1.34

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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

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K. SPERRY
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IN CHARGE
D. THOMSON
DATE
02-13-09

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PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0240
SCALE
AS SHOWN
SHEET NO.
61 OF 245

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Sperry

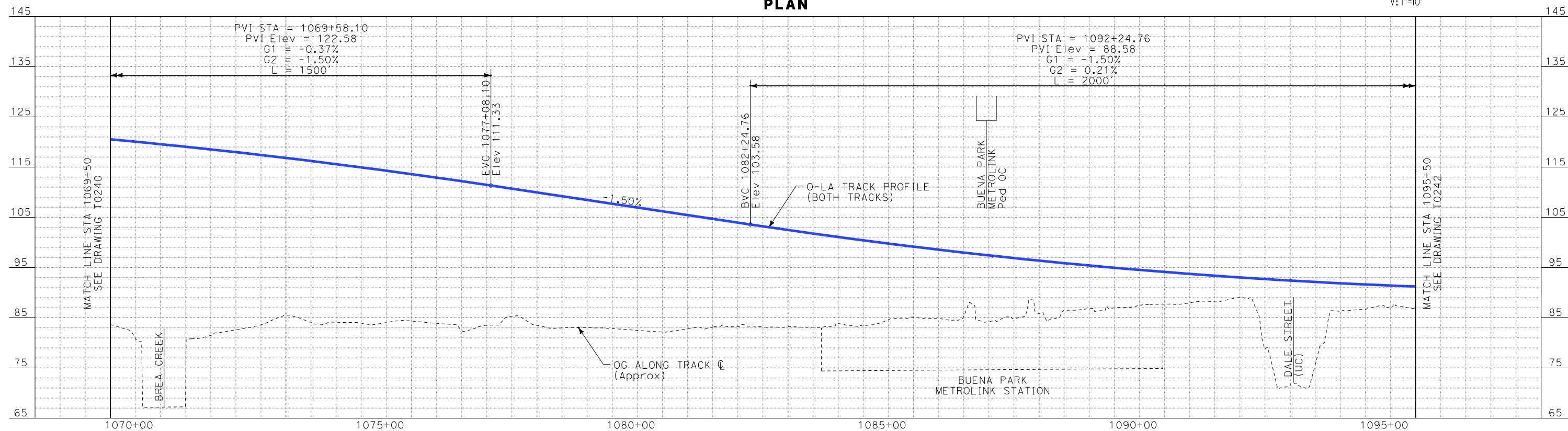


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100 0 100 200
H: 1"=100'

10 0 10 20
V: 1"=10'

PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

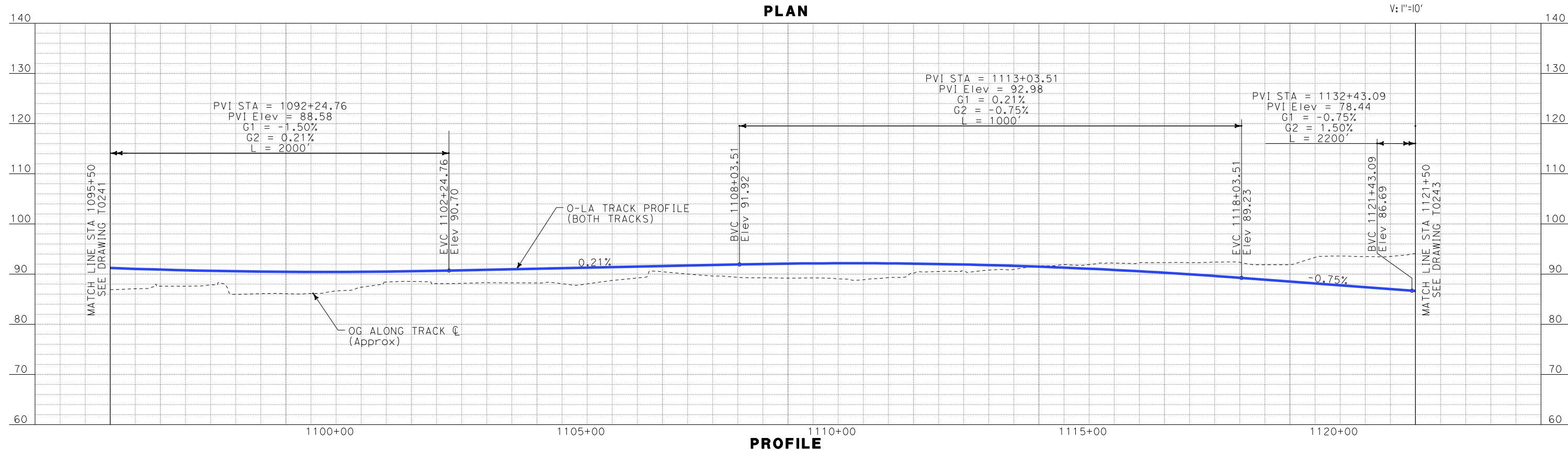
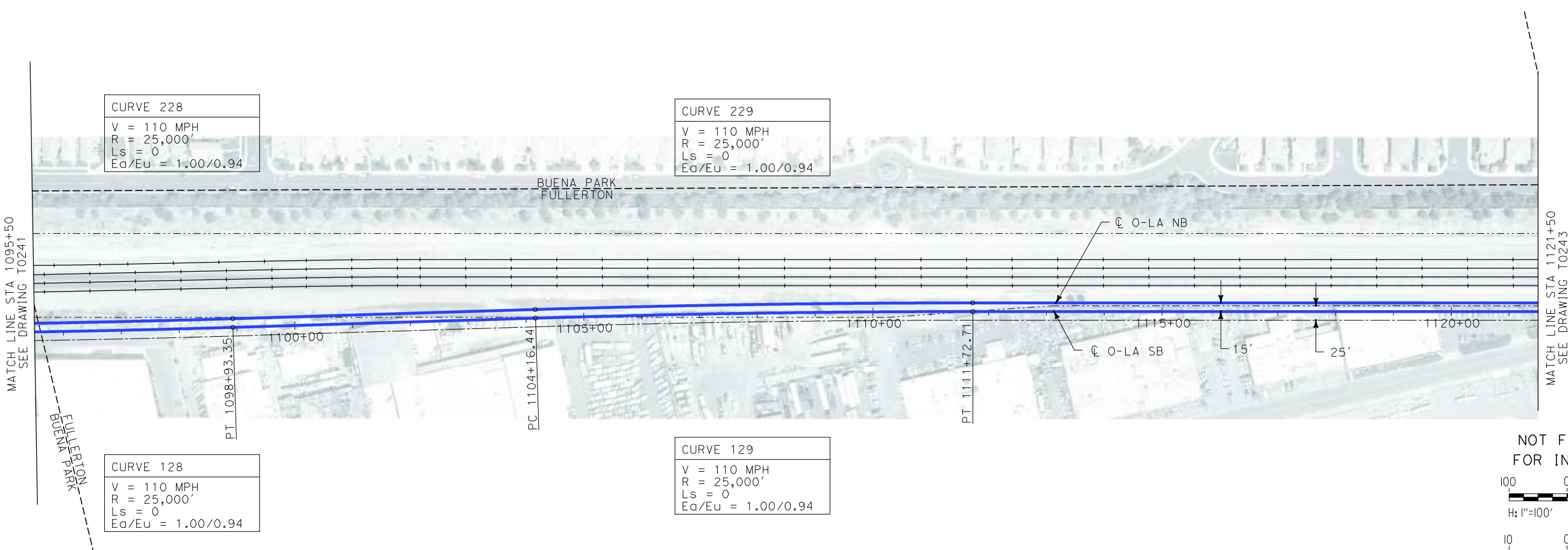
DESIGNED BY K. SPERRY
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PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0241
SCALE AS SHOWN
SHEET NO. 62 OF 245



SHEET NO. _____ OF _____ DATE _____						
REV	DATE	BY	SUB	APP	DESCRIPTION	



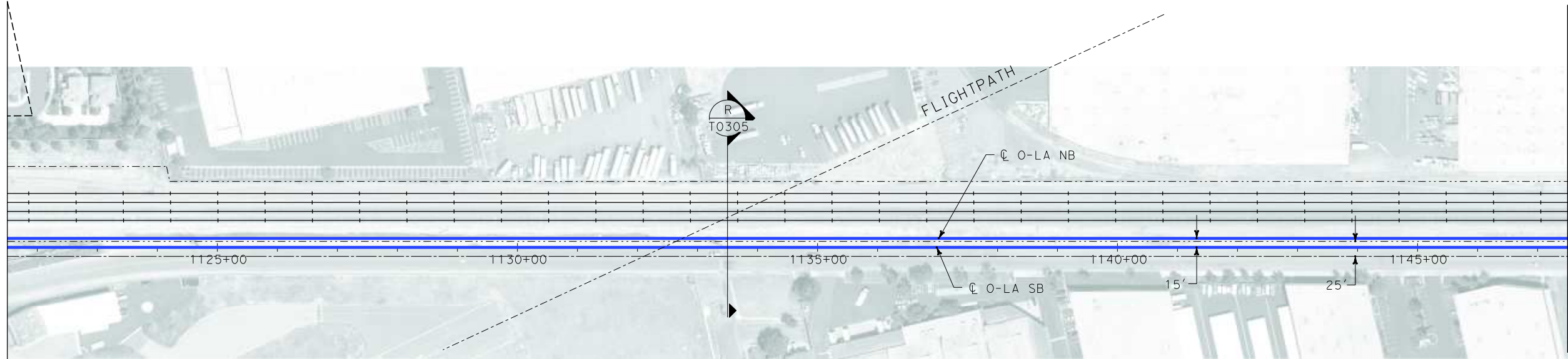
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO.	HSR06-0005
DRAWING NO.	T0242
SCALE	AS SHOWN
SHEET NO.	63 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k

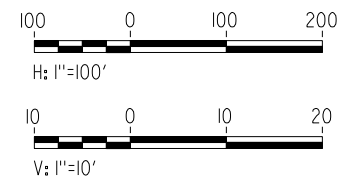
MATCH LINE STA 1121+50
SEE DRAWING T0242



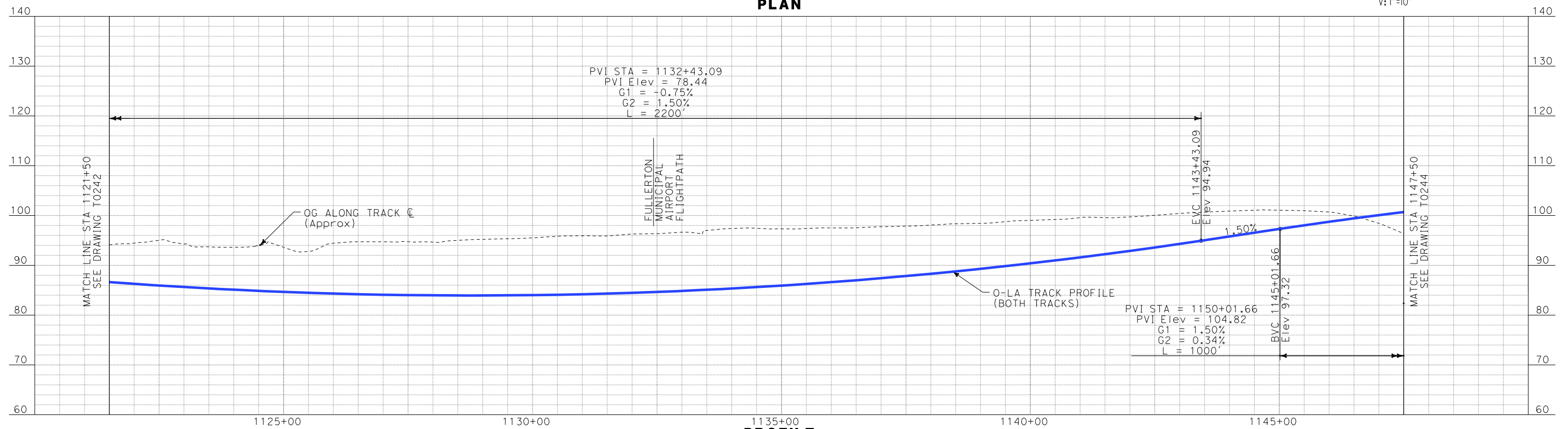
MATCH LINE STA 1147+50
SEE DRAWING T0244

FULLERTON AIRPORT

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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

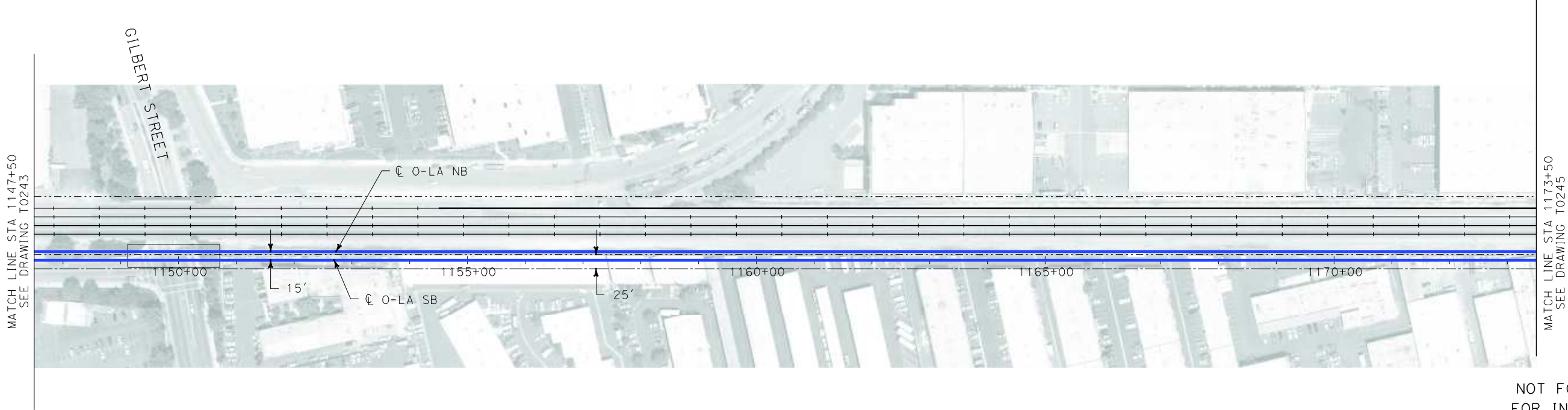
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LOS ANGELES, CA 90017-2556

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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0243
SCALE AS SHOWN
SHEET NO. 64 OF 245

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Sperry.km
\$\$\$\$\$SYTIME\$\$\$\$\$



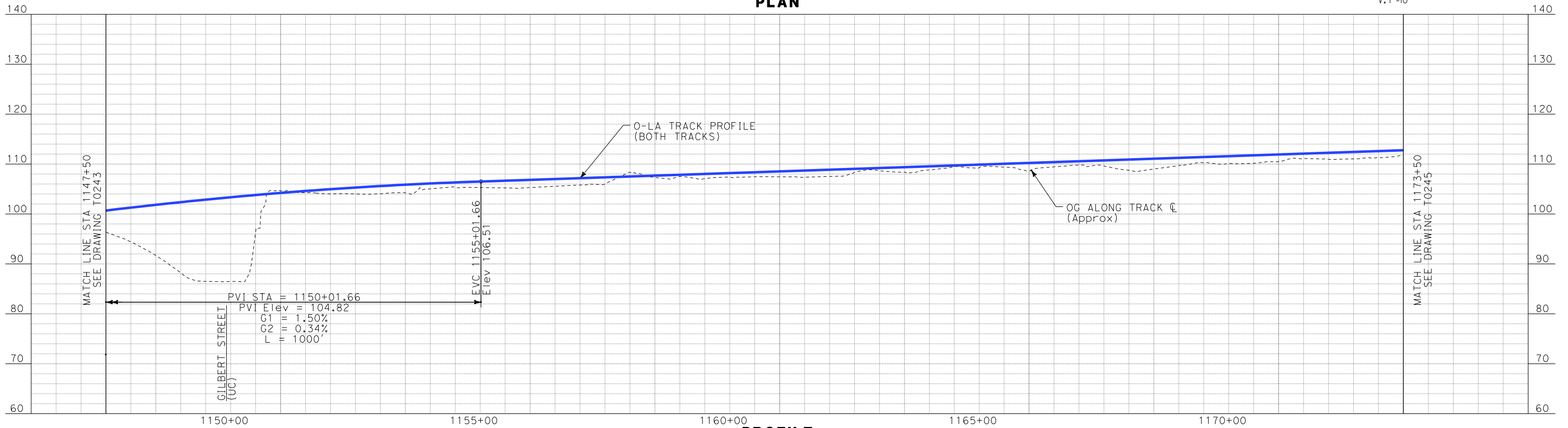
NOT FOR CONSTRUCTION
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100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
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DRAWN BY
K. SPERRY
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IN CHARGE
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DATE
02-13-09



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CALIFORNIA HIGH SPEED RAIL AUTHORITY

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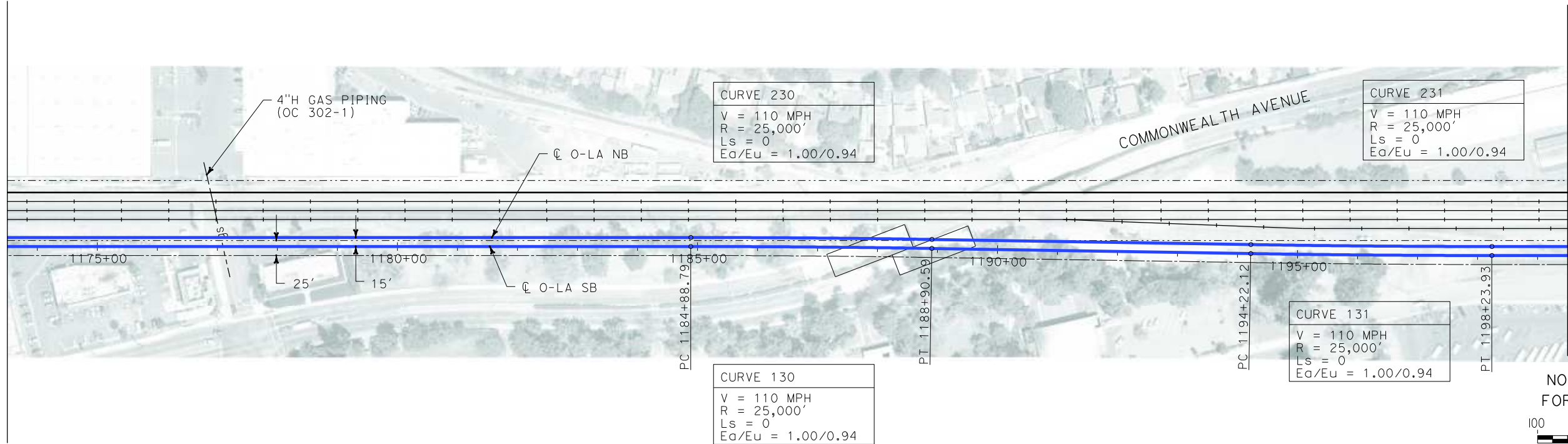
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT

PLAN AND PROFILE

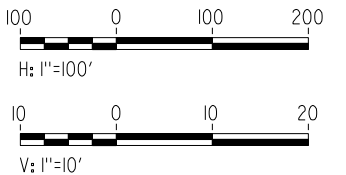
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HSR06-0005
DRAWING NO.
T0244
SCALE
AS SHOWN
SHEET NO.
65 OF 245

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Sperry\km
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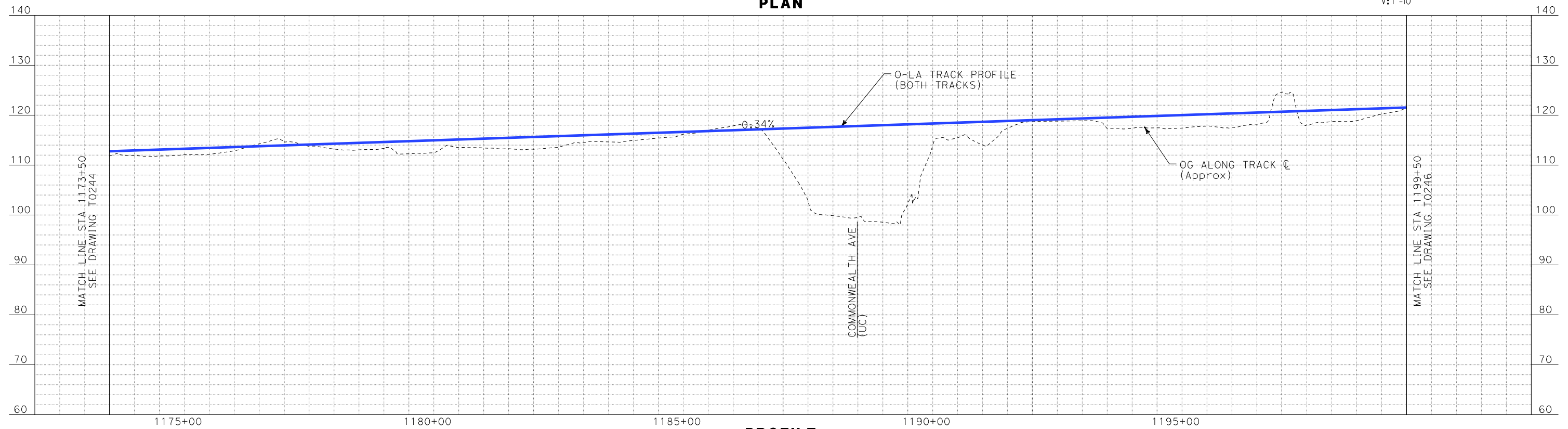
MATCH LINE STA 1173+50
SEE DRAWING T0244



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PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

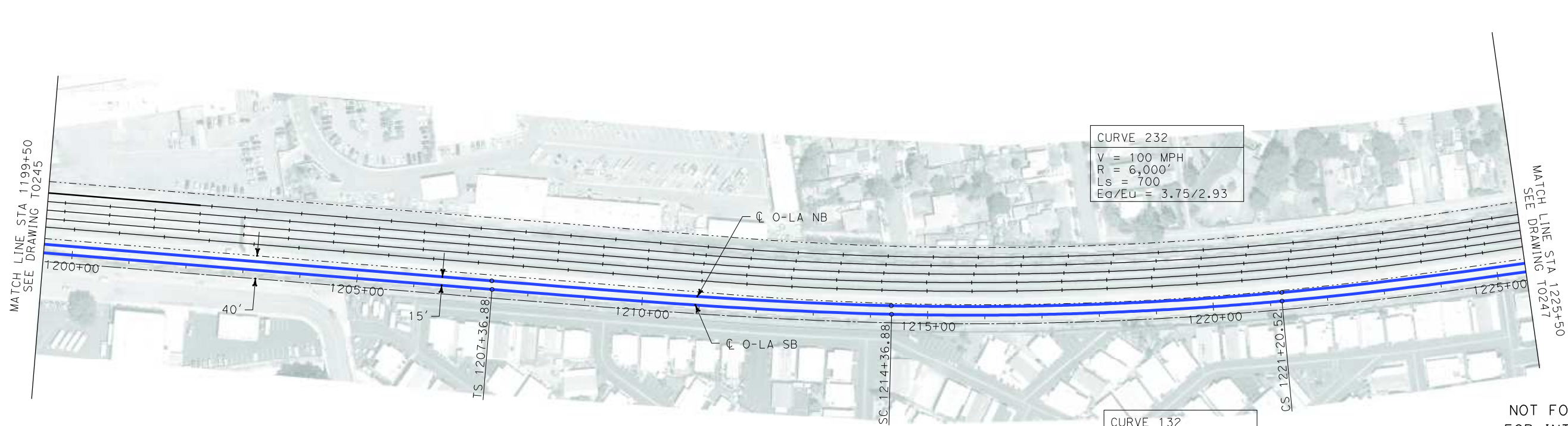
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1055 WEST SEVENTH STREET, SUITE 3150
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0245
SCALE AS SHOWN
SHEET NO. 66 OF 245

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Sperry\km

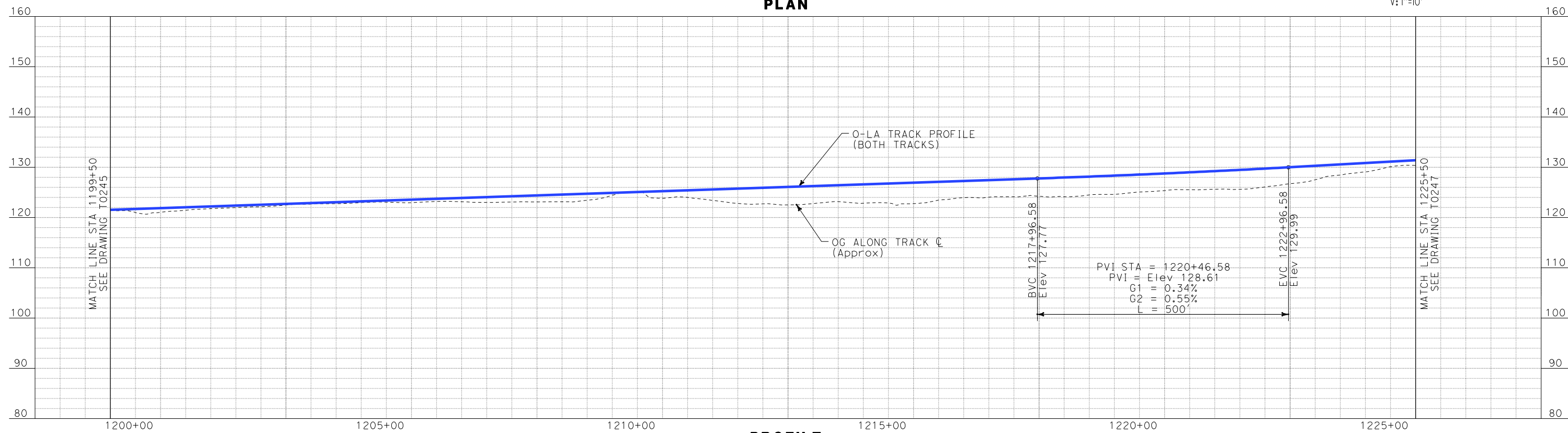


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H: 1"=100'

10 0 10 20
V: 1"=10'


PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

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1055 WEST SEVENTH STREET, SUITE 3150
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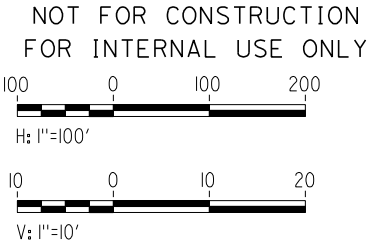
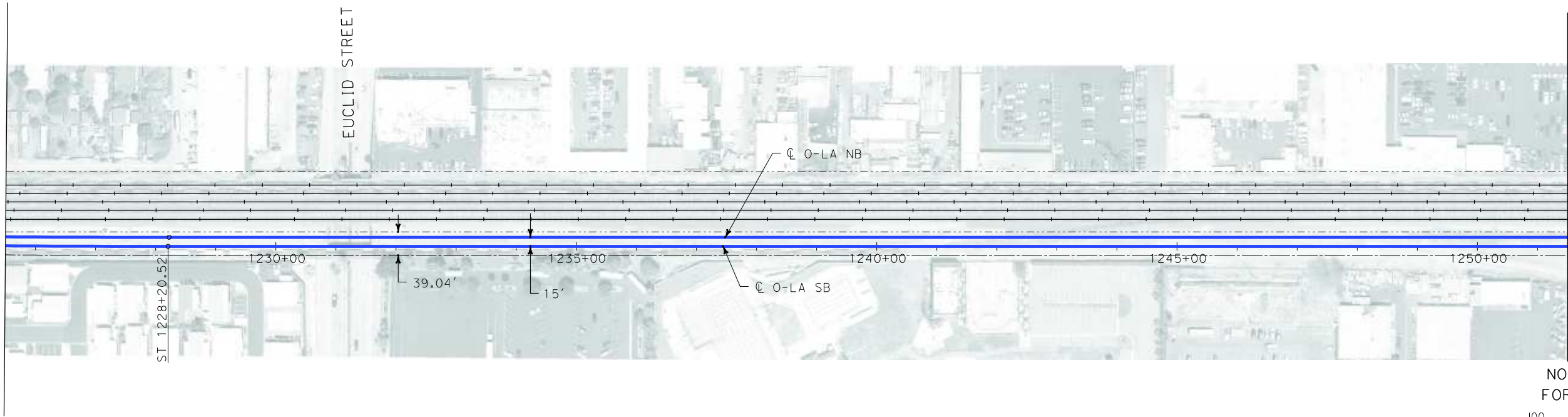
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

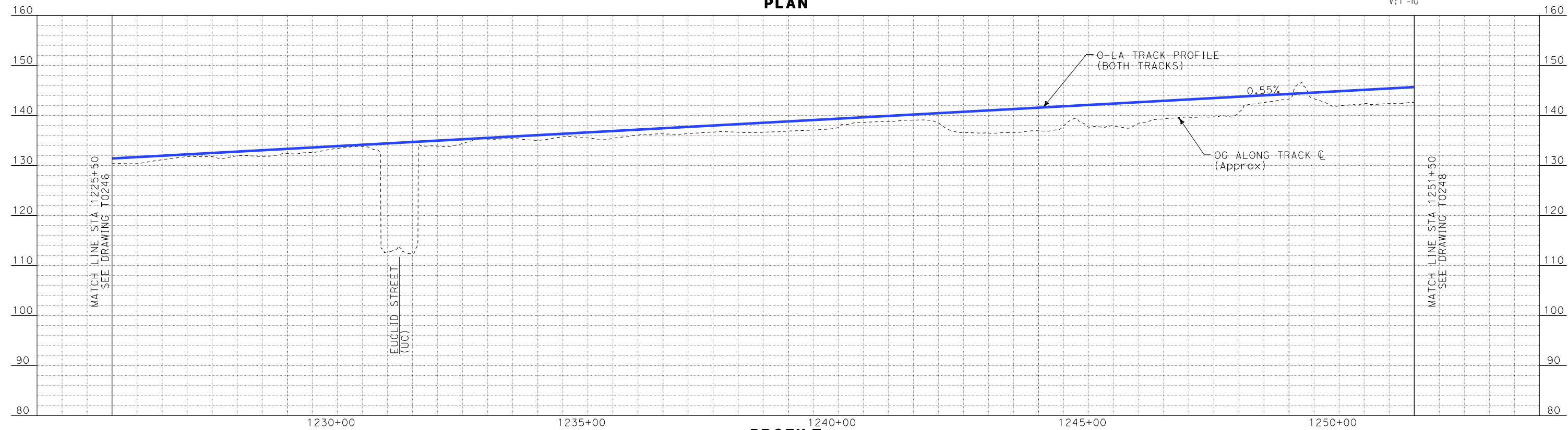
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DRAWING NO. T0246
SCALE AS SHOWN
SHEET NO. 67 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km

MATCH LINE STA 1225+50
SEE DRAWING T0246




PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



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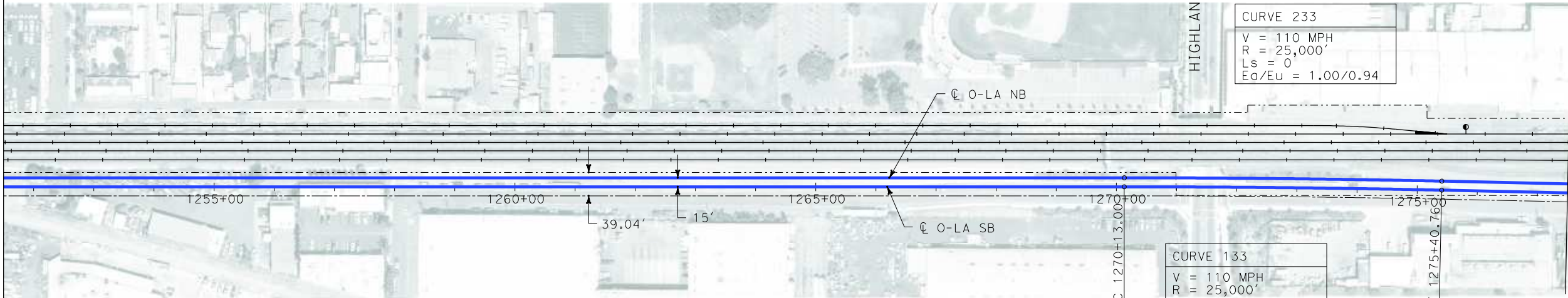
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0247
SCALE AS SHOWN
SHEET NO. 68 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k

MATCH LINE STA 1251+50
SEE DRAWING T0247



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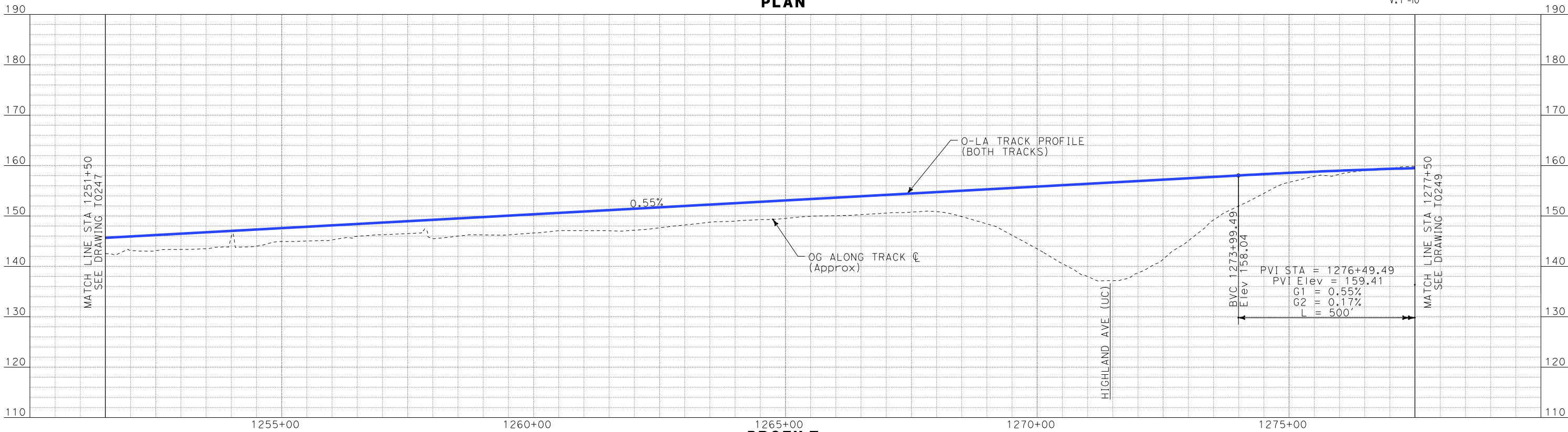
100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'

PLAN



PROFILE

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
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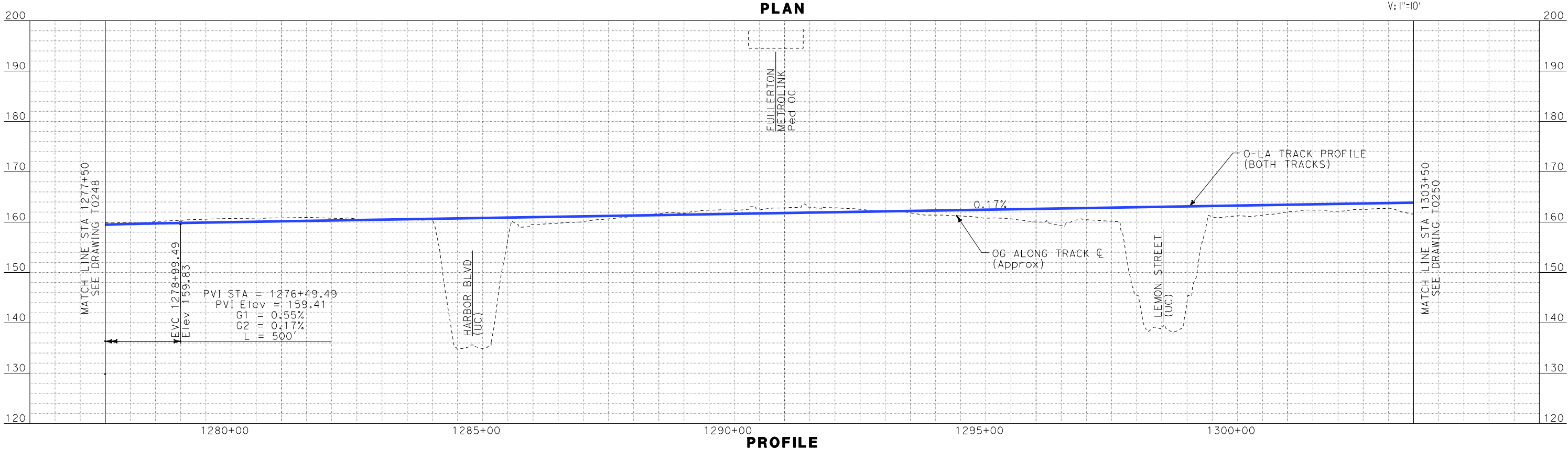
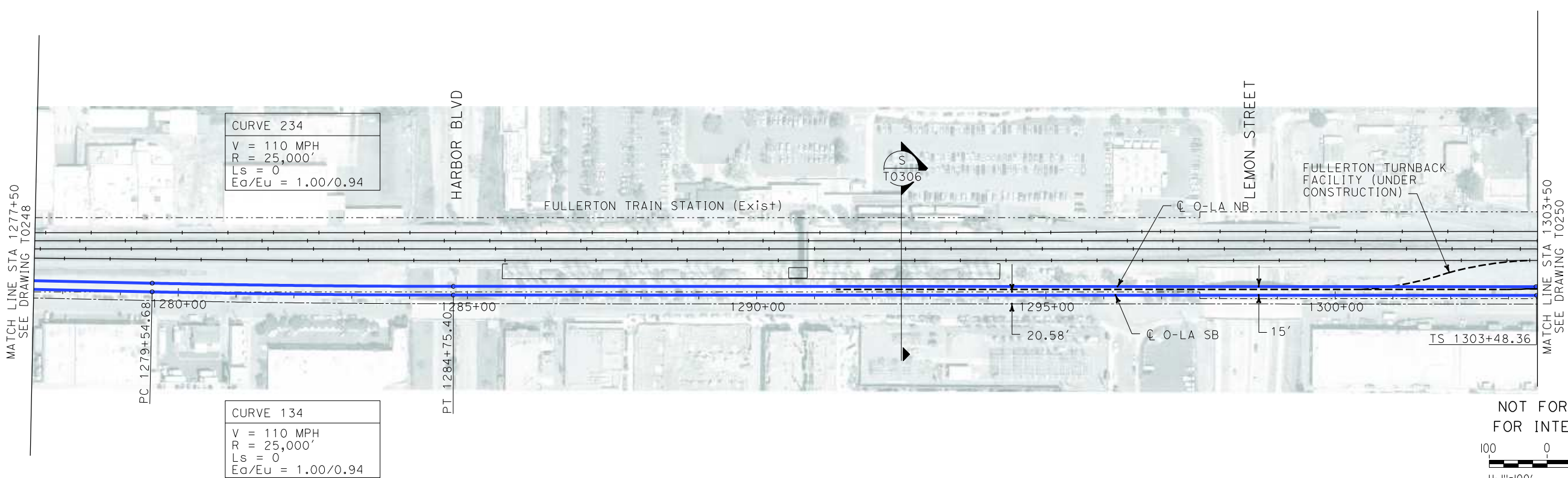
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ORANGE COUNTY TO LOS ANGELES**
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DRAWING NO.
T0248
SCALE
AS SHOWN
SHEET NO.
69 OF 245


REV	DATE	BY	SUB	APP	DESCRIPTION

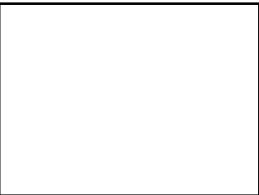


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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

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ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

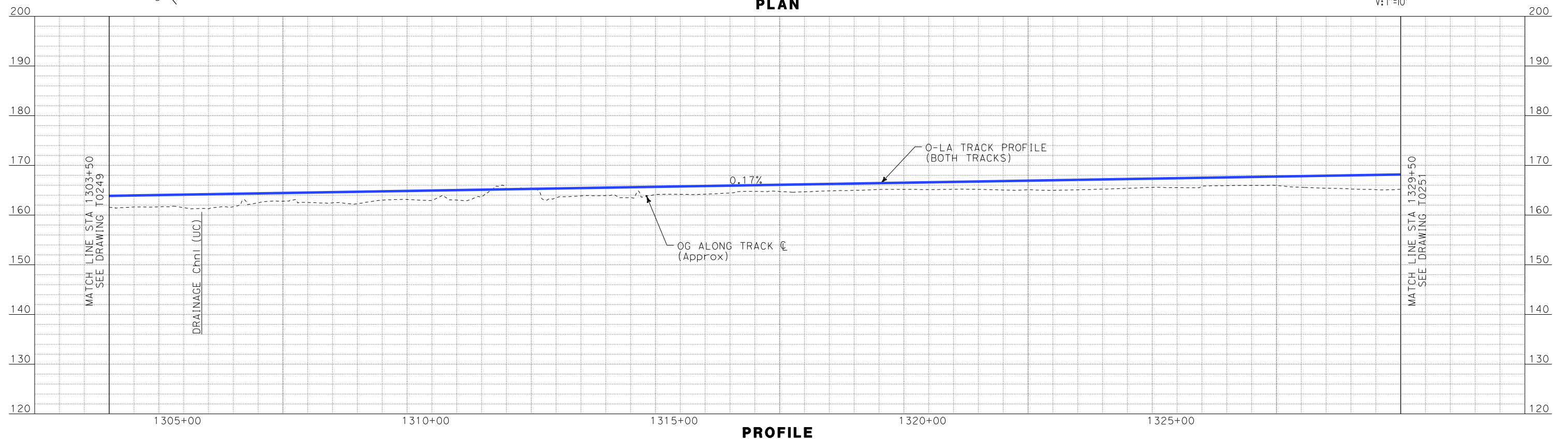
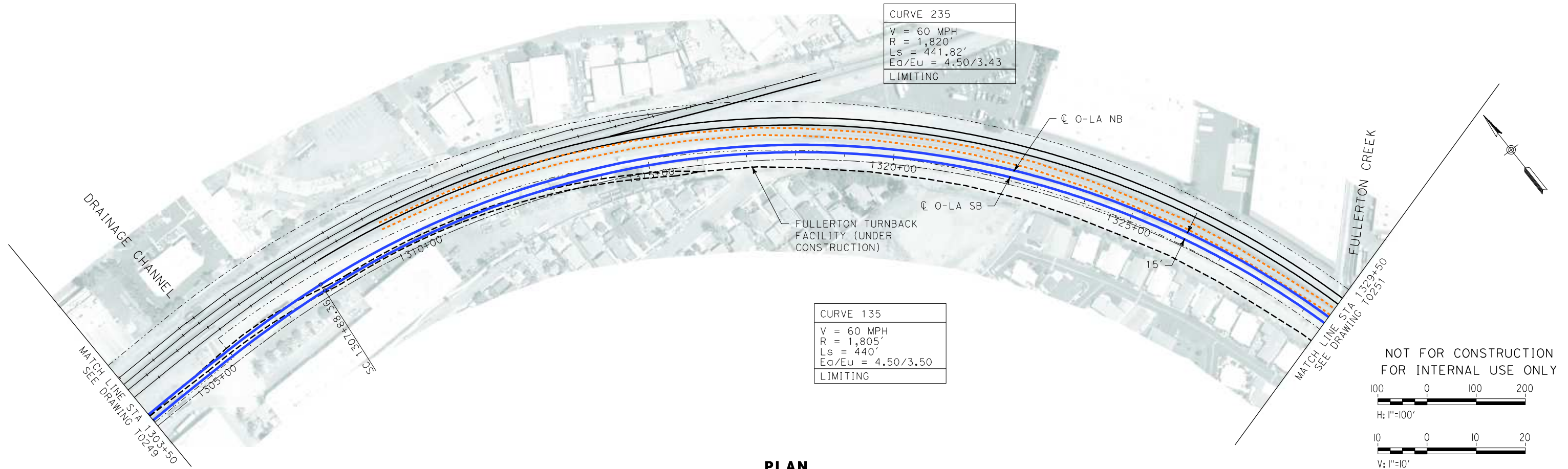


 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

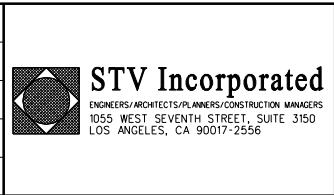
CONTRACT NO. HSR06-0005
DRAWING NO. T0249
SCALE AS SHOWN
SHEET NO. 70 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km



REV	DATE	BY	SUB	APP	DESCRIPTION

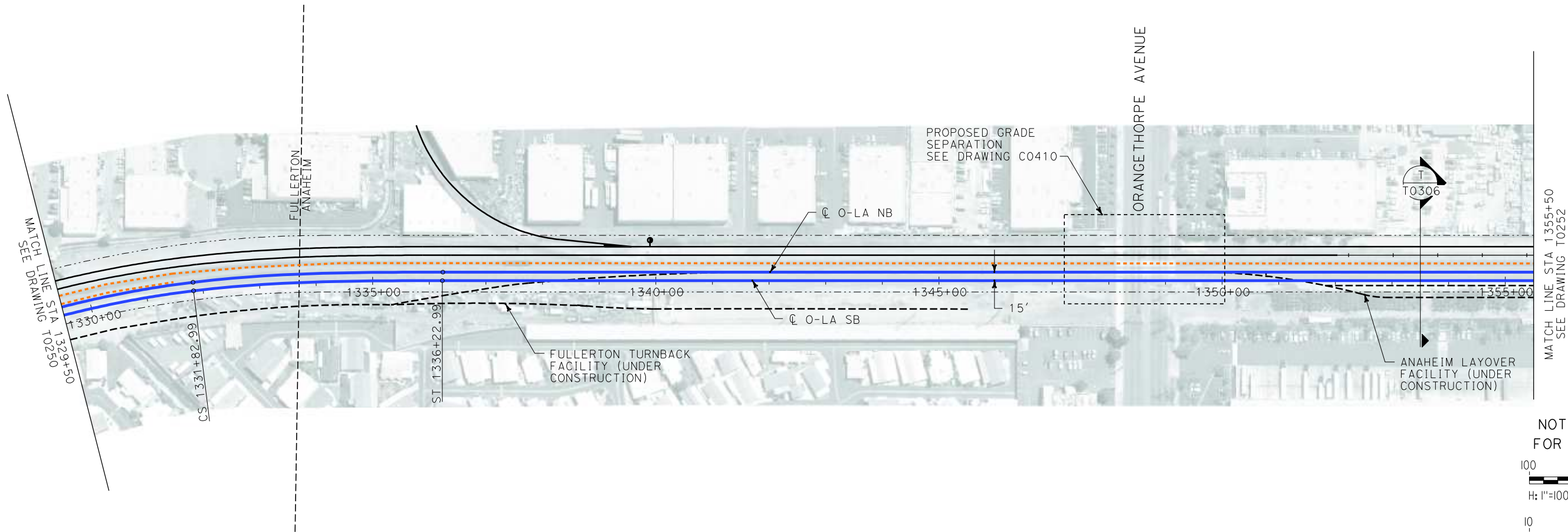
DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0250
SCALE
AS SHOWN
SHEET NO.
71 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$



NOT FOR CONSTRUCTION
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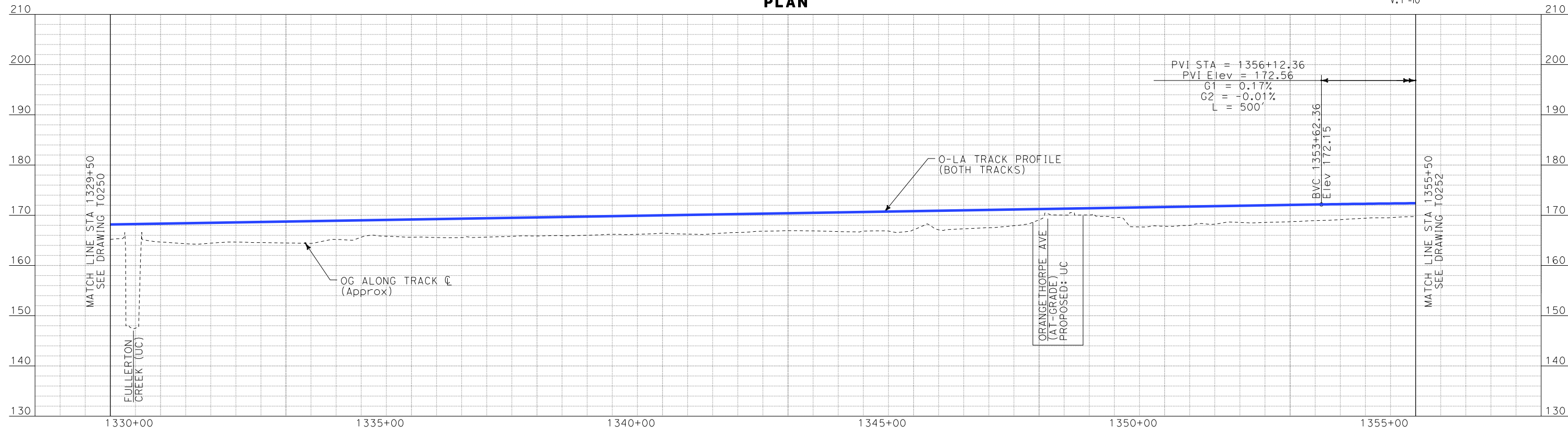
100 0 100 200

H: 1"=100'

10 0 10 20

V: 1"=10'


PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





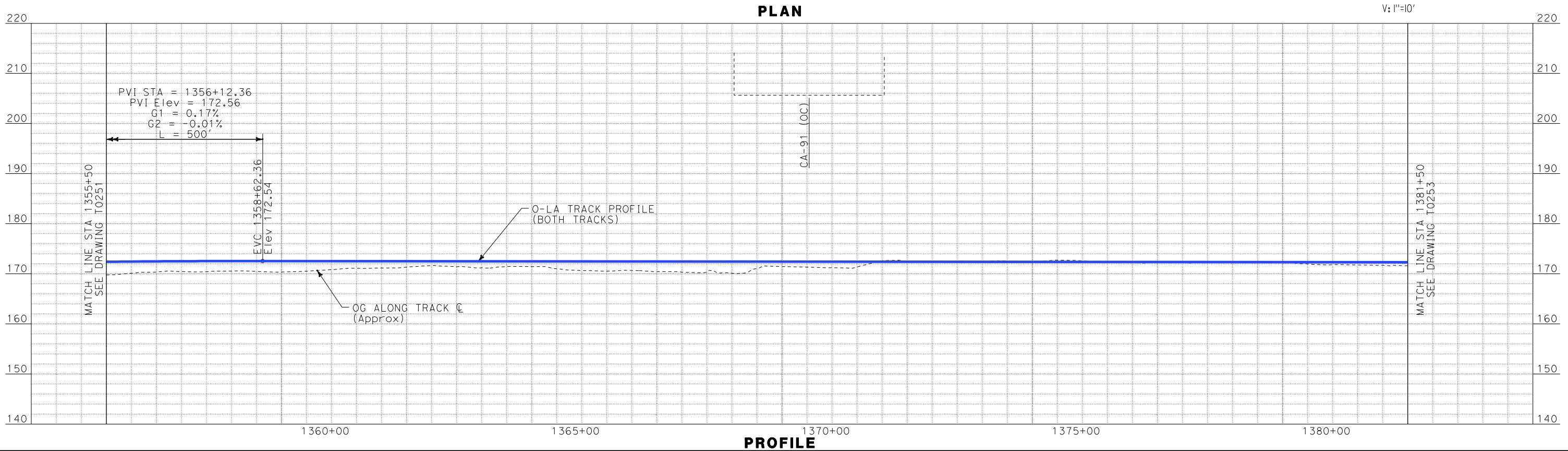
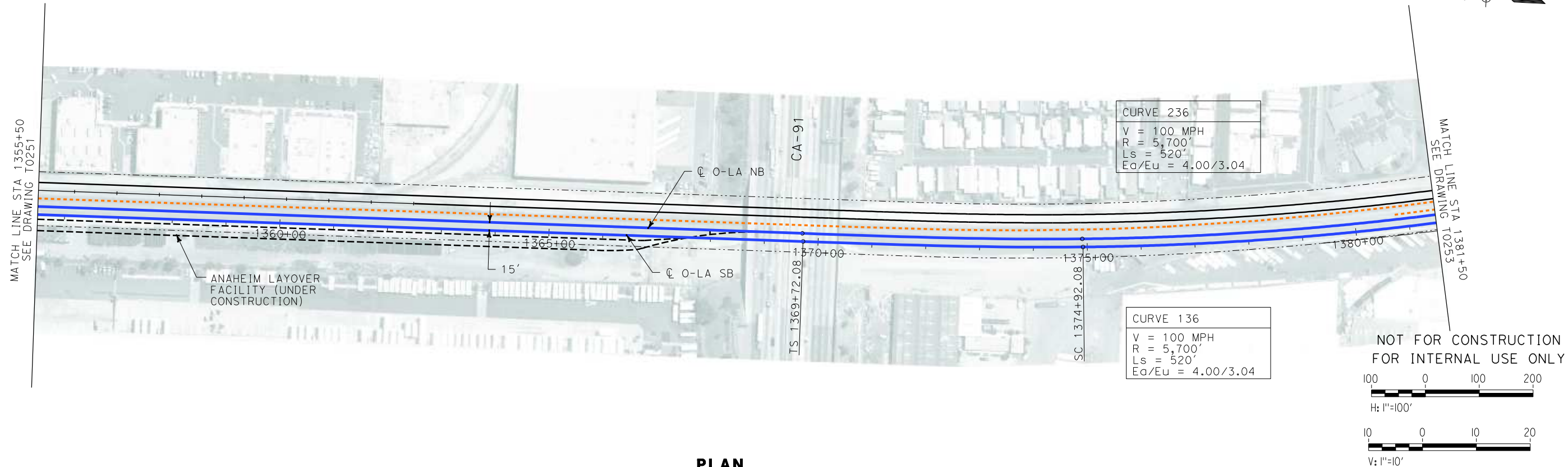
CALIFORNIA HIGH SPEED RAIL AUTHORITY
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE


CONTRACT NO.
HSR06-0005
DRAWING NO.
T0251
SCALE
AS SHOWN
SHEET NO.
72 OF 245

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Sperry\km
\$\$\$\$\$SYTIME\$\$\$\$\$



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



**FLY CALIFORNIA**
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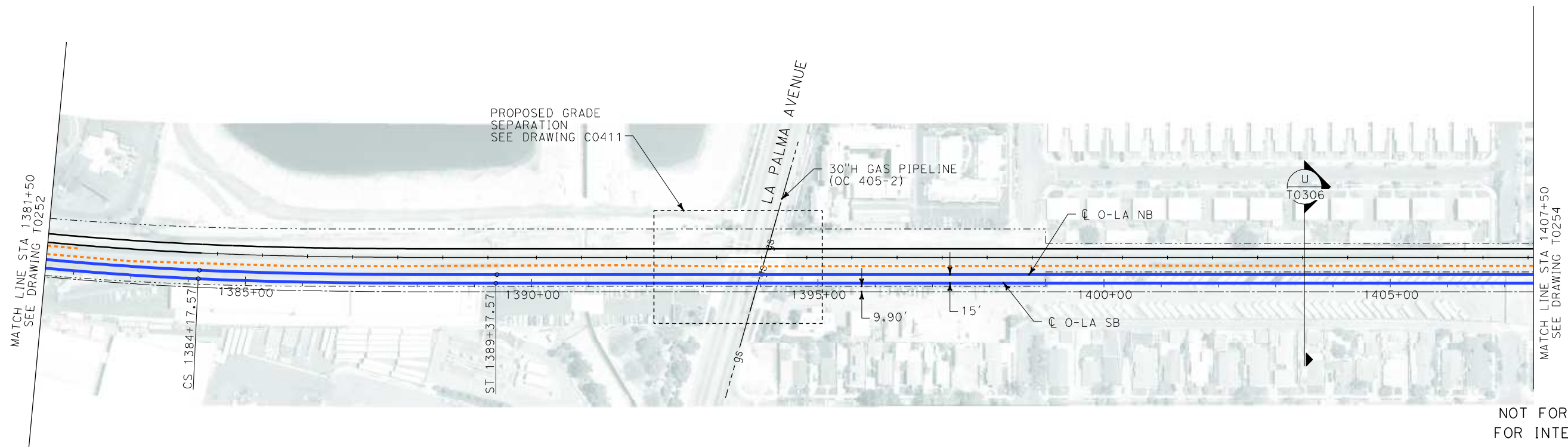
CALIFORNIA HIGH SPEED RAIL AUTHORITY

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0252
SCALE AS SHOWN
SHEET NO. 73 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km

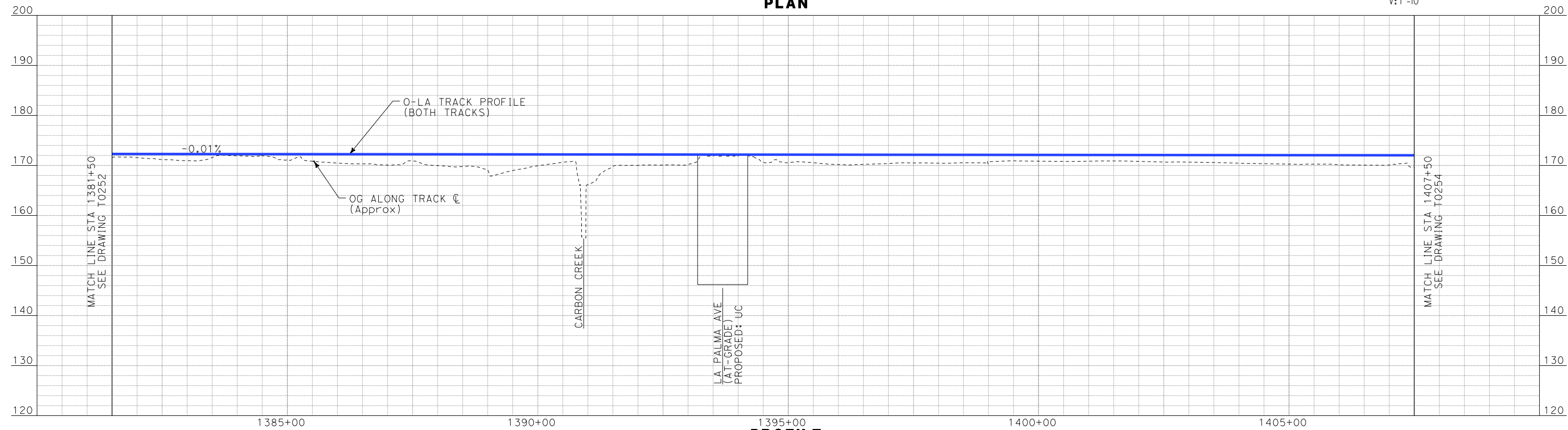


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100 0 100 200
H: 1"=100'

10 0 10 20
V: 1"=10'

PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





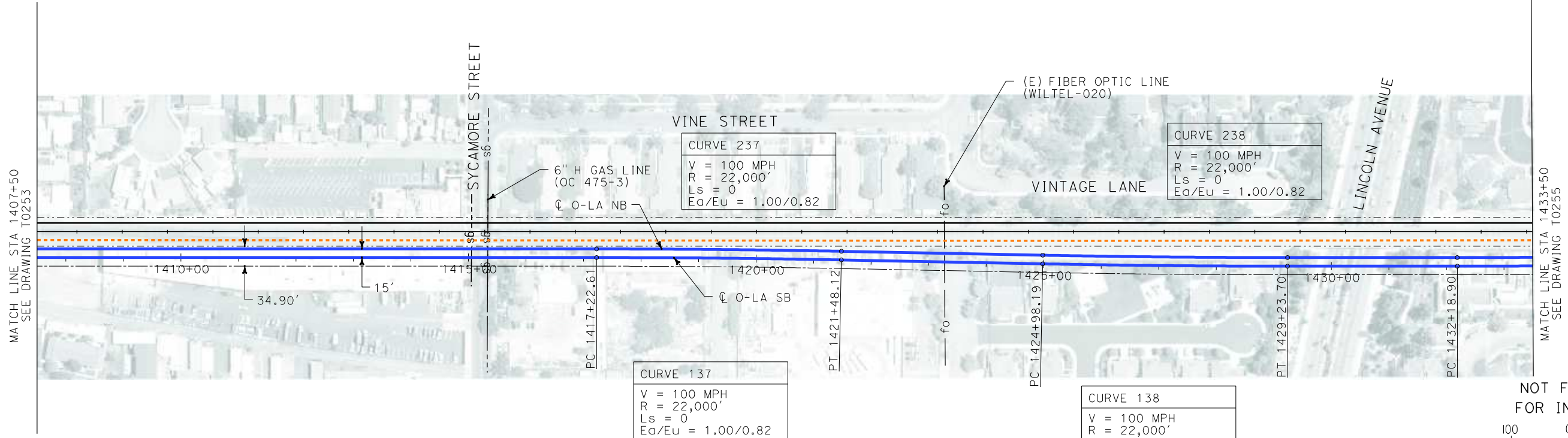
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

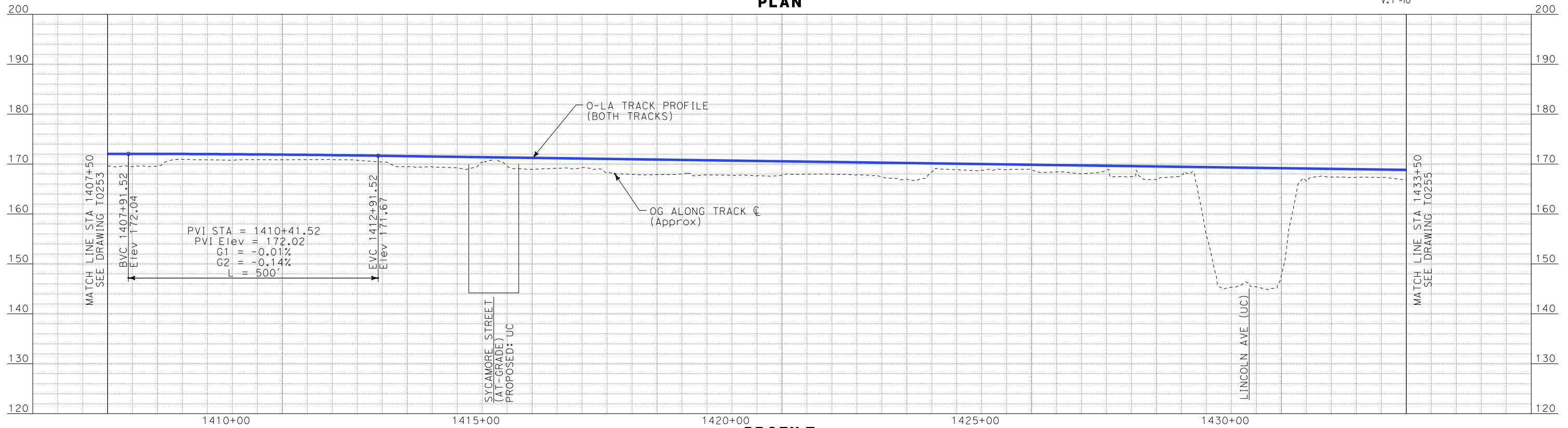
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0253
SCALE AS SHOWN
SHEET NO. 74 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

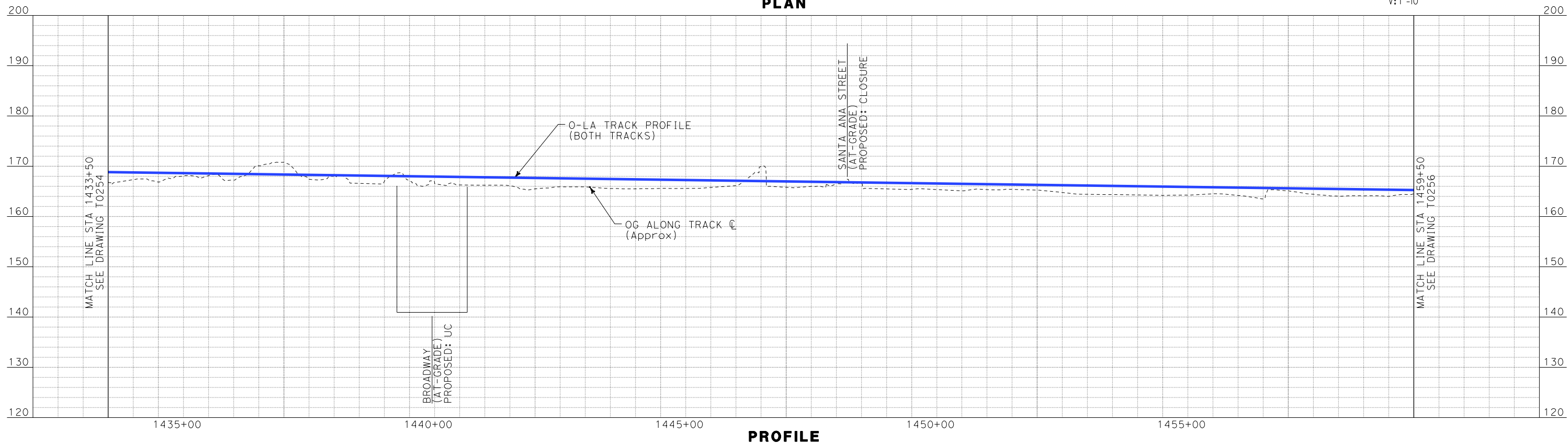
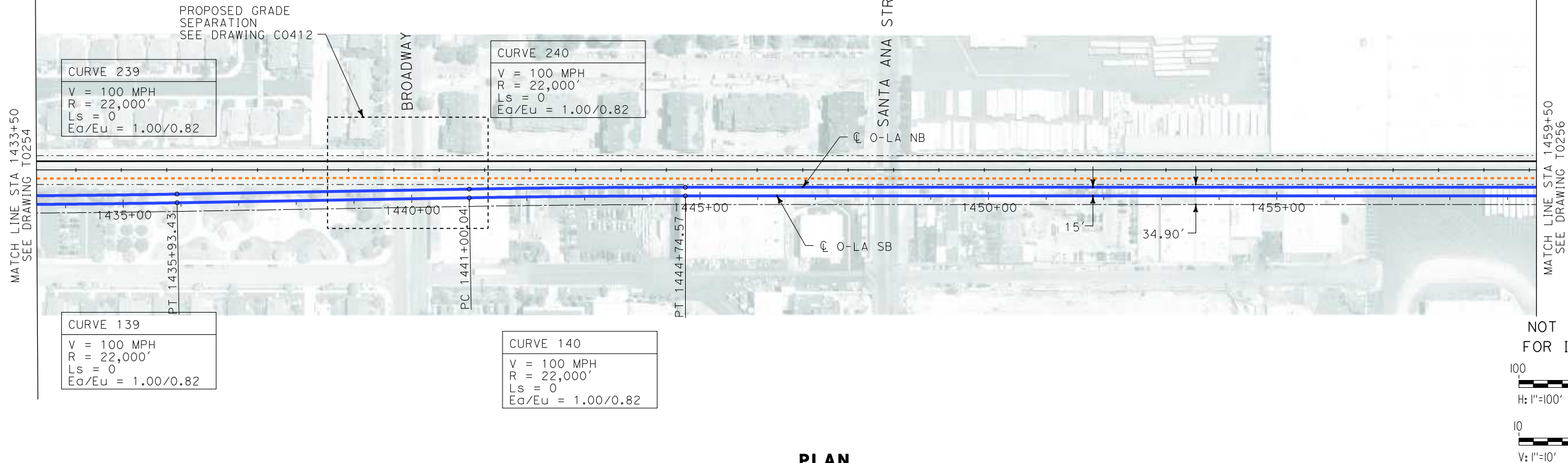
STV Incorporated
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0254
SCALE AS SHOWN
SHEET NO. 75 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k



REV	DATE	BY	SUB	APP	DESCRIPTION

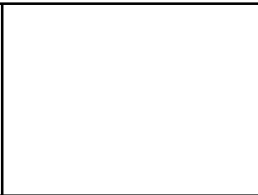
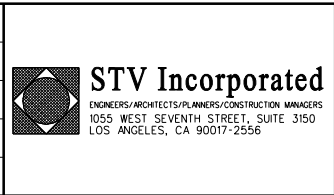
DESIGNED BY
K. SPERRY

DRAWN BY
K. SPERRY

CHECKED BY
A. SOKOL

IN CHARGE
D. THOMSON

DATE
02-13-09



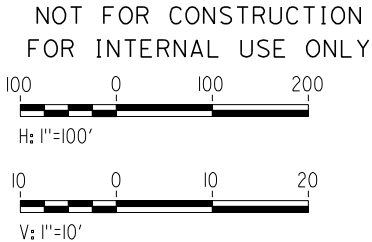
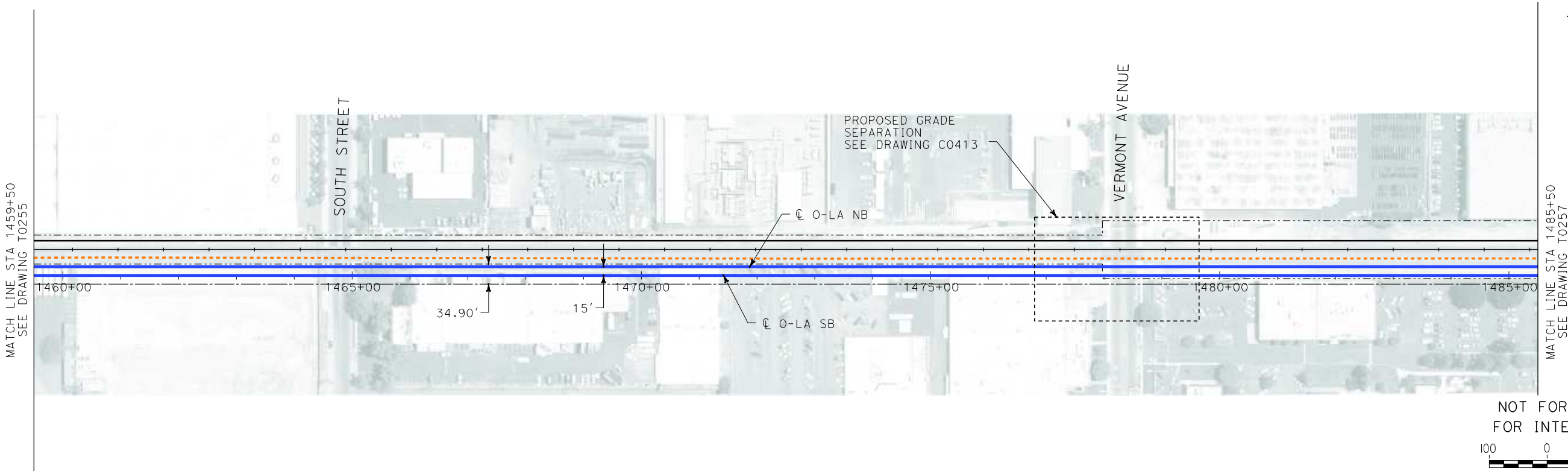
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005

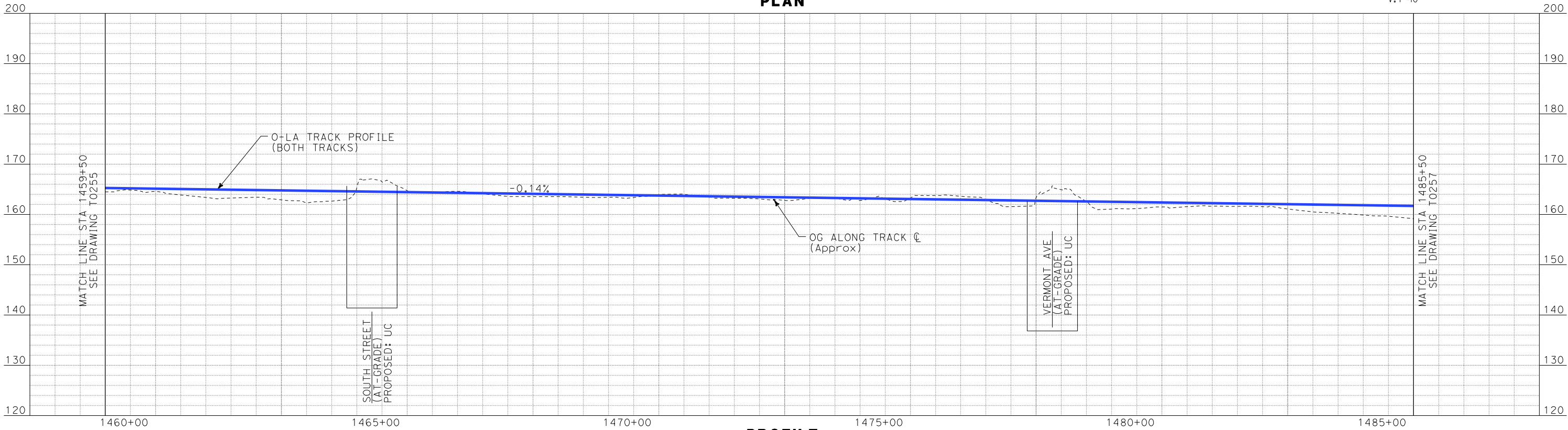
DRAWING NO.
T0255

SCALE
AS SHOWN

SHEET NO.
76 OF 245



PLAN




PROFILE

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\km

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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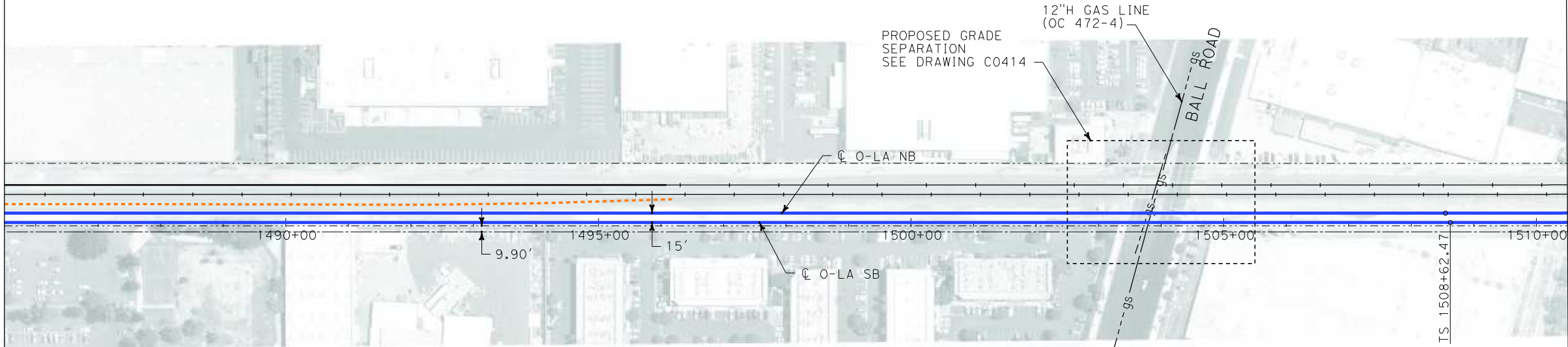
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0256
SCALE
AS SHOWN
SHEET NO.
77 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k

MATCH LINE STA 1485+50
SEE DRAWING T0256



MATCH LINE STA 1510+50
SEE DRAWING T0258

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FOR INTERNAL USE ONLY

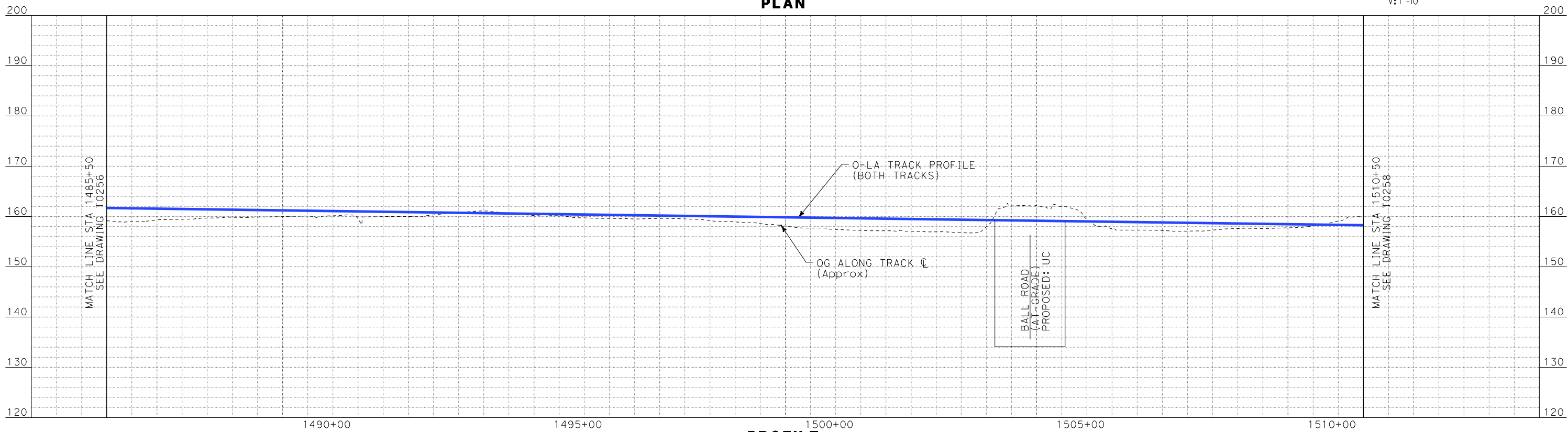


H: 1"=100'



V: 1"=10'


PLAN



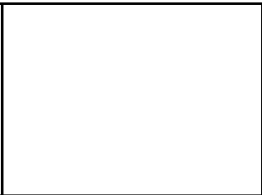
PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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IN CHARGE D. THOMSON
DATE 02-13-09



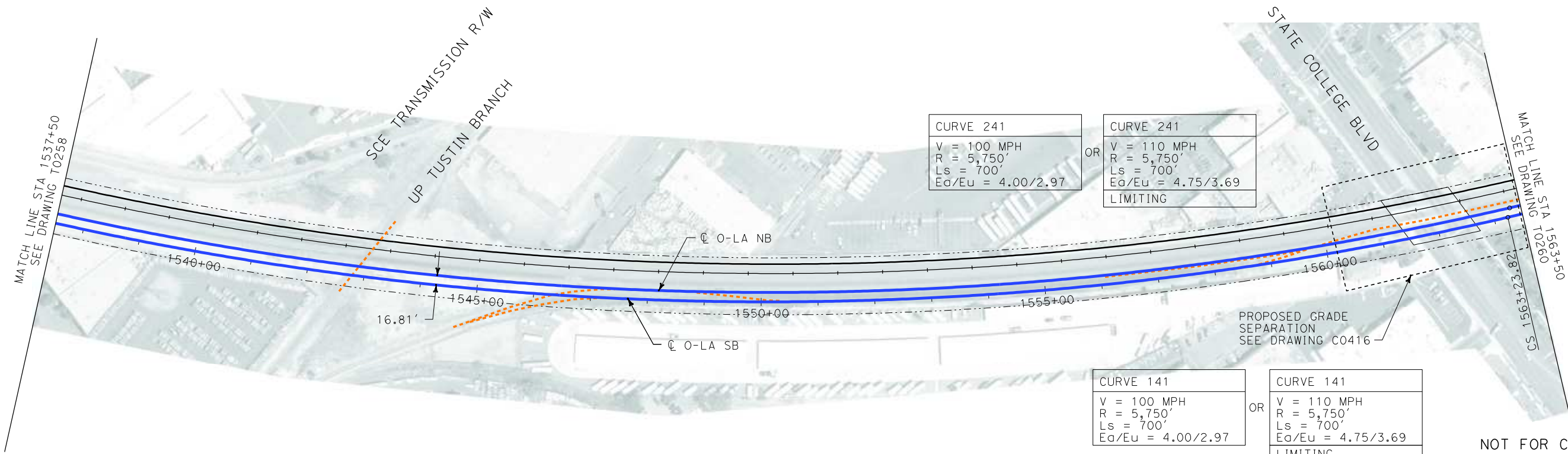
STV Incorporated
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0257
SCALE AS SHOWN
SHEET NO. 78 OF 245



CURVE 241
V = 100 MPH
R = 5,750'
Ls = 700'
Ea/Eu = 4.00/2.97

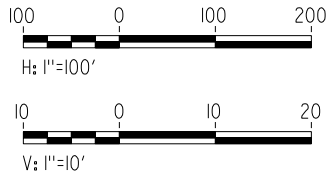
OR
CURVE 241
V = 110 MPH
R = 5,750'
Ls = 700'
Ea/Eu = 4.75/3.69
LIMITING

CURVE 141
V = 100 MPH
R = 5,750'
Ls = 700'
Ea/Eu = 4.00/2.97

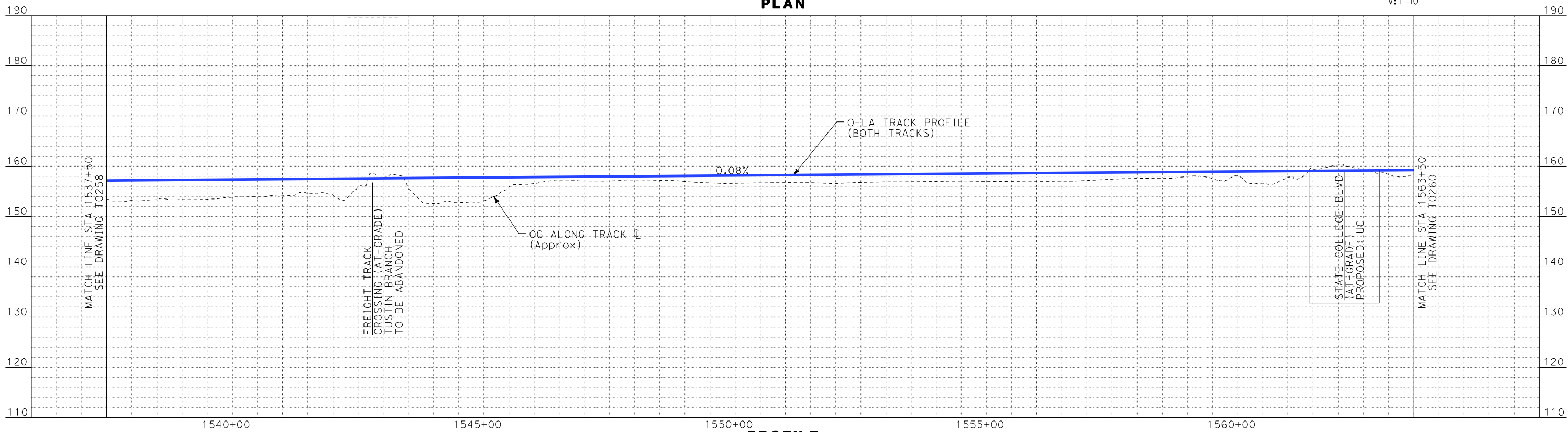
OR
CURVE 141
V = 110 MPH
R = 5,750'
Ls = 700'
Ea/Eu = 4.75/3.69
LIMITING

NOTE:
1. ALTERNATIVE CURVE DATA USING
LIMITING OR EXCEPTIONAL CRITERIA
TO ACHIEVE HIGHER DESIGN SPEED IS
PRESENTED FOR CONSIDERATION.

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PLAN




PROFILE

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Sperry\kjm

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





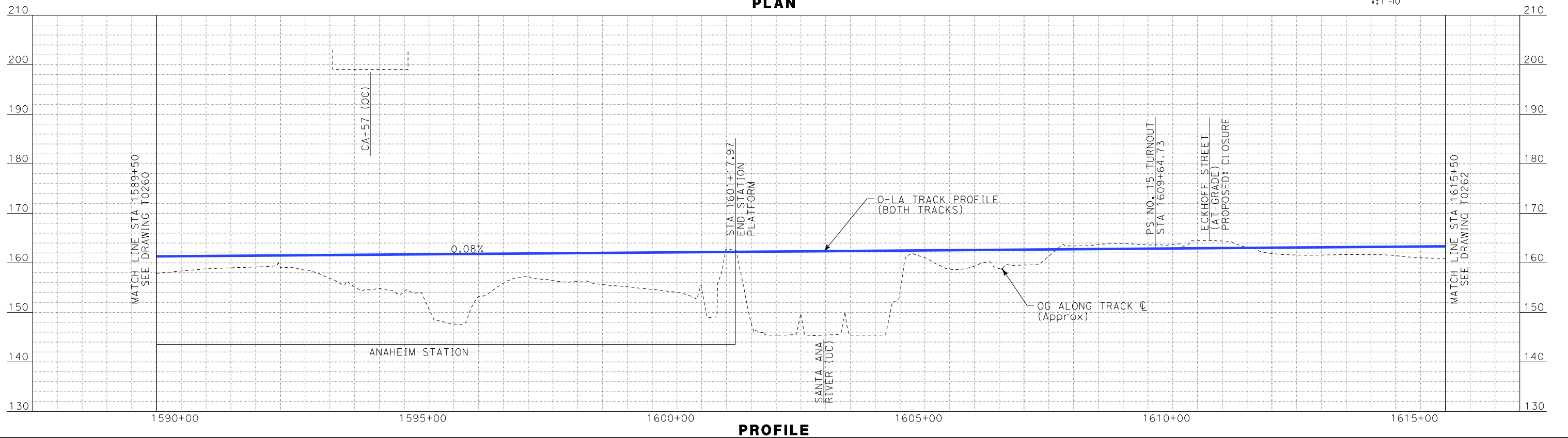
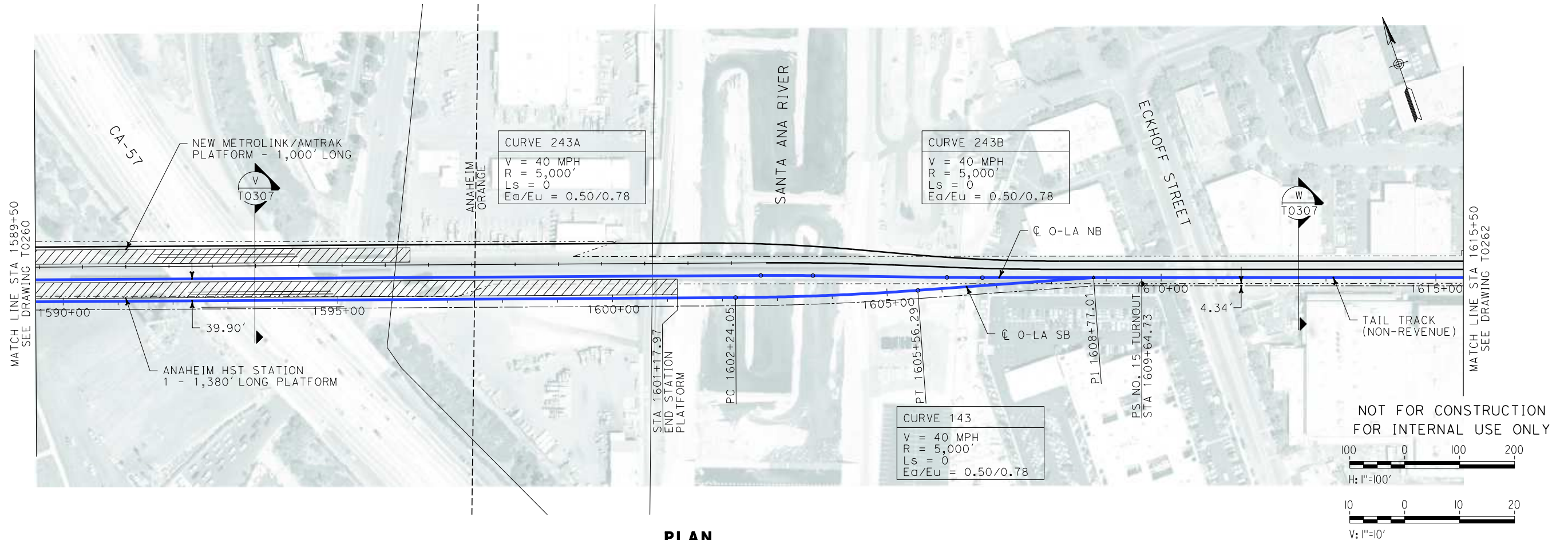
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0259
SCALE
AS SHOWN
SHEET NO.
80 OF 245

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Sperry\kkm
\$\$\$\$\$SYTIME\$\$\$\$\$



REV	DATE	BY	SUB	APP	DESCRIPTION

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DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

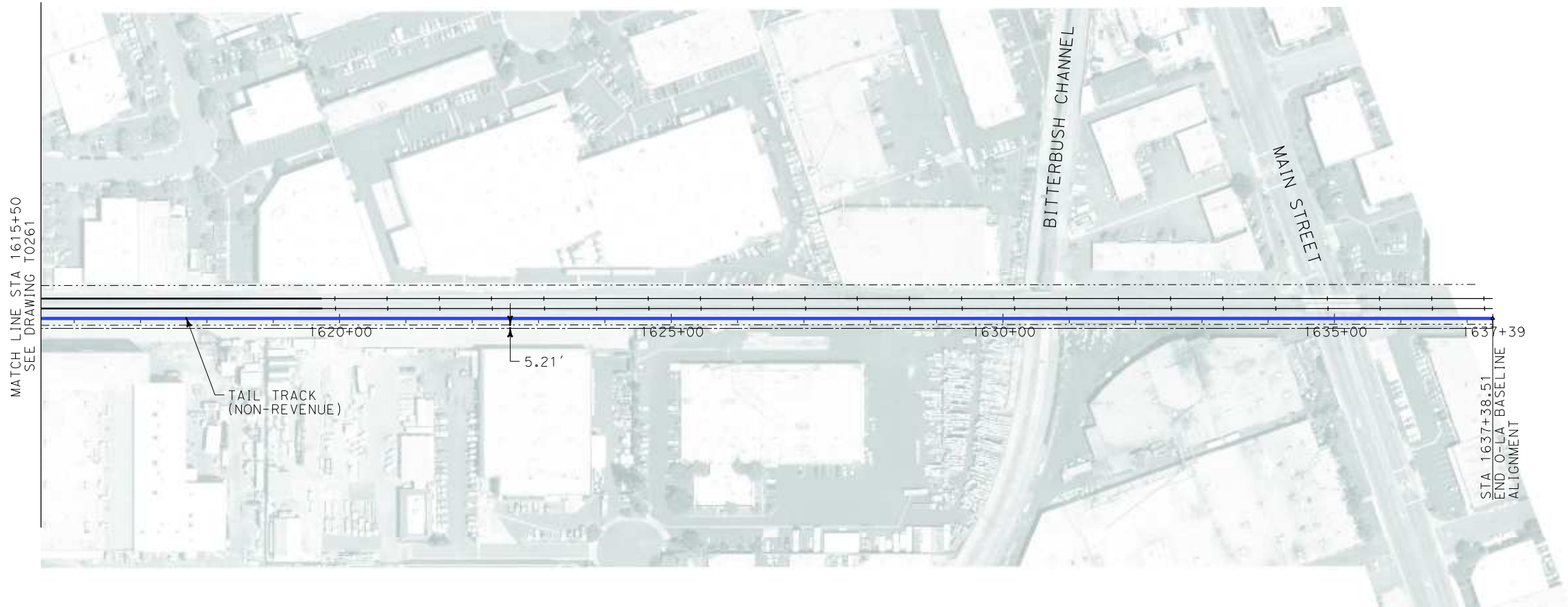


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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0261
SCALE AS SHOWN
SHEET NO. 82 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k

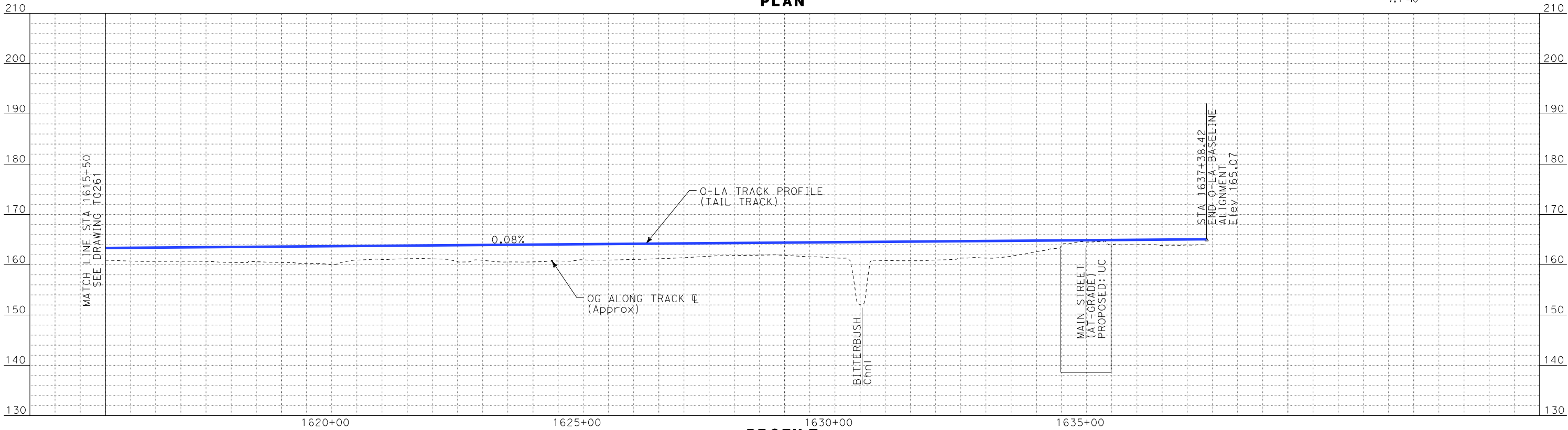


PLAN

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H: 1"=100'
V: 1"=10'



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

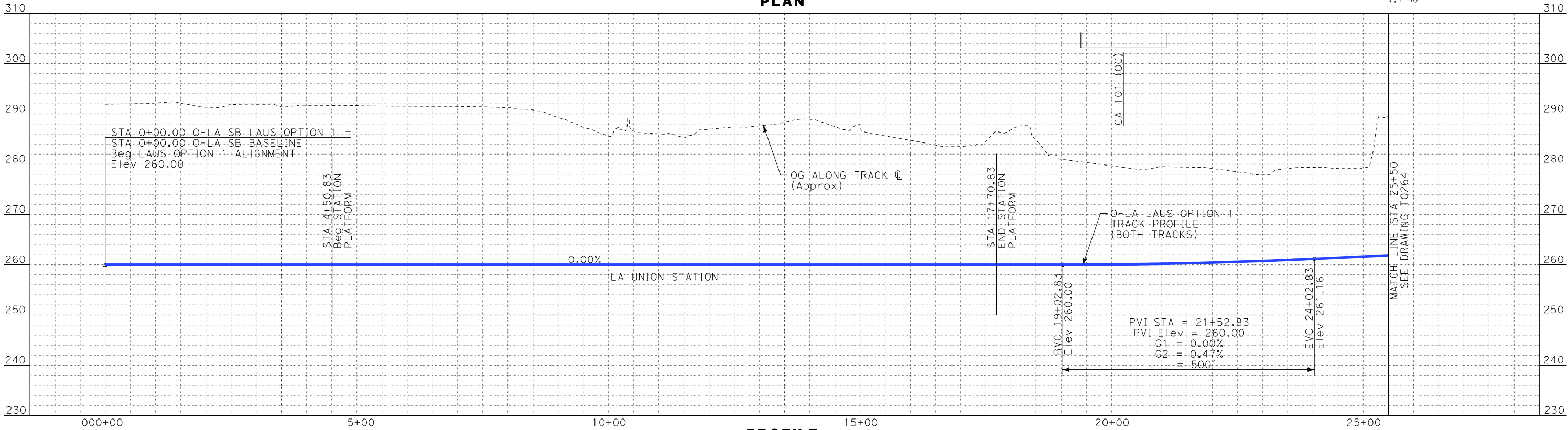
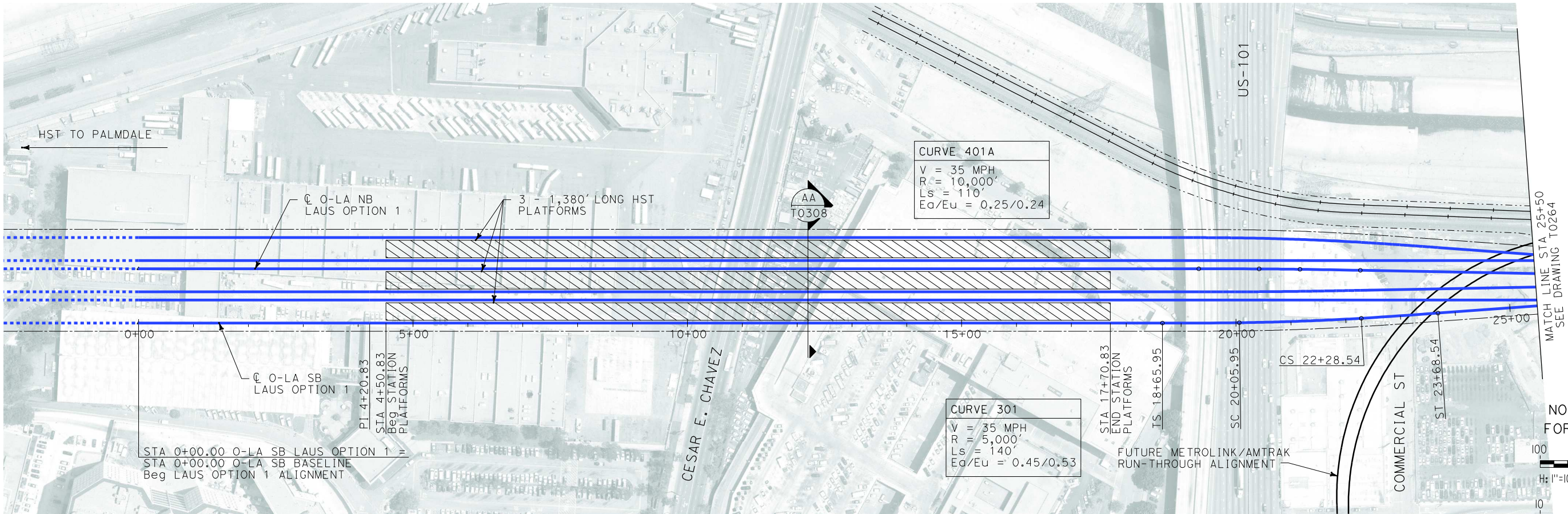


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ORANGE COUNTY TO LOS ANGELES**
BASELINE ALIGNMENT

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CONTRACT NO.
HSR06-0005
DRAWING NO.
T0262
SCALE
AS SHOWN
SHEET NO.
83 OF 245



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Sperry\k

REV	DATE	BY	SUB	APP	DESCRIPTION

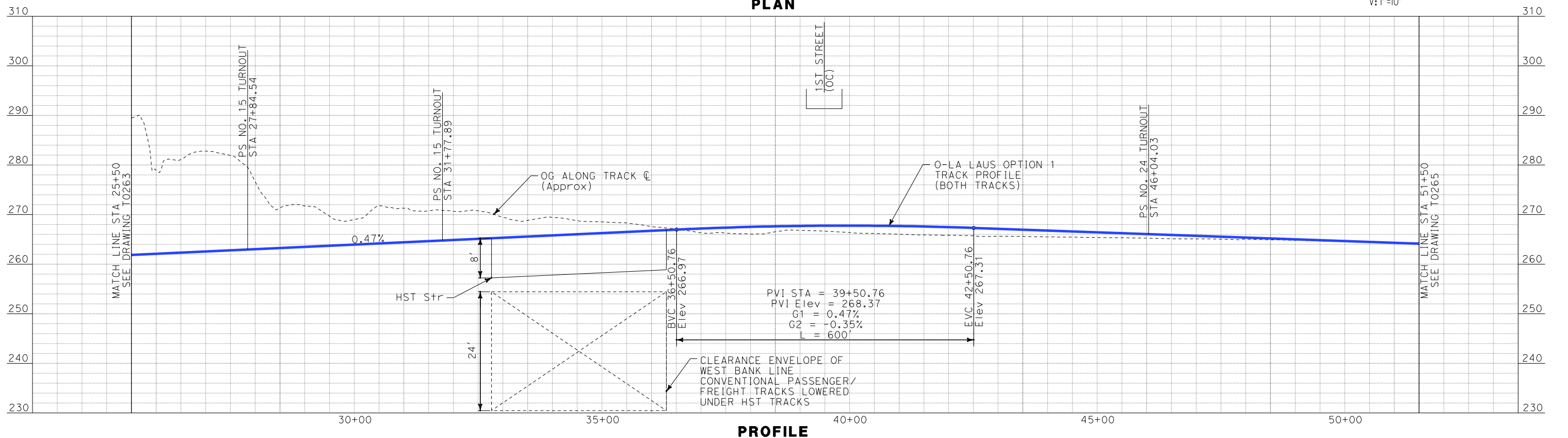
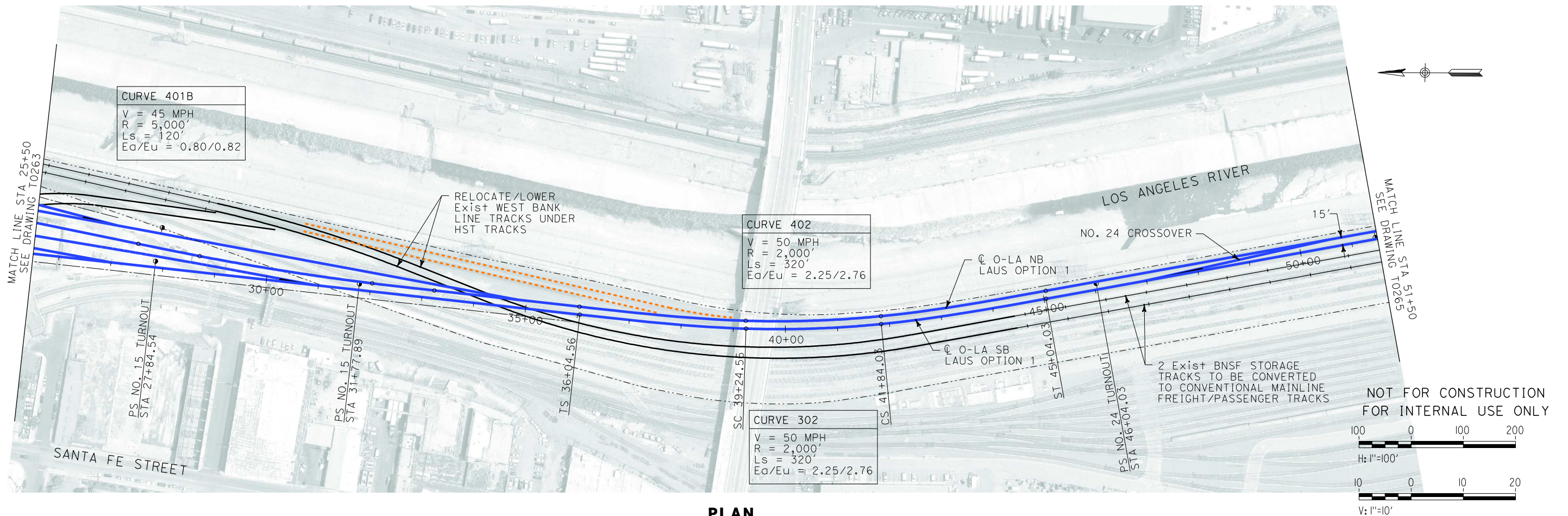
DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

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ORANGE COUNTY TO LOS ANGELES
LAUS OPTION 1 ALIGNMENT
WEST BANK STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0263
SCALE AS SHOWN
SHEET NO. 84 OF 245



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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

REV	DATE	BY	SUB	APP	DESCRIPTION

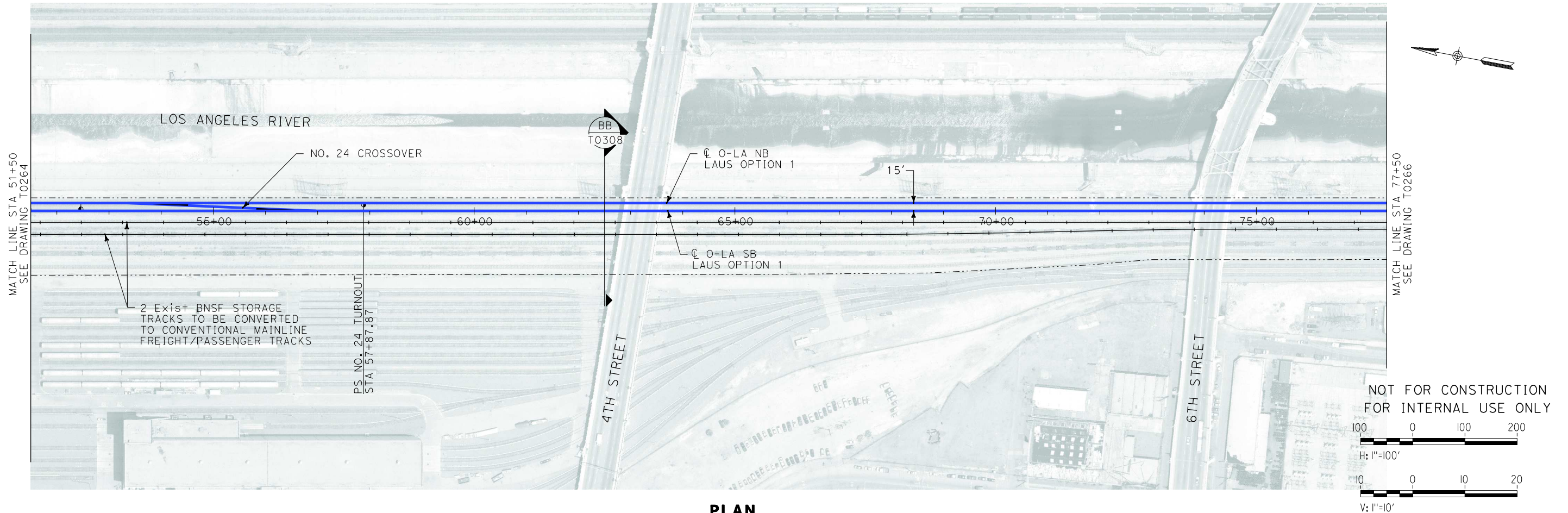
DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09



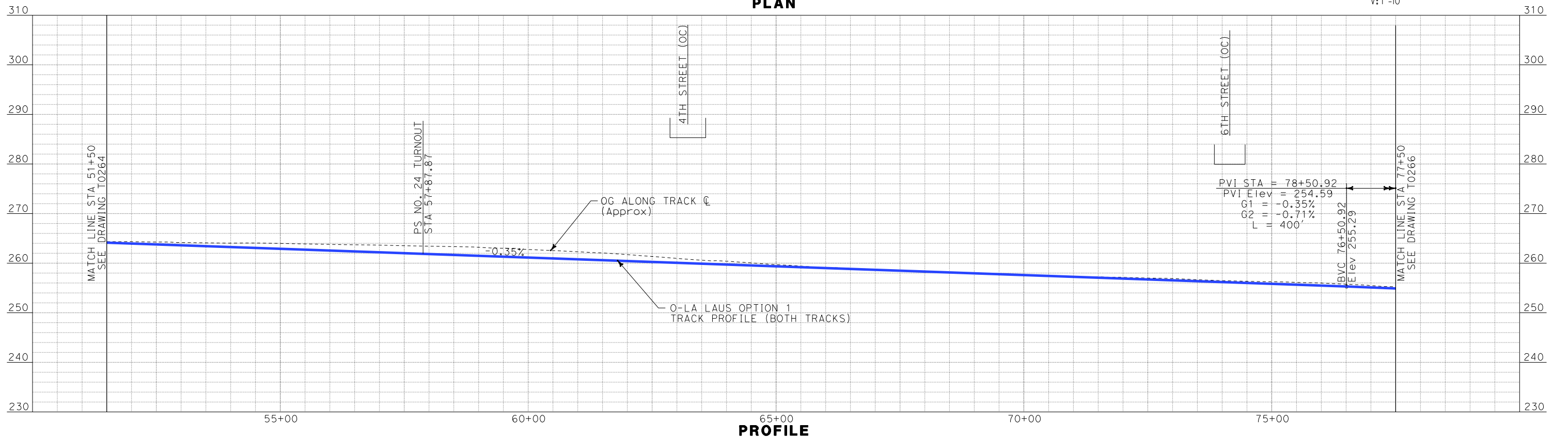
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
LAUS OPTION 1 ALIGNMENT
WEST BANK STATION
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0264
SCALE
AS SHOWN
SHEET NO.
85 OF 245

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Sperry




PLAN



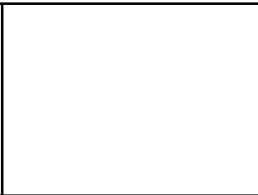
PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

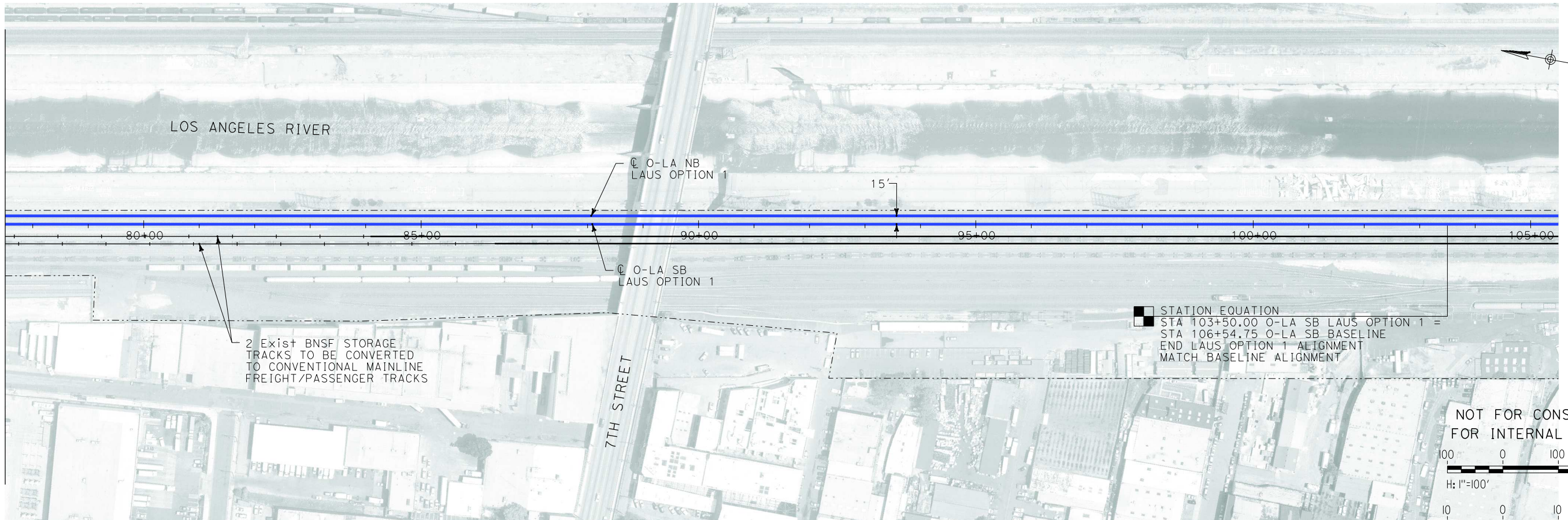


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
LAUS OPTION 1 ALIGNMENT
WEST BANK STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0265
SCALE AS SHOWN
SHEET NO. 86 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

MATCH LINE STA 77+50
SEE DRAWING T0265



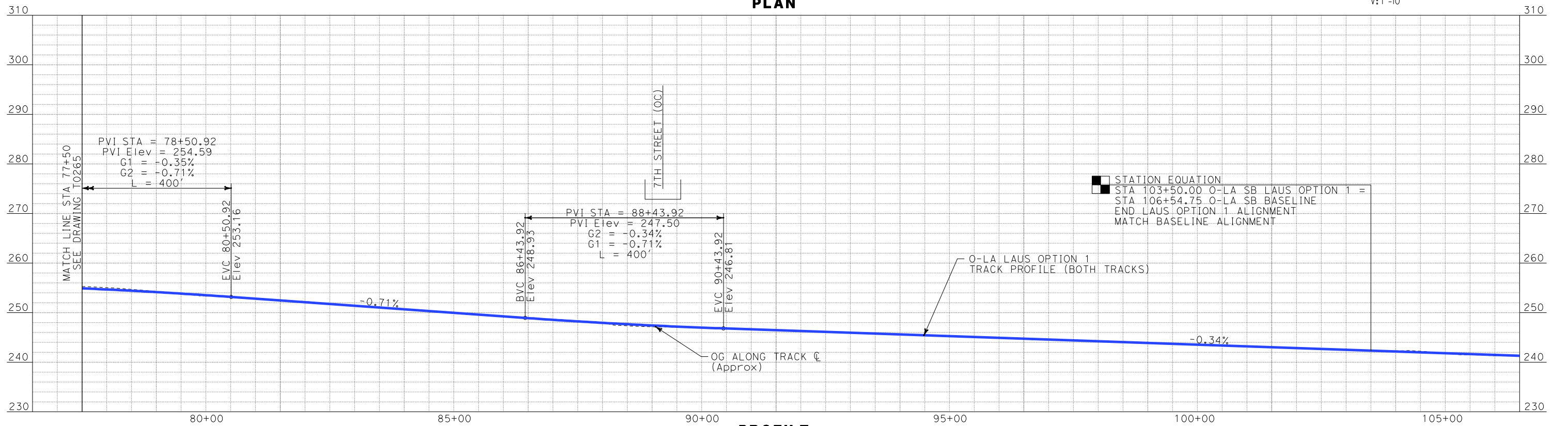
STATION EQUATION
STA 103+50.00 O-LA SB LAUS OPTION 1 =
STA 106+54.75 O-LA SB BASELINE
END LAUS OPTION 1 ALIGNMENT
MATCH BASELINE ALIGNMENT

NOT FOR CONSTRUCTION
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100 0 100 200
H: 1"=100'

10 0 10 20
V: 1"=10'

PLAN



STATION EQUATION
STA 103+50.00 O-LA SB LAUS OPTION 1 =
STA 106+54.75 O-LA SB BASELINE
END LAUS OPTION 1 ALIGNMENT
MATCH BASELINE ALIGNMENT

PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

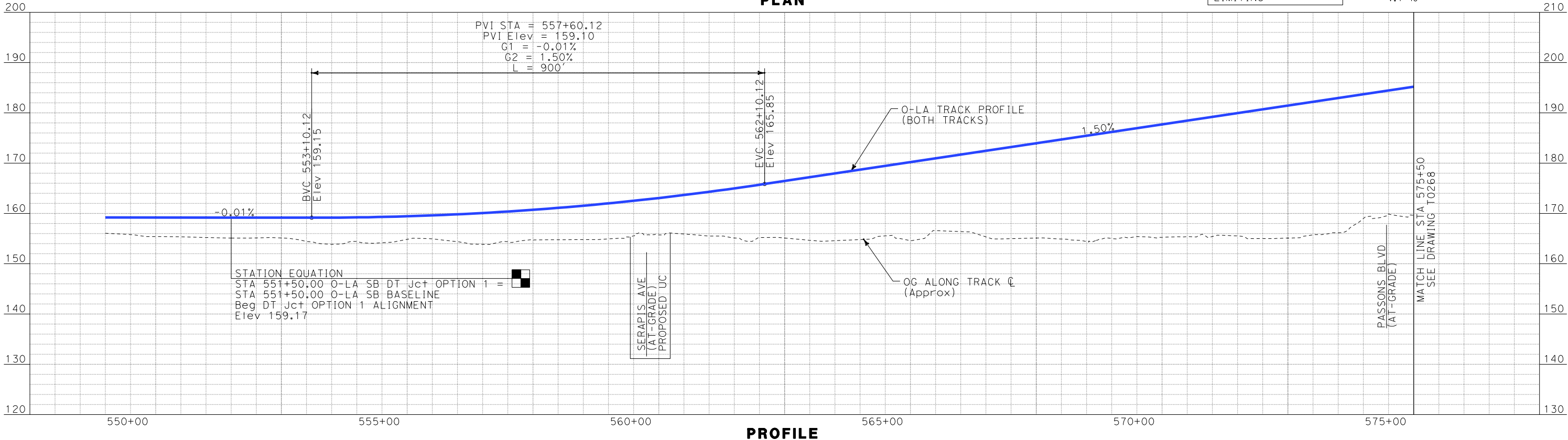
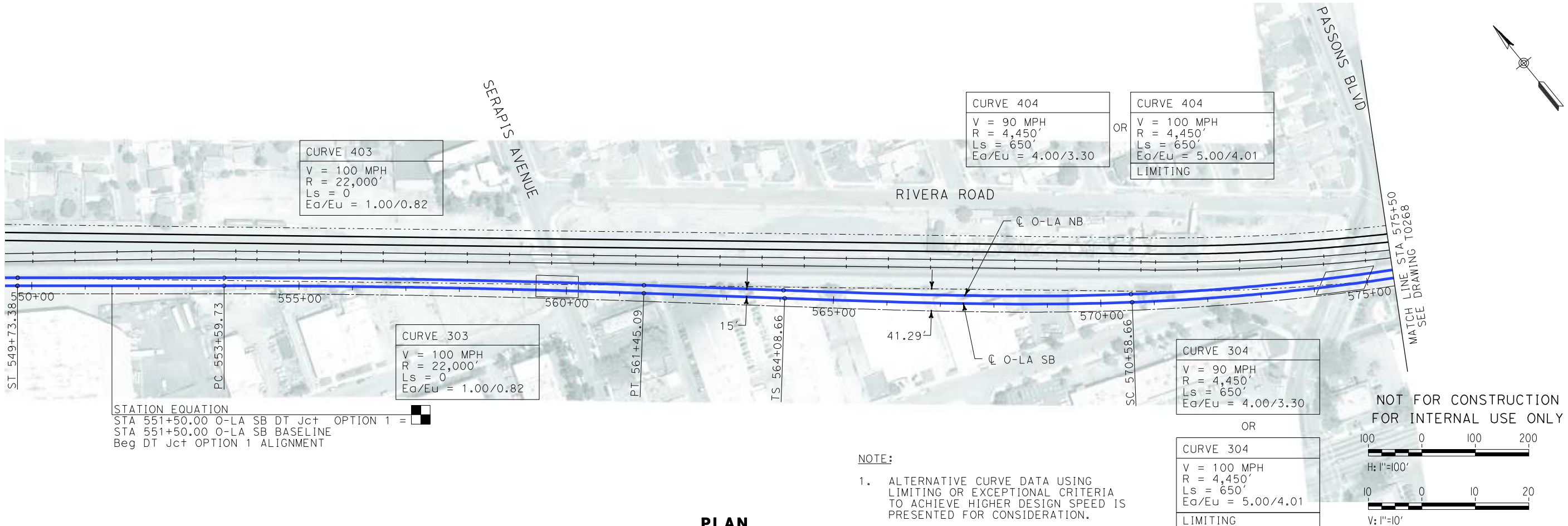
DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

STV Incorporated
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LOS ANGELES, CA 90017-2556



**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**
LAUS OPTION 1 ALIGNMENT
WEST BANK STATION
PLAN AND PROFILE


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HSR06-0005
DRAWING NO.
T0266
SCALE
AS SHOWN
SHEET NO.
87 OF 245



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REV	DATE	BY	SUB	APP	DESCRIPTION

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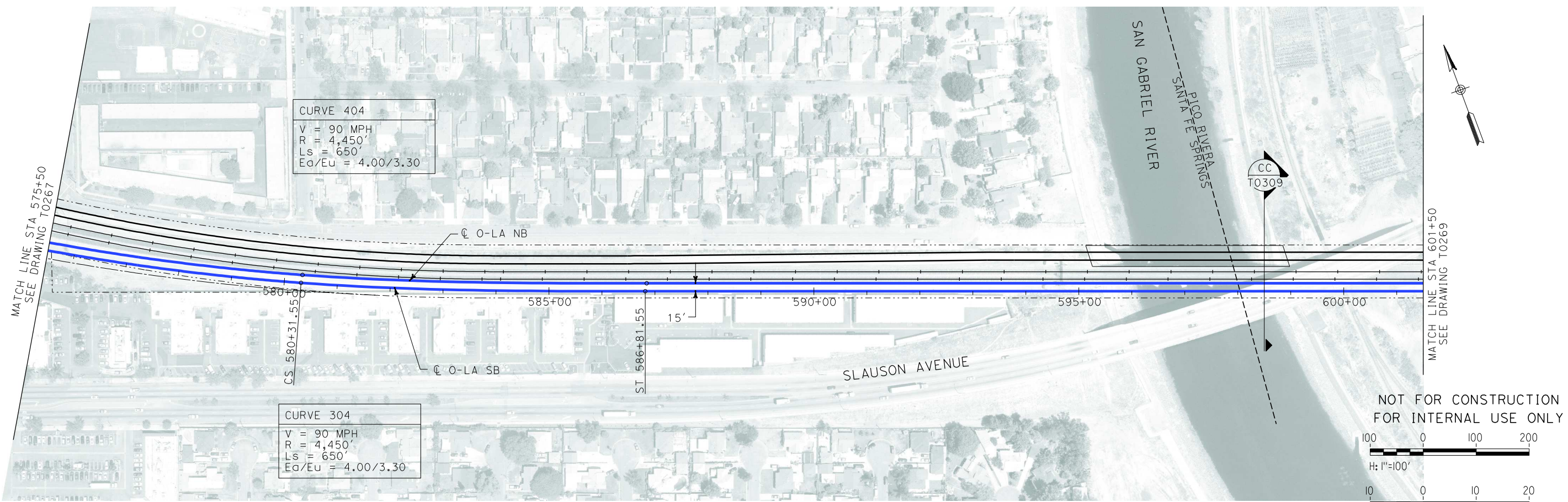




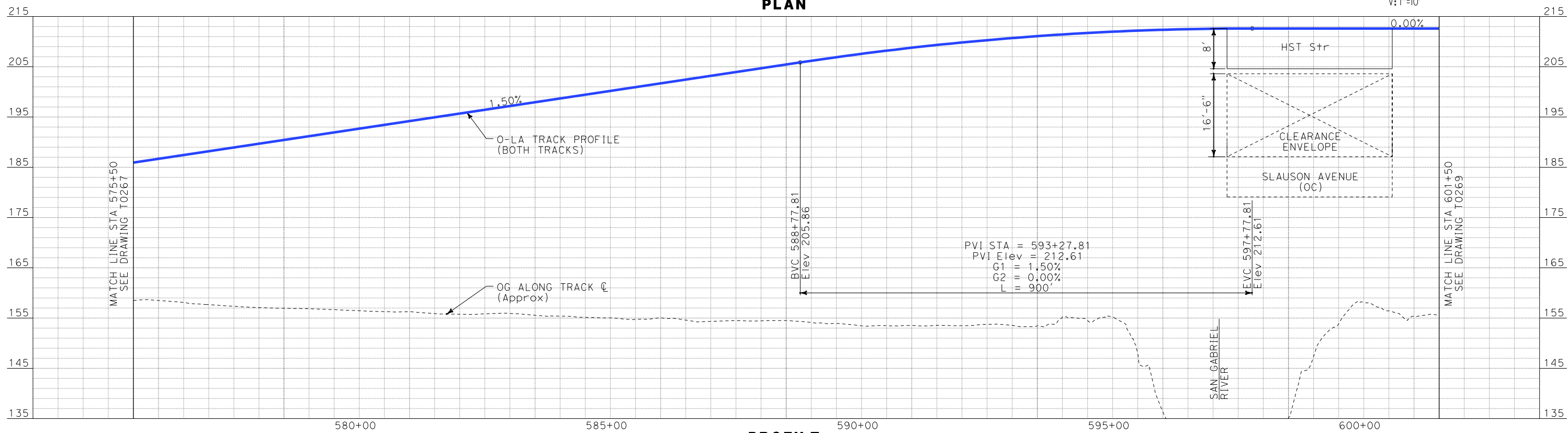
CALIFORNIA HIGH SPEED RAIL AUTHORITY
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0267
SCALE
AS SHOWN
SHEET NO.
88 OF 245



PLAN




PROFILE

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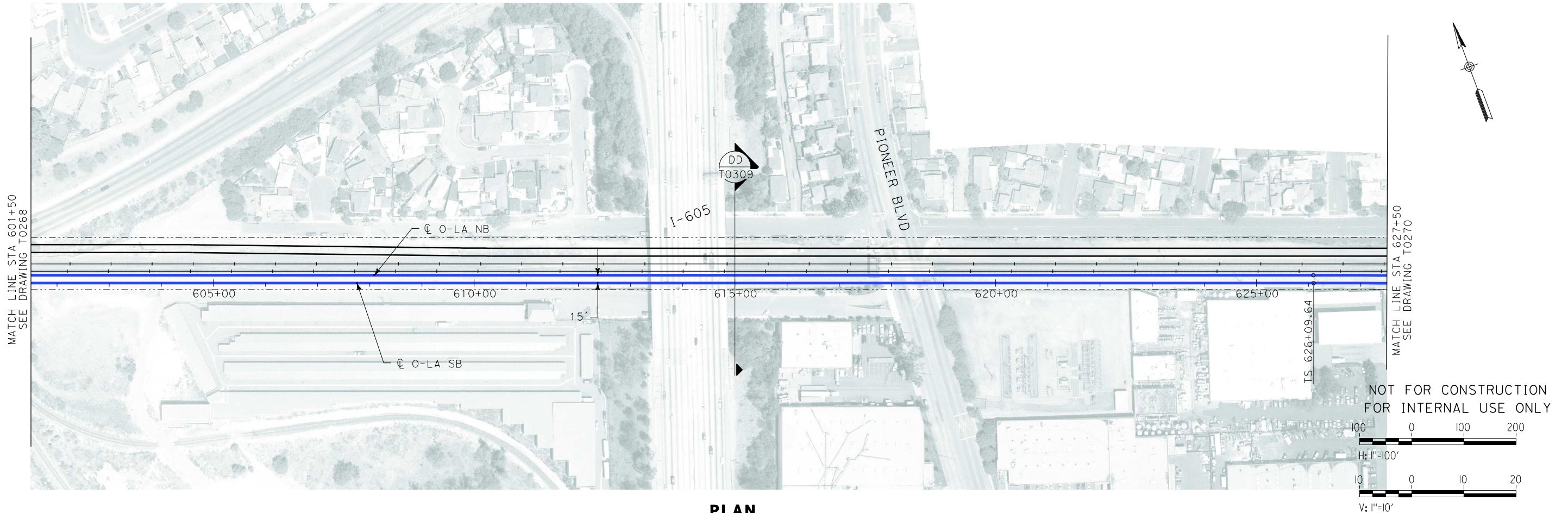


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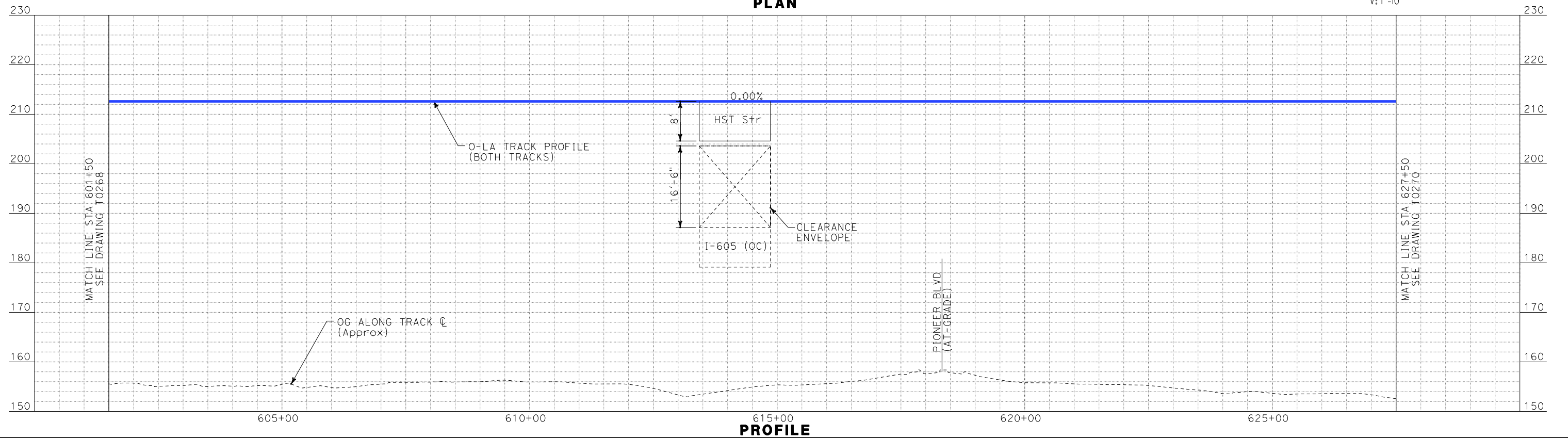
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ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0268
SCALE
AS SHOWN
SHEET NO.
89 OF 245

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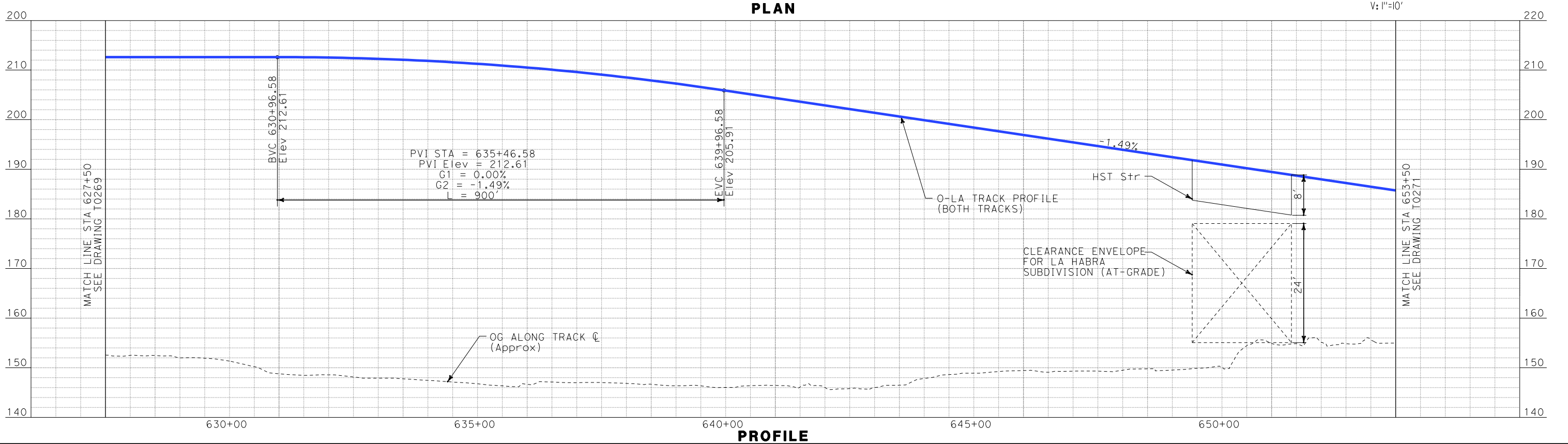
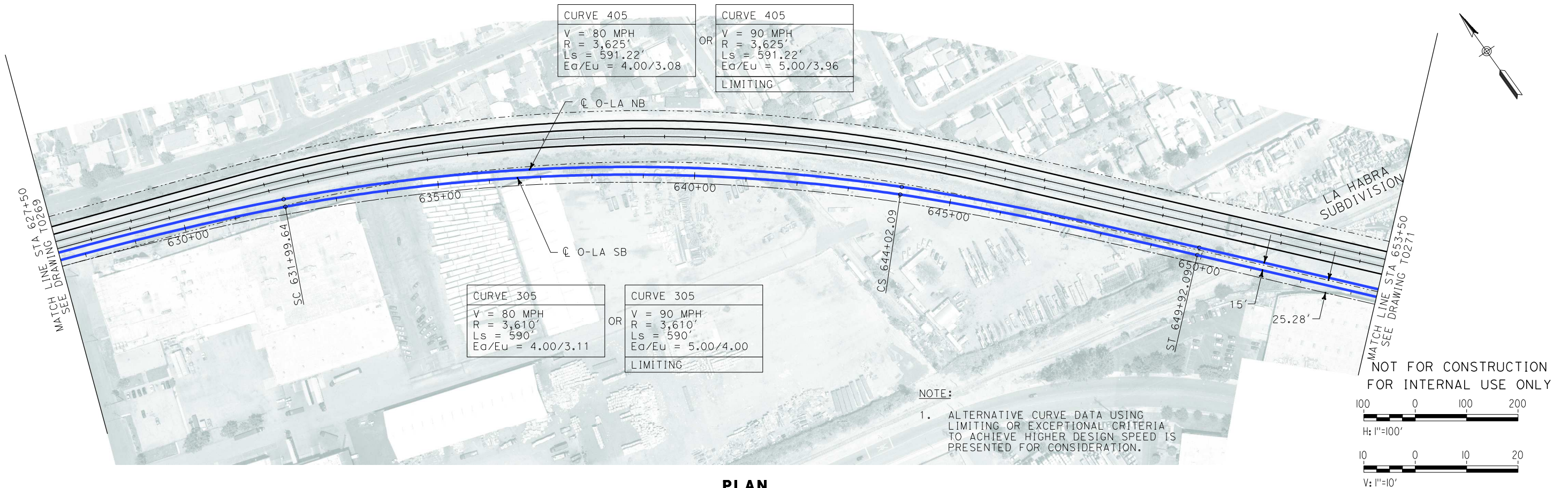
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ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
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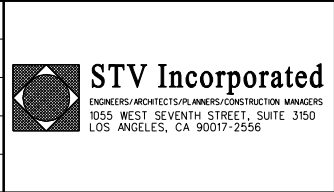
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SHEET NO. 90 OF 245

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Sperry



REV	DATE	BY	SUB	APP	DESCRIPTION

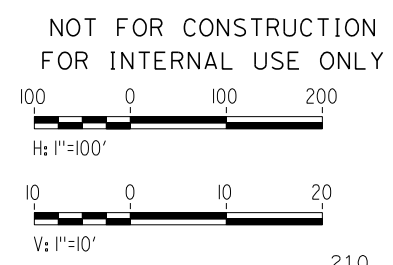
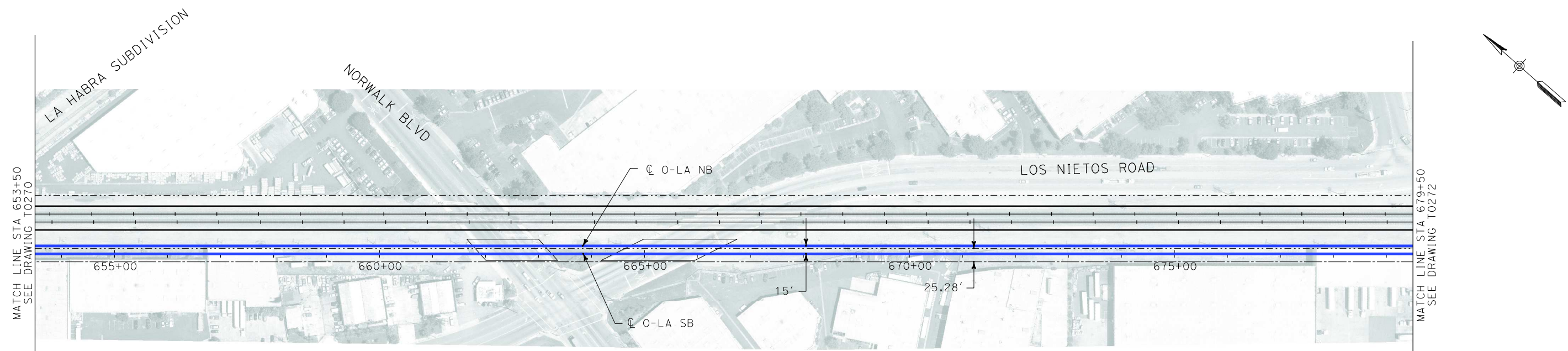
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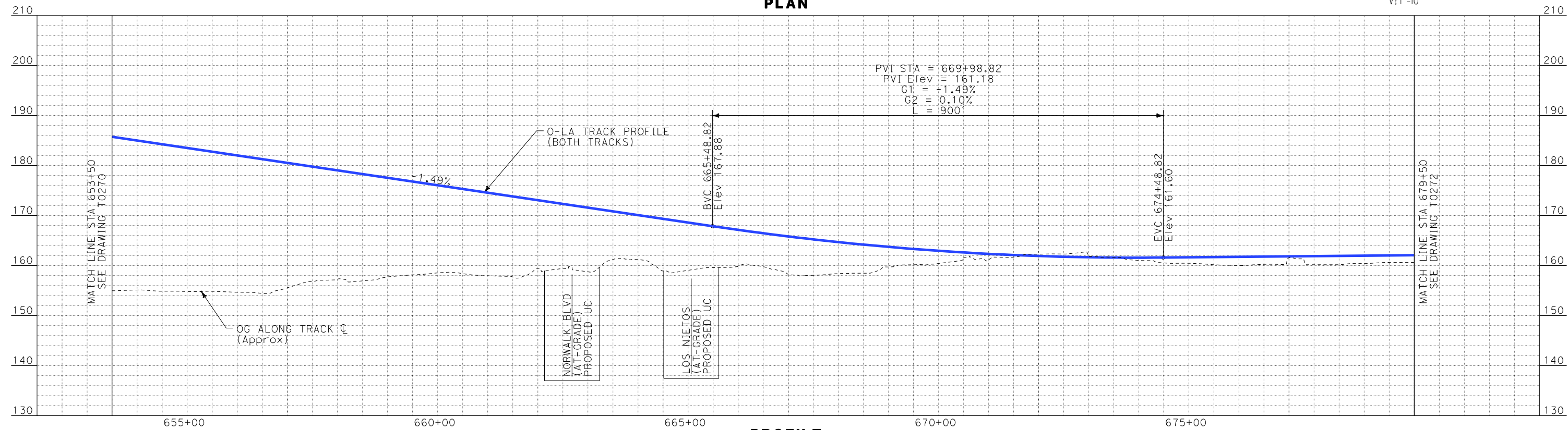
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0270
SCALE
AS SHOWN
SHEET NO.
91 OF 245

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PLAN



PROFILE

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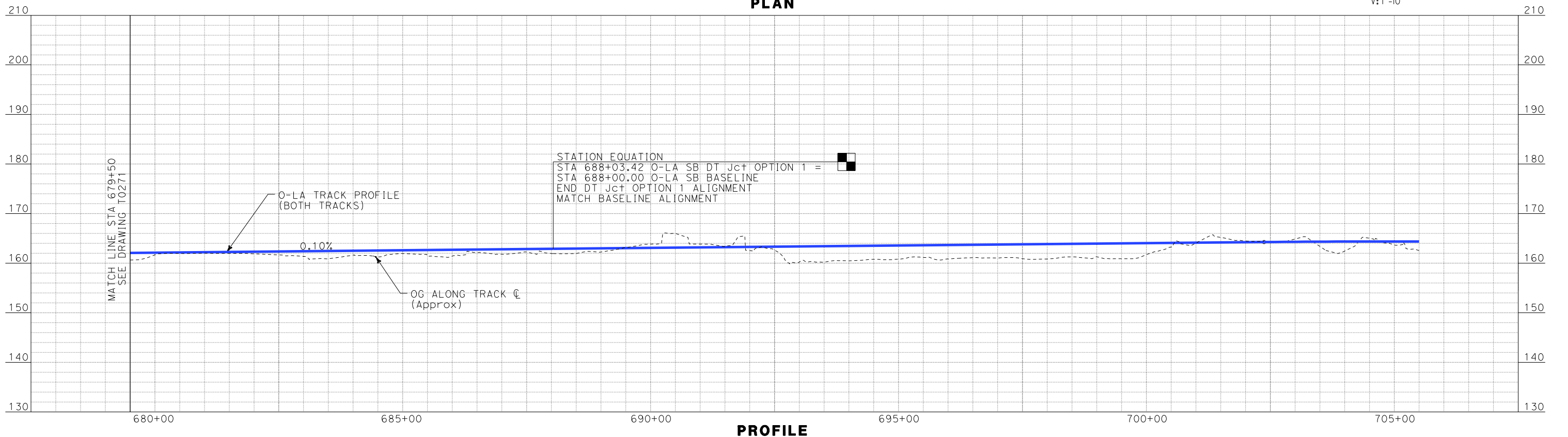
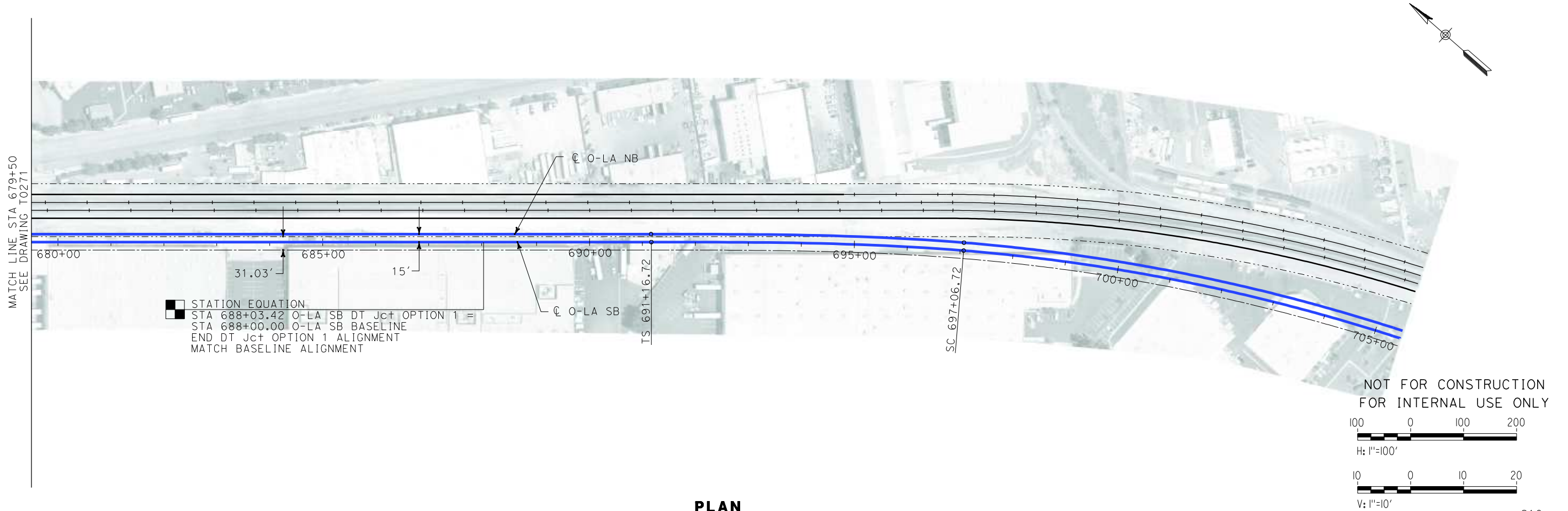




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ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
PLAN AND PROFILE


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DRAWING NO.	T0271
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SHEET NO.	92 OF 245



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\$\$\$\$\$SYTIME\$\$\$\$\$
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REV	DATE	BY	SUB	APP	DESCRIPTION

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IN CHARGE D. THOMSON
DATE 02-13-09

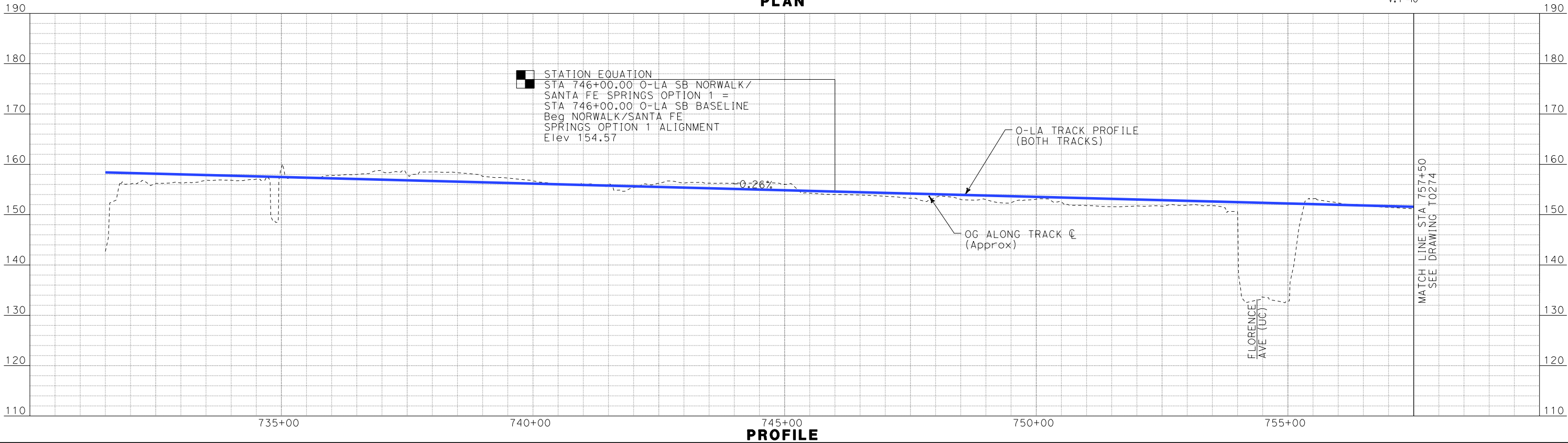
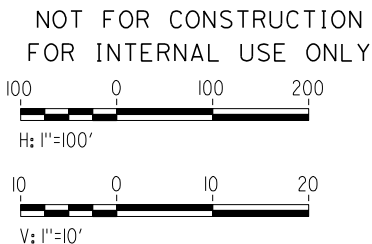
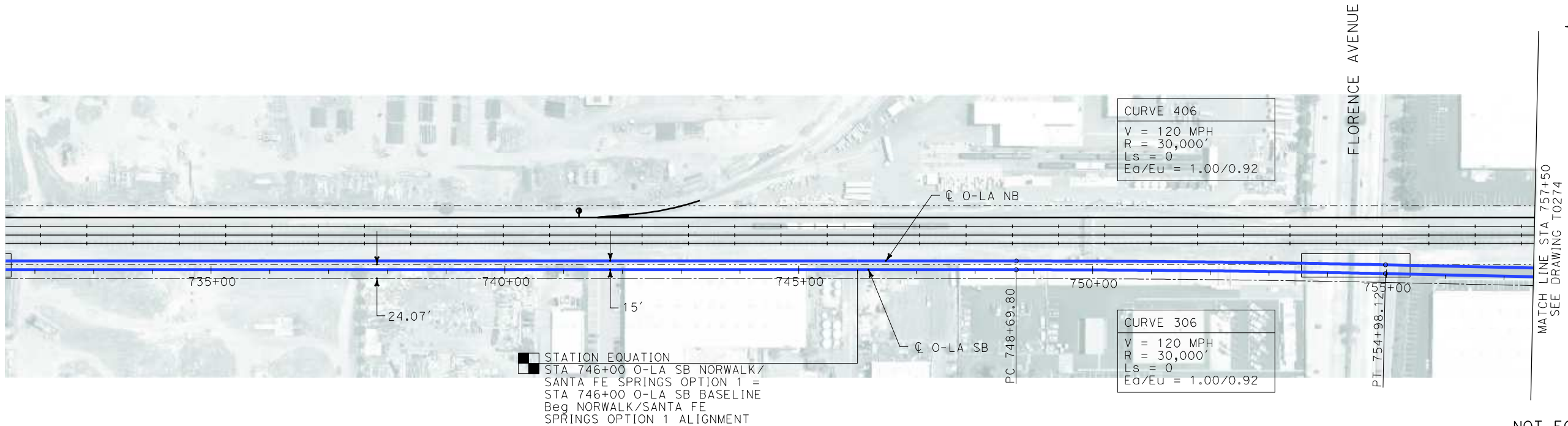
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ORANGE COUNTY TO LOS ANGELES
DT JUNCTION OPTION 1 ALIGNMENT
AERIAL STRUCTURE
PLAN AND PROFILE


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DRAWING NO. T0272
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SHEET NO. 93 OF 245



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1055 WEST SEVENTH STREET, SUITE 3150
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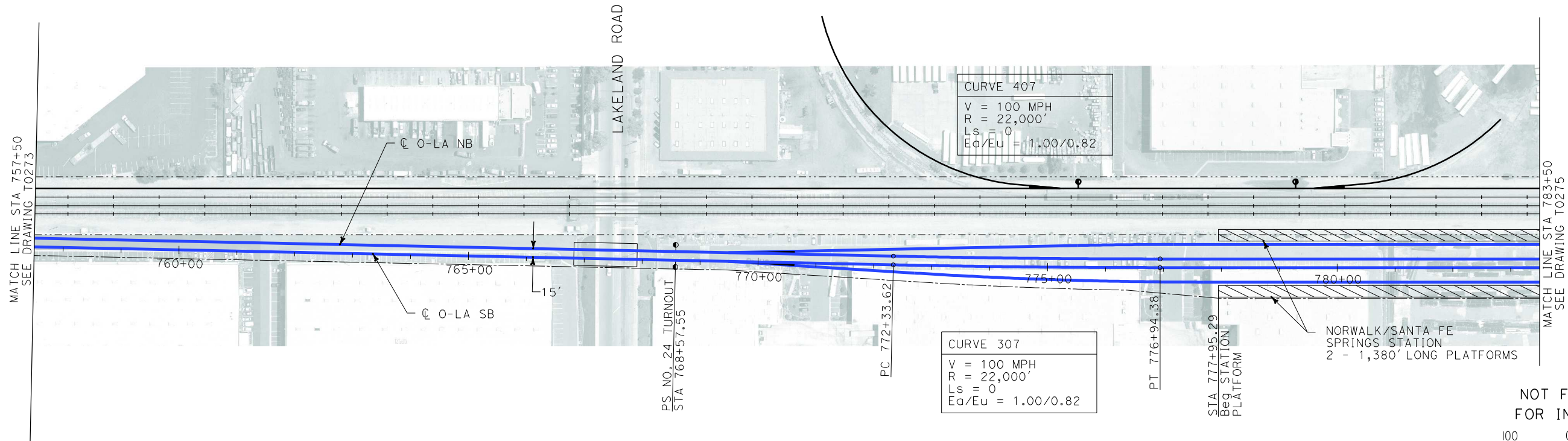


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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
NORWALK/SANTA FE SPRINGS STATION
OPTION 1 ALIGNMENT
NORTH OF IMPERIAL HIGHWAY
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0273
SCALE
AS SHOWN
SHEET NO.
94 OF 245

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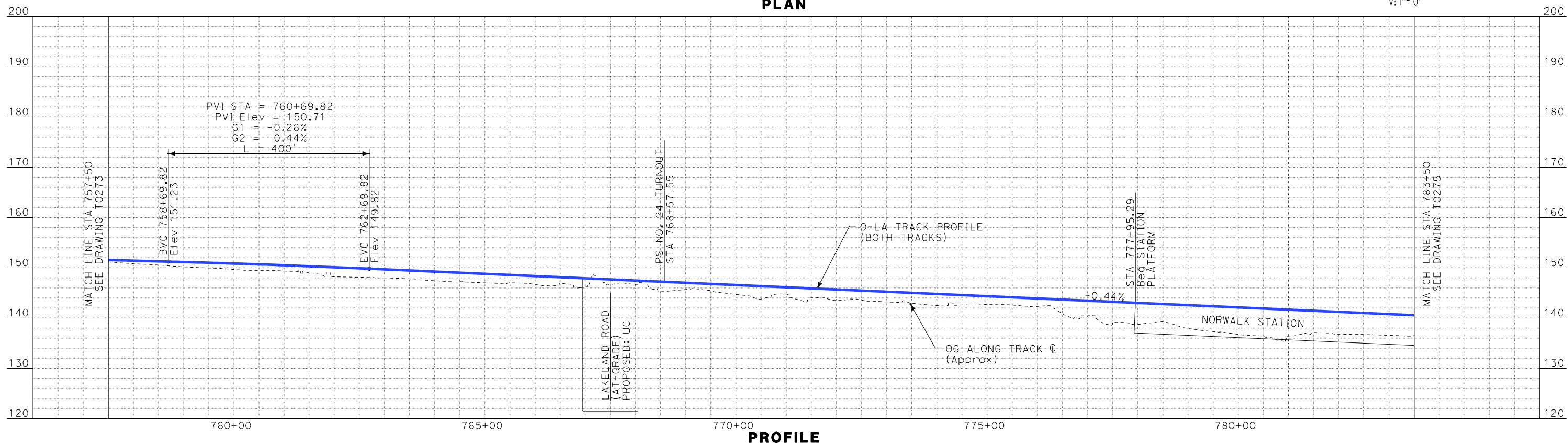
NOT FOR CONSTRUCTION
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100 0 100 200

H: 1"=100'


10 0 10 20

V: 1"=10'



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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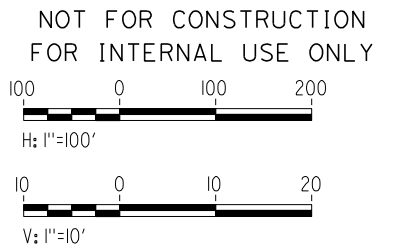
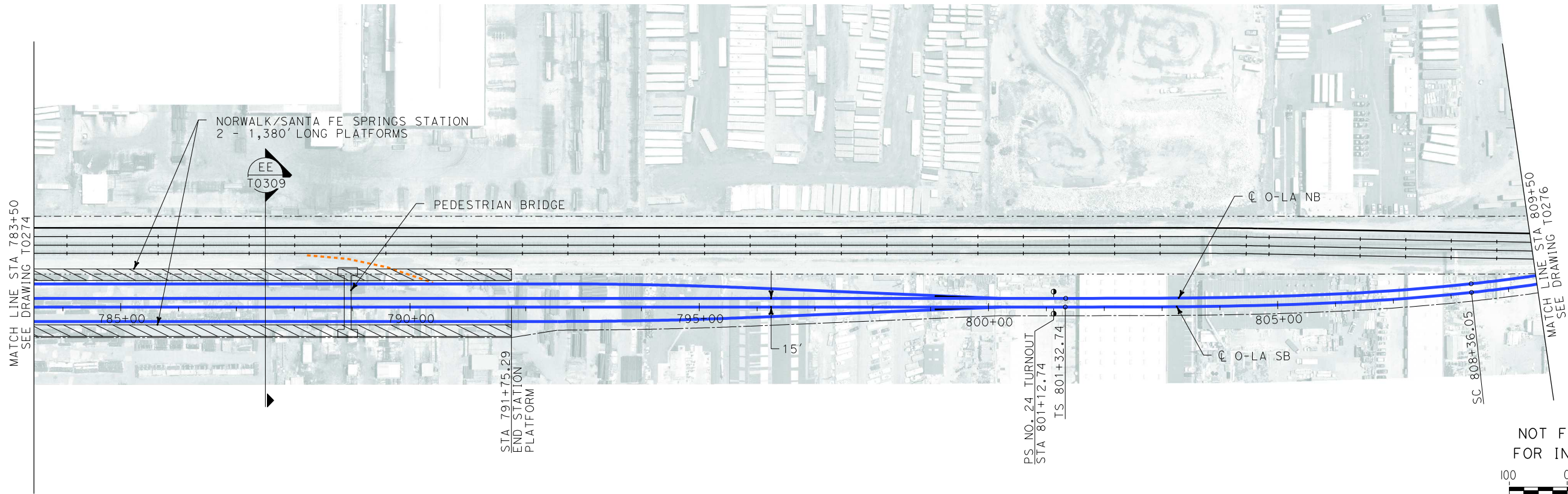
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

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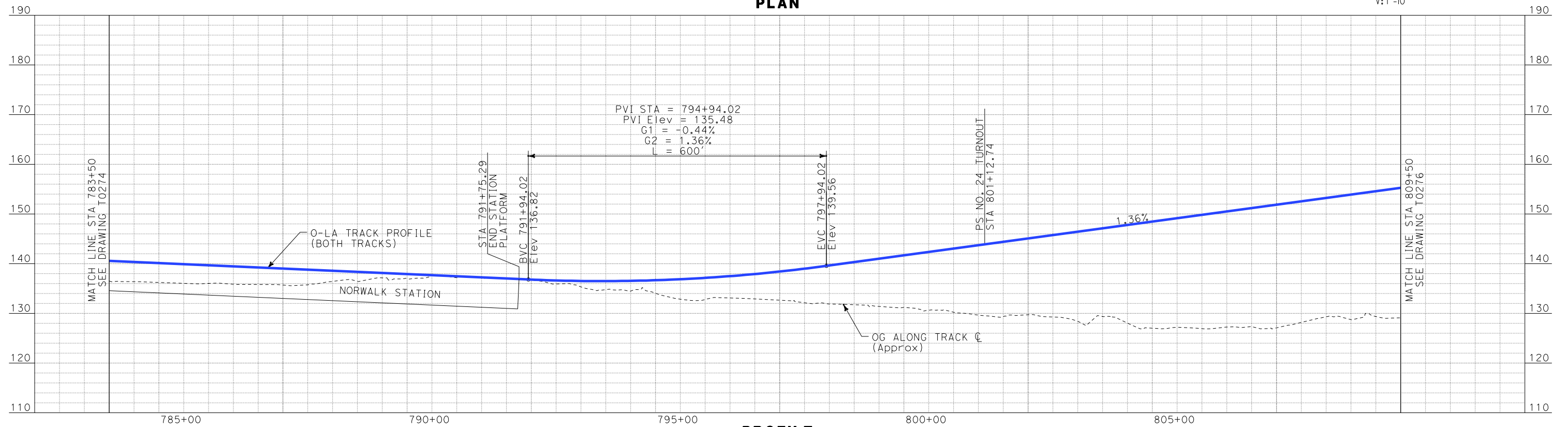
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
NORWALK/SANTA FE SPRINGS STATION
OPTION 1 ALIGNMENT
NORTH OF IMPERIAL HIGHWAY
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0274
SCALE AS SHOWN
SHEET NO. 95 OF 245

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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

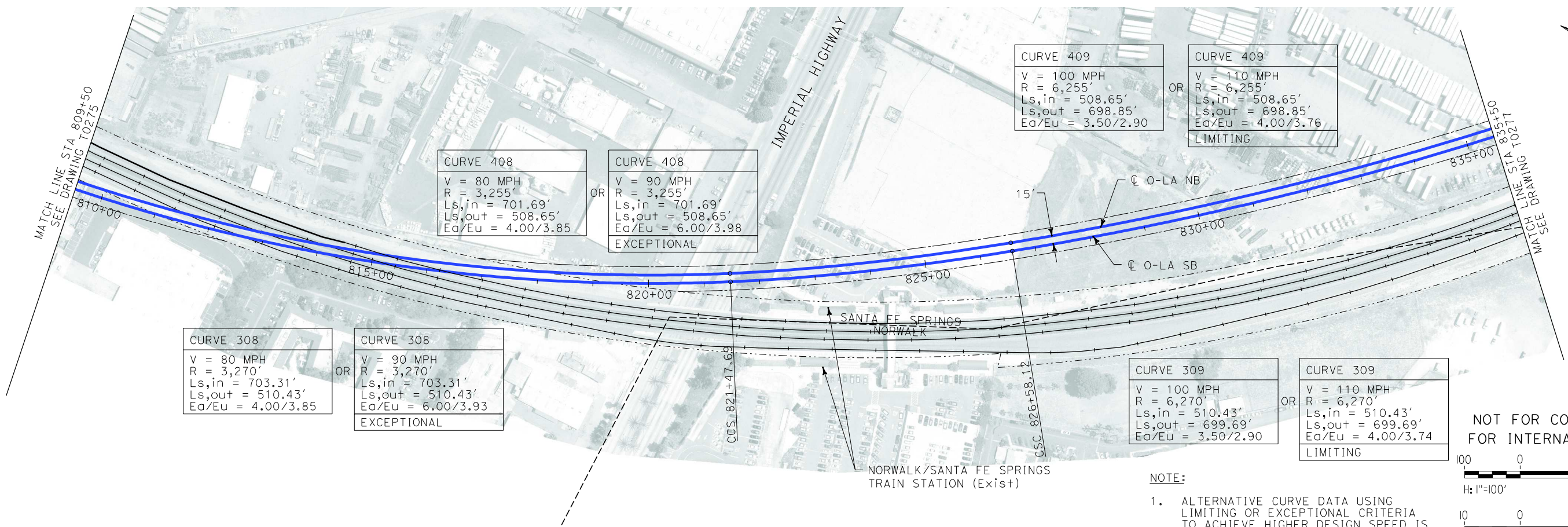
DESIGNED BY K. SPERRY
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ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

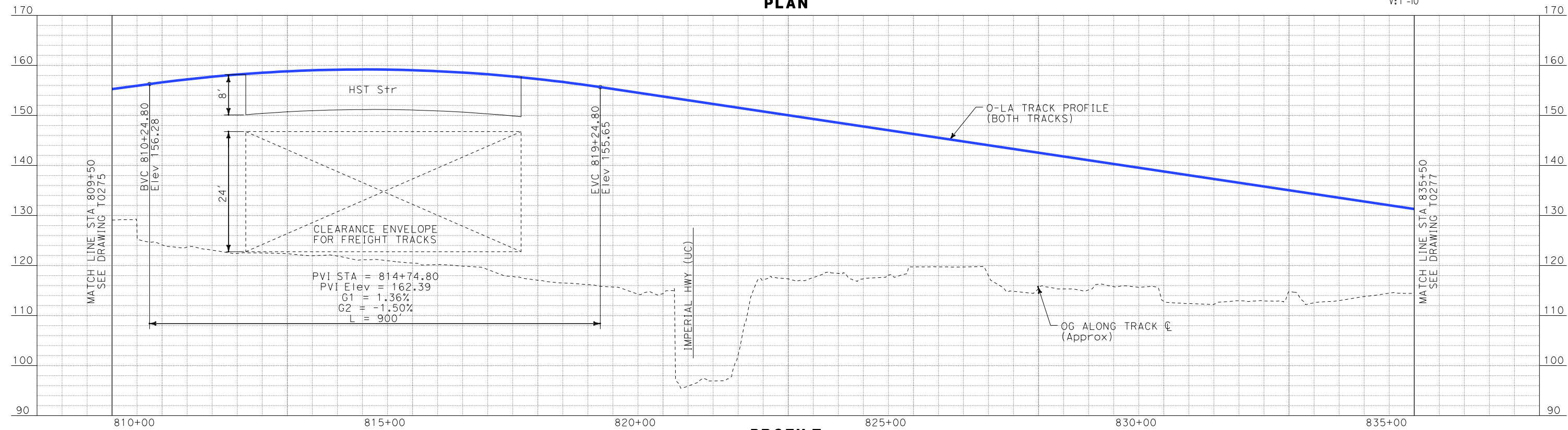
 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
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CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES NORWALK/SANTA FE SPRINGS STATION OPTION 1 ALIGNMENT NORTH OF IMPERIAL HIGHWAY PLAN AND PROFILE	CONTRACT NO. HSR06-0005
	DRAWING NO. T0275
	SCALE AS SHOWN
	SHEET NO. 96 OF 245

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PLAN



PROFILE

NOTE:
1. ALTERNATIVE CURVE DATA USING
LIMITING OR EXCEPTIONAL CRITERIA
TO ACHIEVE HIGHER DESIGN SPEED IS
PRESENTED FOR CONSIDERATION.

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100 0 100 200
H: 1"=100'

10 0 10 20
V: 1"=10'

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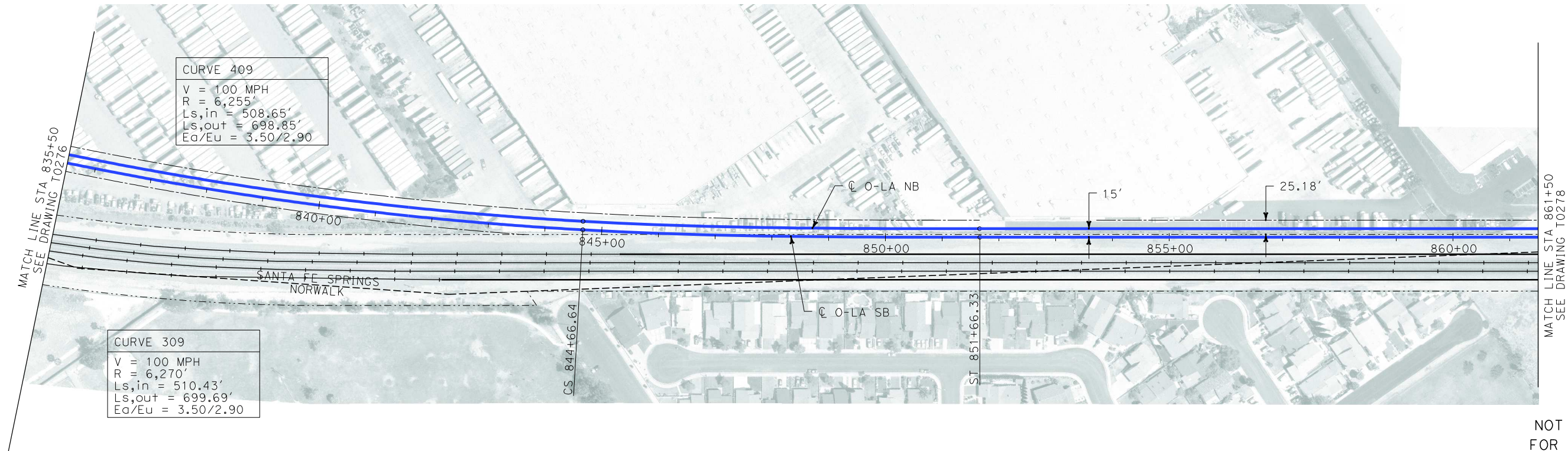
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LOS ANGELES, CA 90017-2556

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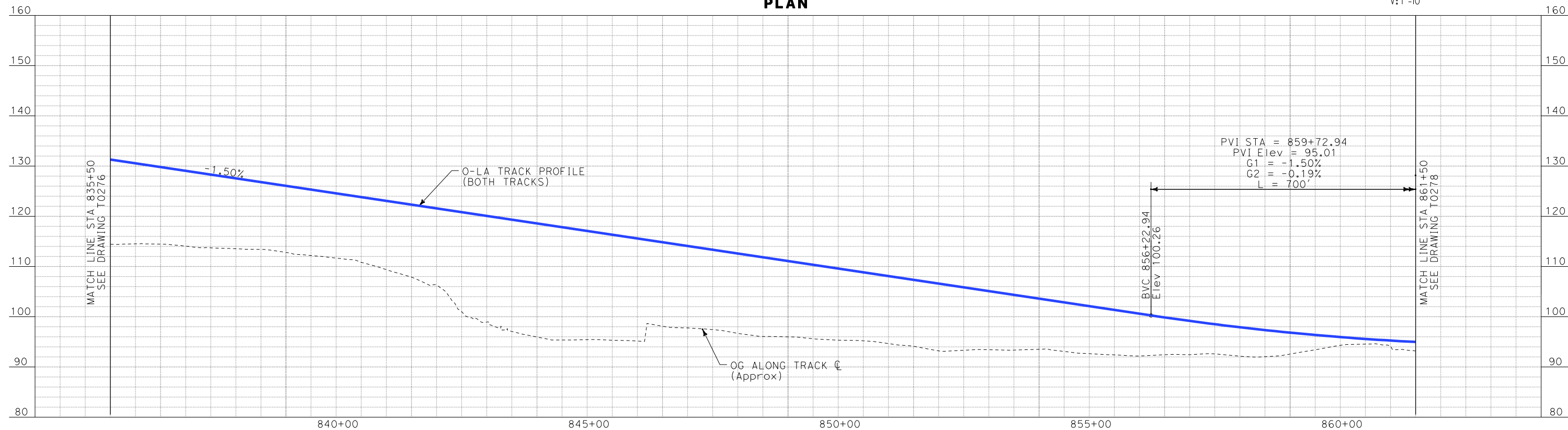
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
NORWALK/SANTA FE SPRING STATION
OPTION 1 ALIGNMENT
NORTH OF IMPERIAL HIGHWAY
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
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SCALE
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SHEET NO.
97 OF 245

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
PLAN



PROFILE

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

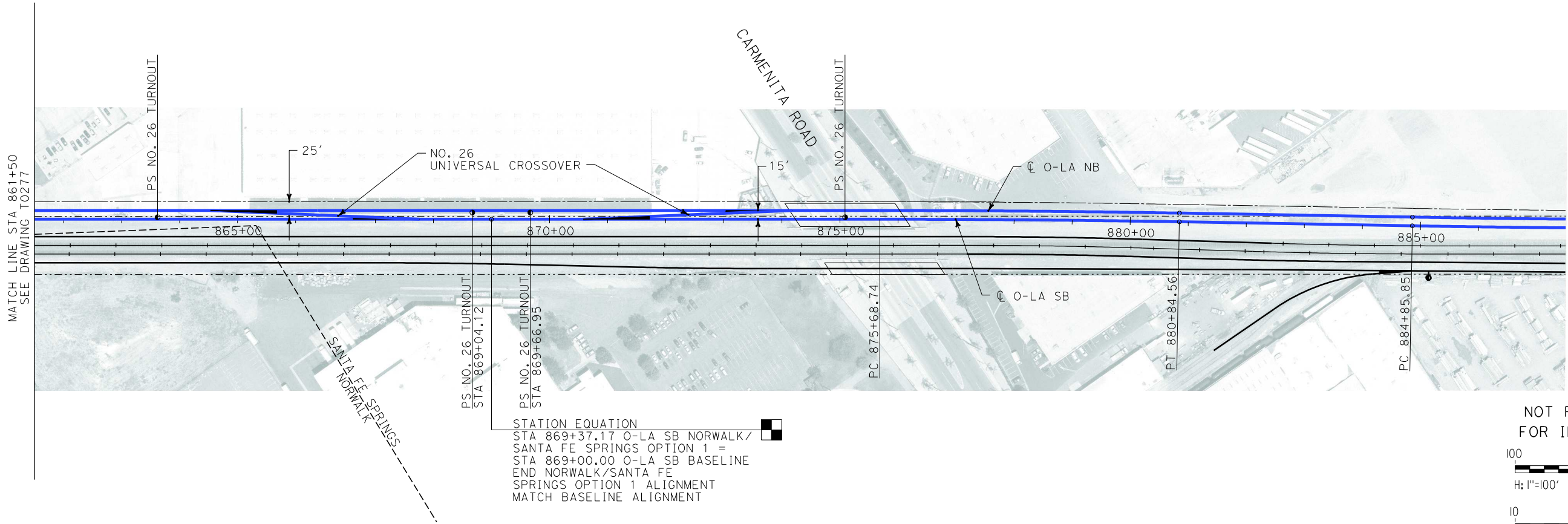


CALIFORNIA HIGH SPEED RAIL AUTHORITY
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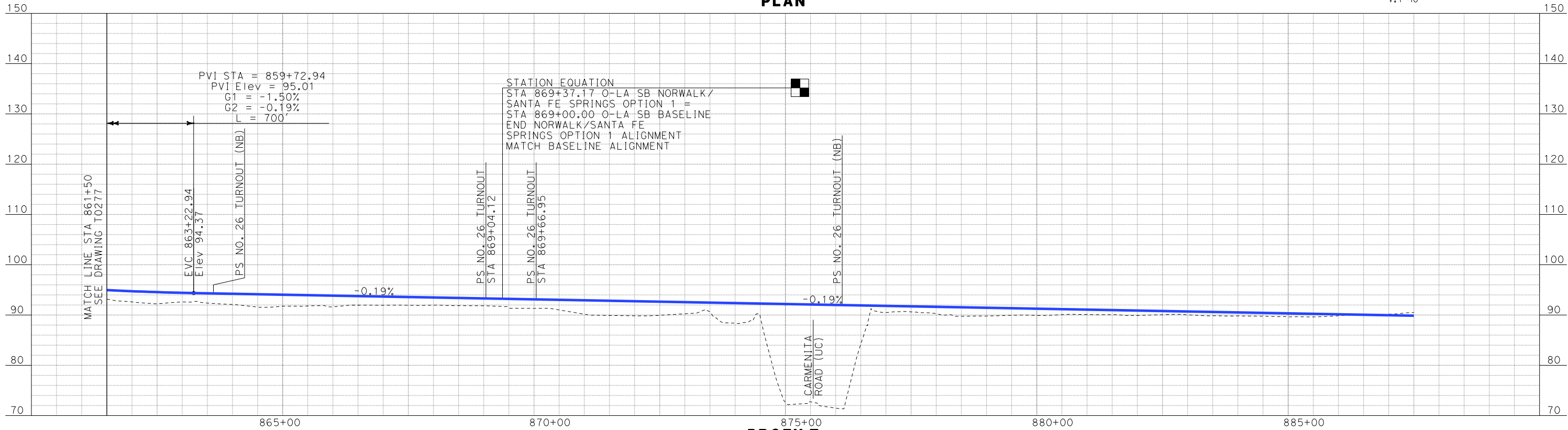
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
NORWALK/SANTA FE SPRINGS STATION
OPTION 1 ALIGNMENT
NORTH OF IMPERIAL HIGHWAY
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0277
SCALE
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SHEET NO.
98 OF 245

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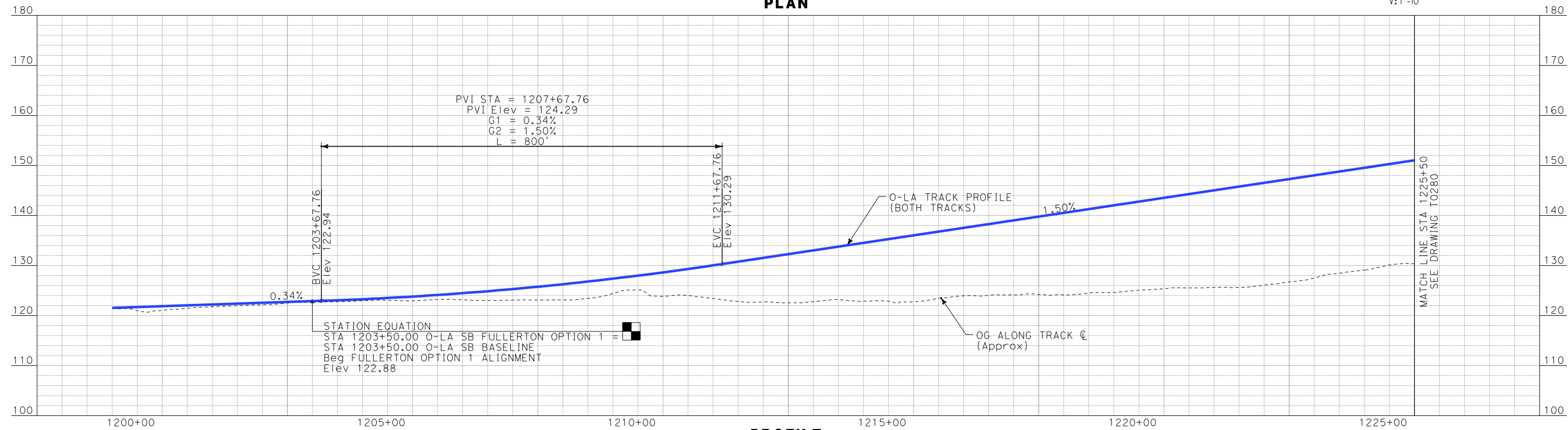
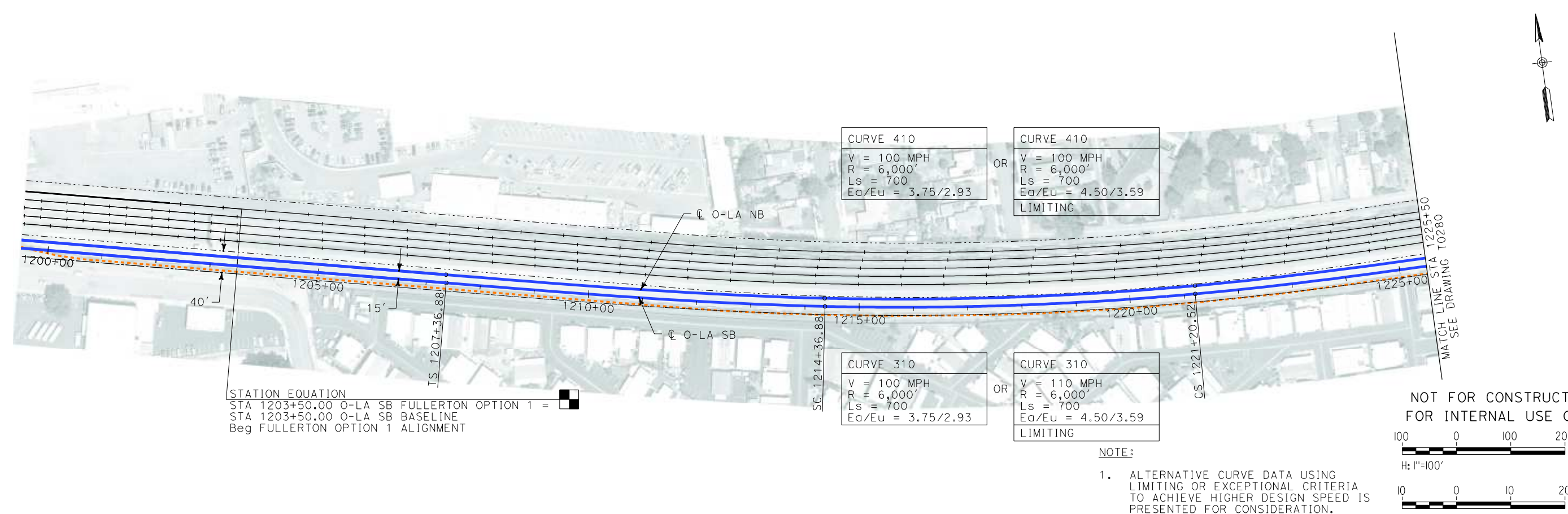
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ORANGE COUNTY TO LOS ANGELES
NORWALK/SANTA FE SPRINGS STATION
OPTION 1 ALIGNMENT
NORTH OF IMPERIAL HIGHWAY
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0278
SCALE AS SHOWN
SHEET NO. 99 OF 245



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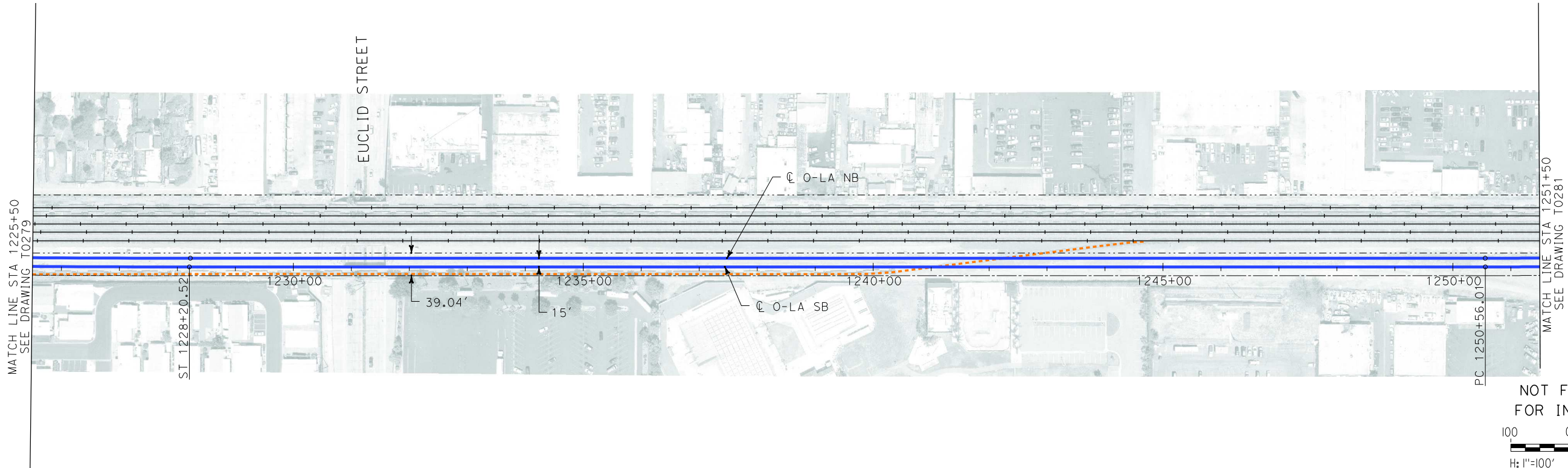
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ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0279
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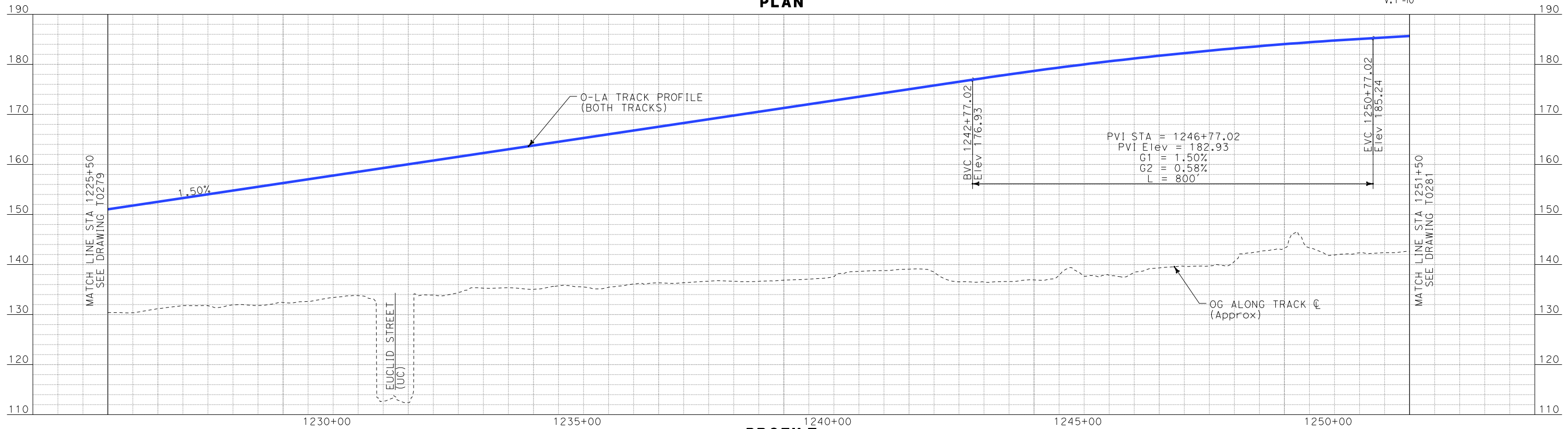
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10 0 10 20

V: 1"=10'


PLAN

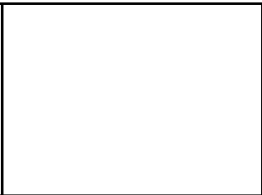


PROFILE

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LOS ANGELES, CA 90017-2556

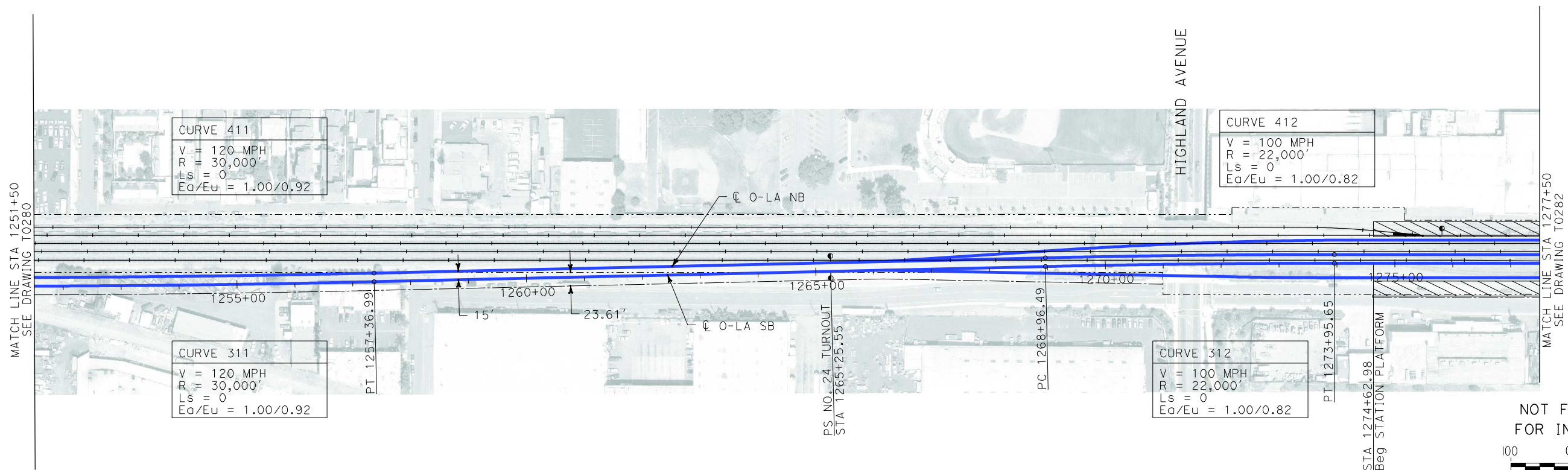


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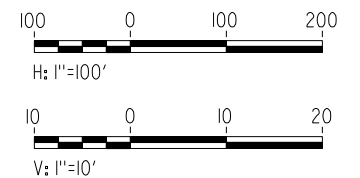
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
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SCALE AS SHOWN
SHEET NO. 101 OF 245

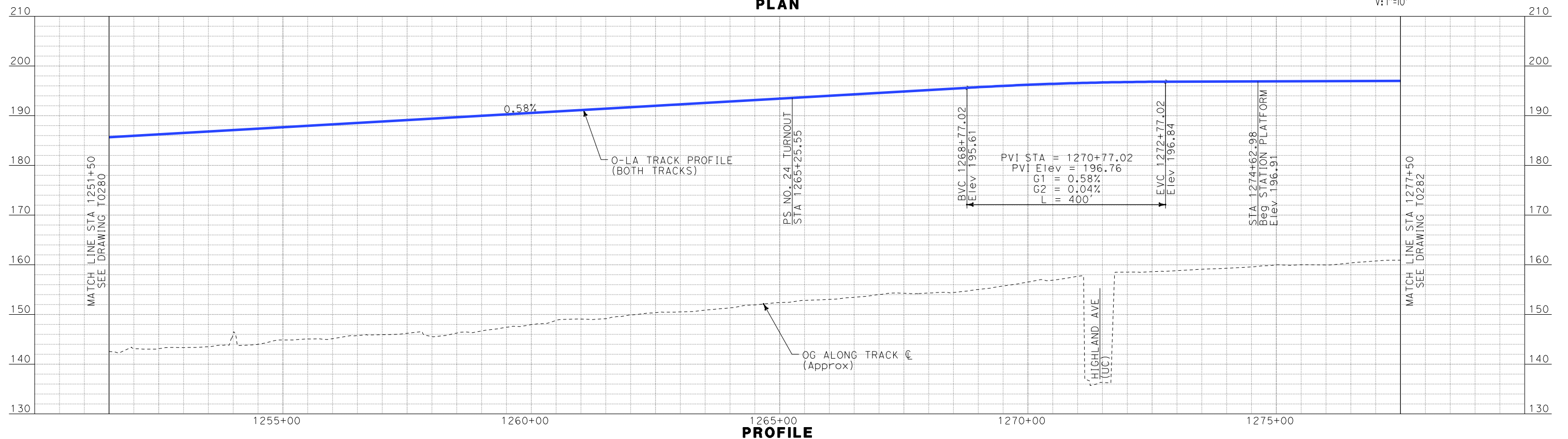
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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k



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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

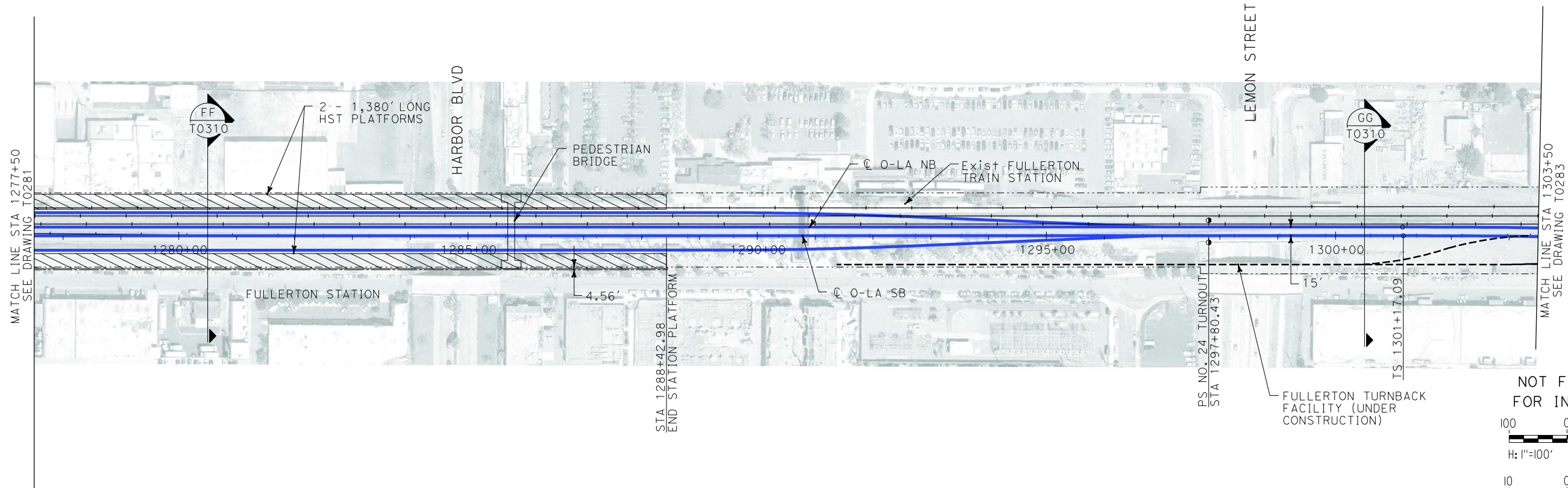
STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



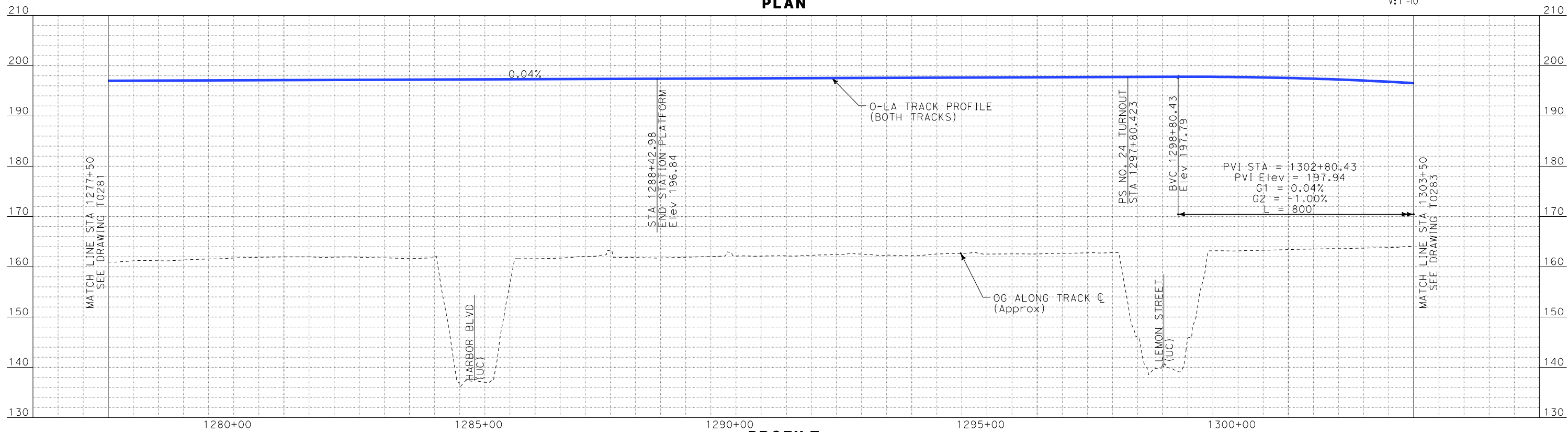
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0281
SCALE AS SHOWN
SHEET NO. 102 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
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PLAN



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
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DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

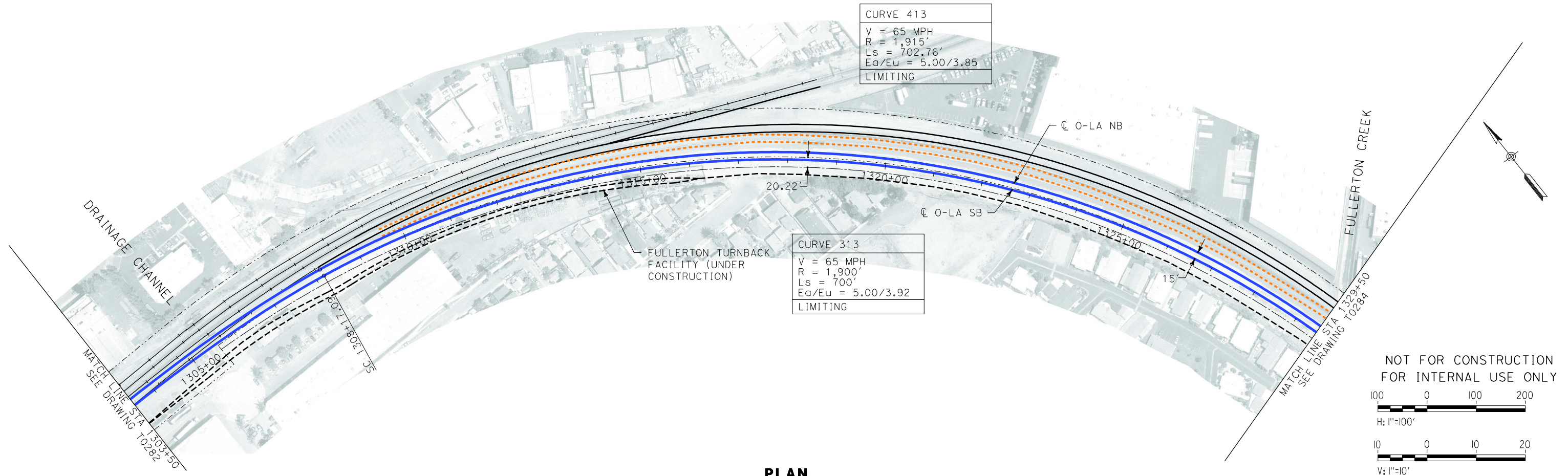


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

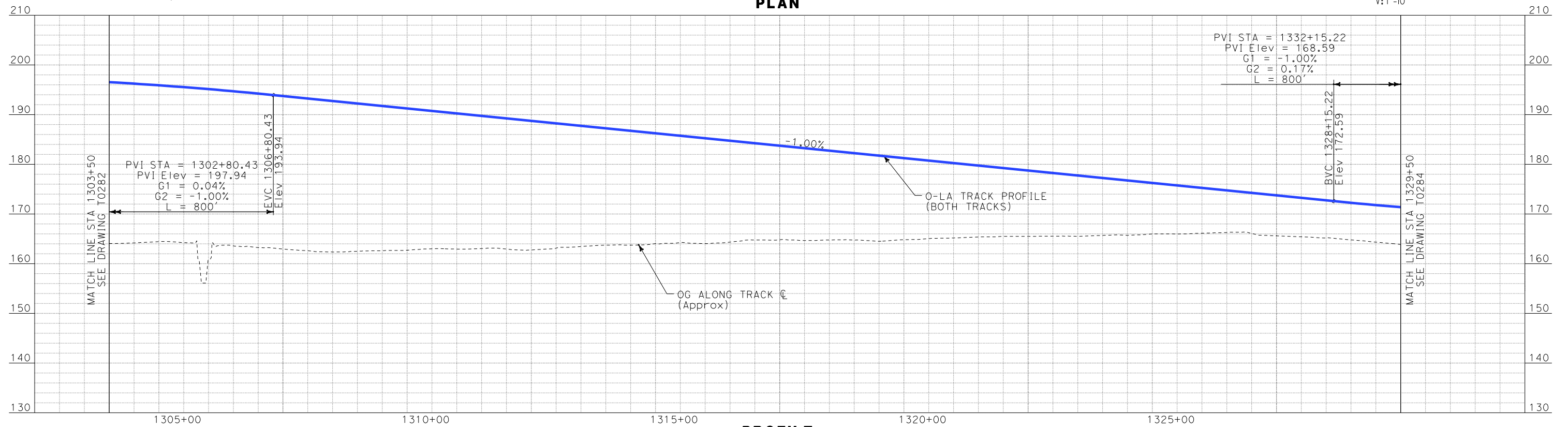
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0282
SCALE AS SHOWN
SHEET NO. 103 OF 245

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Sperry\k




PLAN

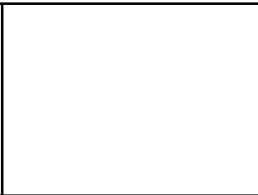


PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

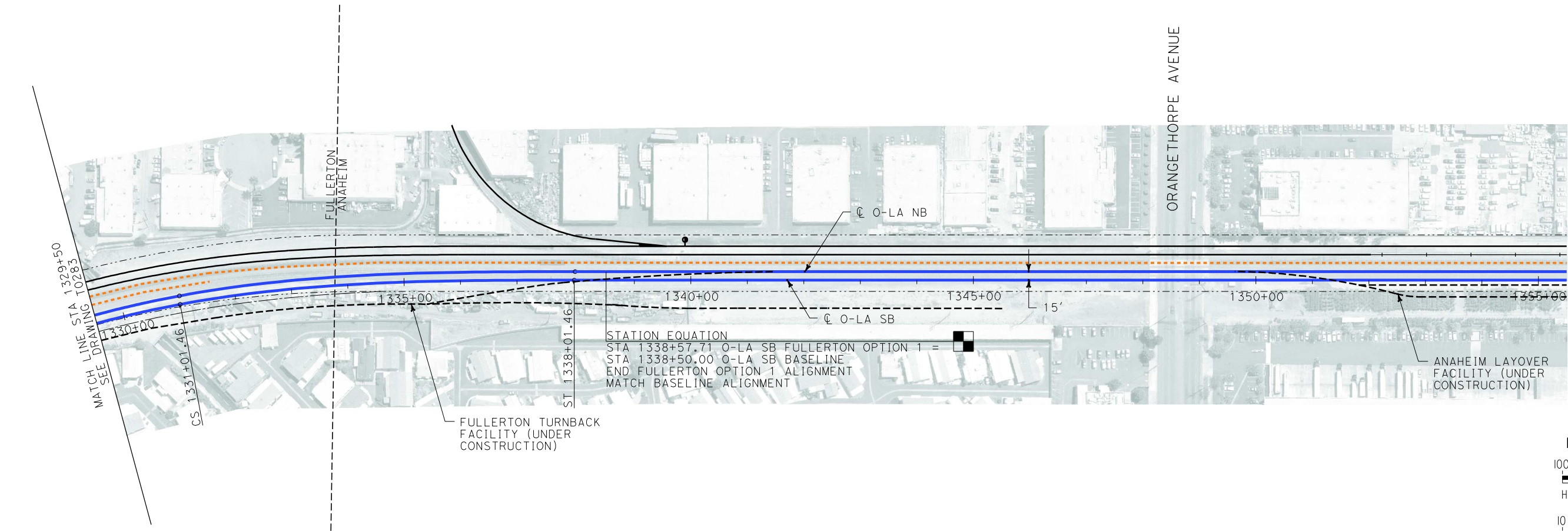
**STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0283
SCALE AS SHOWN
SHEET NO. 104 OF 245

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Sperry\K



PLAN

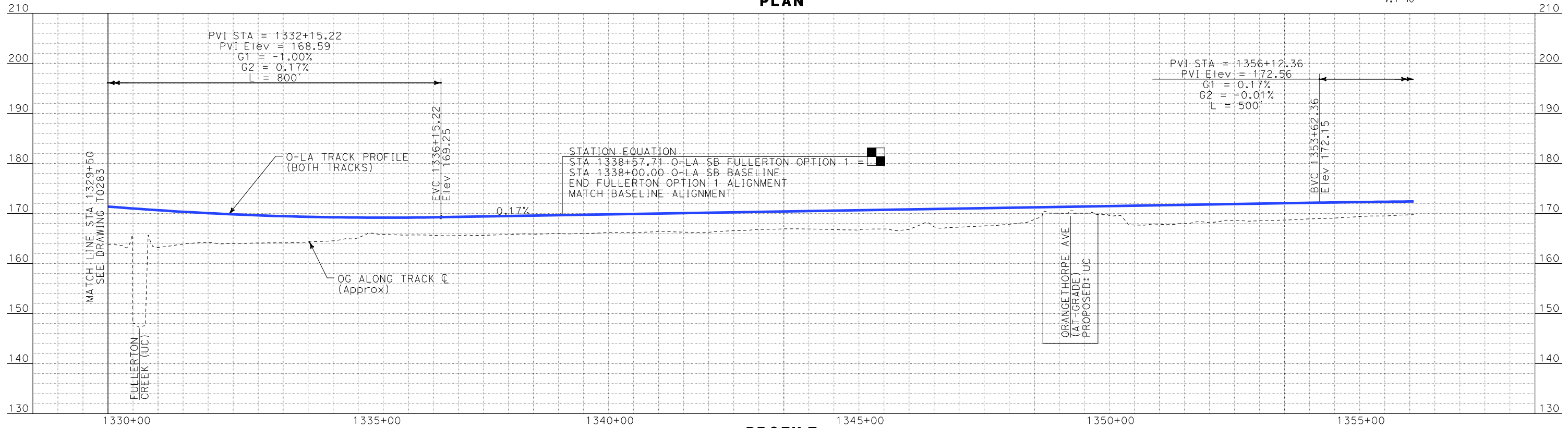
NOT FOR CONSTRUCTION
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H: 1"=100'



V: 1"=10'



PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

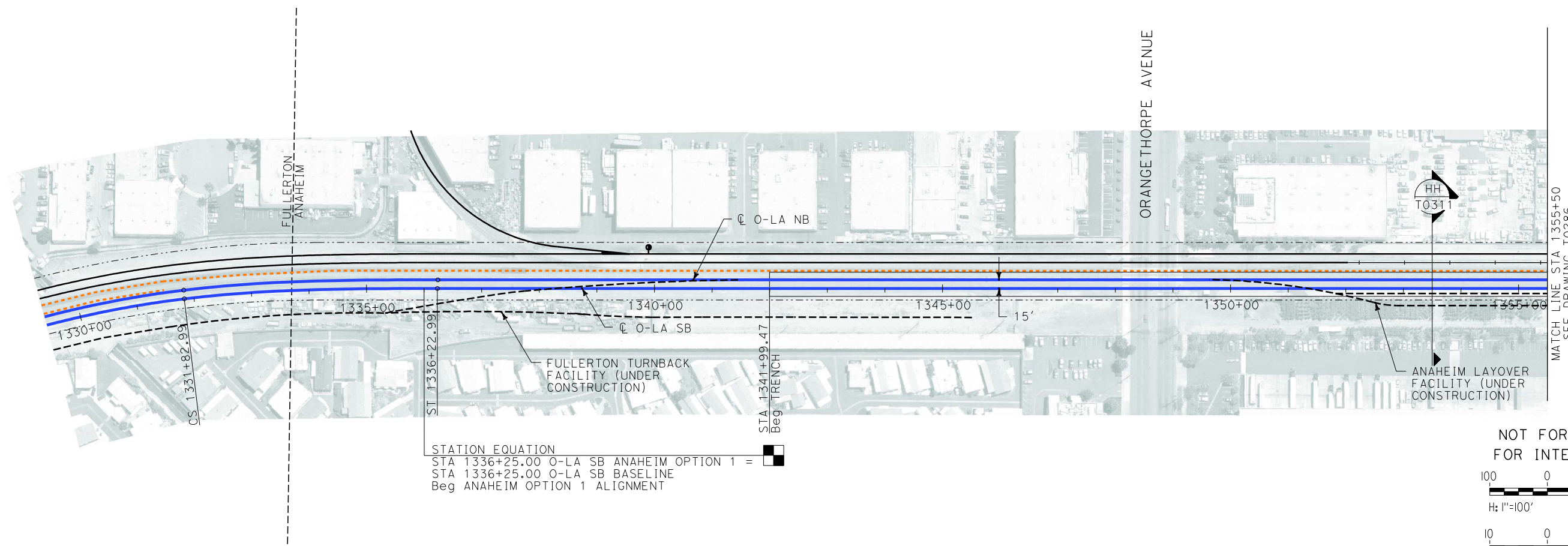
DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

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ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
FULLERTON STATION OPTION 1 ALIGNMENT
AERIAL STATION
PLAN AND PROFILE

CONTRACT NO.	HSR06-0005
DRAWING NO.	T0284
SCALE	AS SHOWN
SHEET NO.	105 OF 245



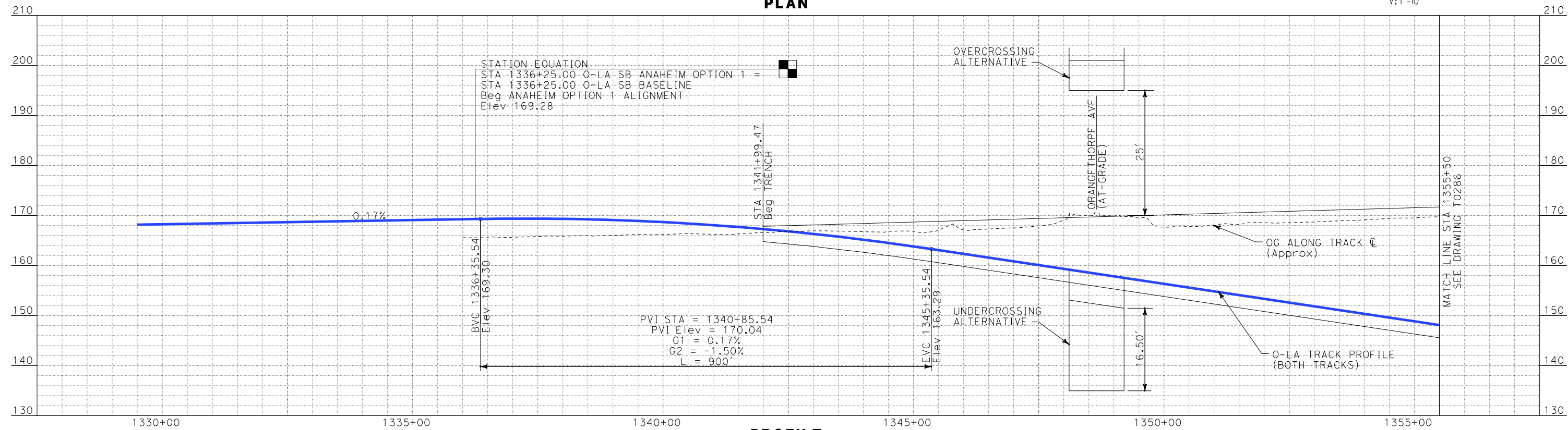
STATION EQUATION
STA 1336+25.00 O-LA SB ANAHEIM OPTION 1 =
STA 1336+25.00 O-LA SB BASELINE
Beg ANAHEIM OPTION 1 ALIGNMENT

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100 0 100 200
H: 1"=100'

10 0 10 20
V: 1"=10'

PLAN



STATION EQUATION
STA 1336+25.00 O-LA SB ANAHEIM OPTION 1 =
STA 1336+25.00 O-LA SB BASELINE
Beg ANAHEIM OPTION 1 ALIGNMENT
Elev 169.28


PVI STA = 1340+85.54
PVI Elev = 170.04
G1 = 0.17%
G2 = -1.50%
L = 900'

PROFILE

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

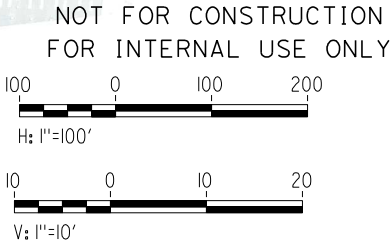
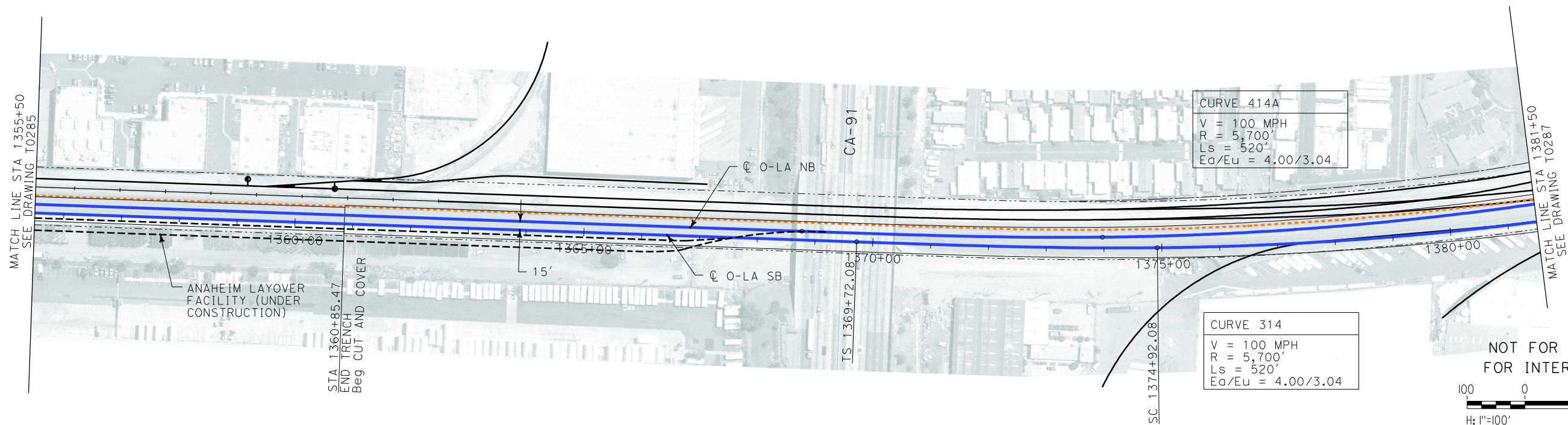


**FLY CALIFORNIA**
Without ever leaving the ground.

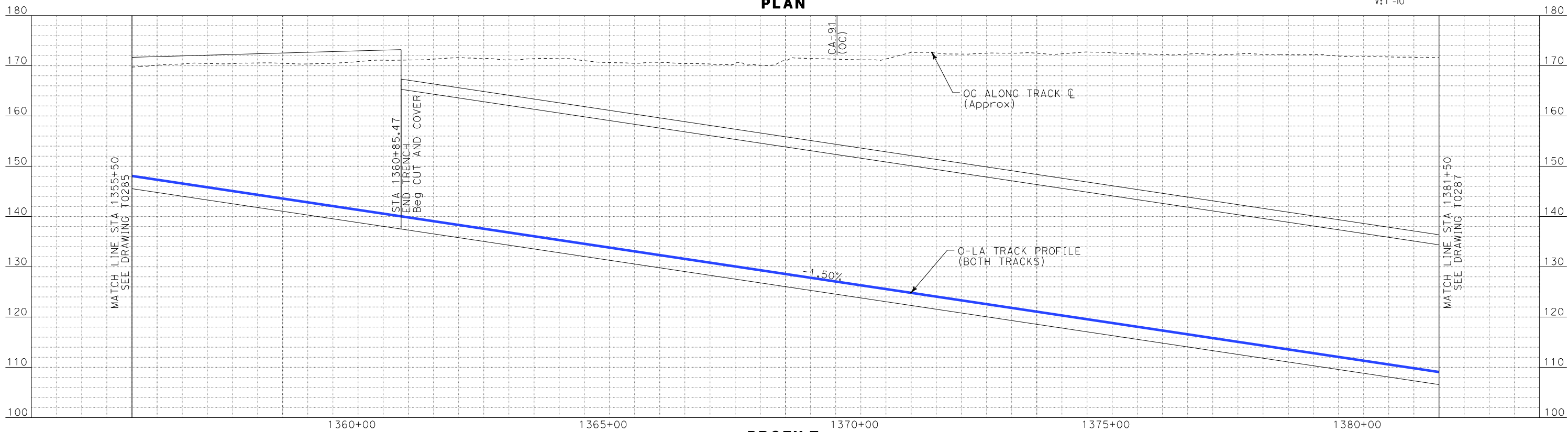
CALIFORNIA HIGH SPEED RAIL AUTHORITY

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0285
SCALE
AS SHOWN
SHEET NO.
106 OF 245



PLAN



PROFILE

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



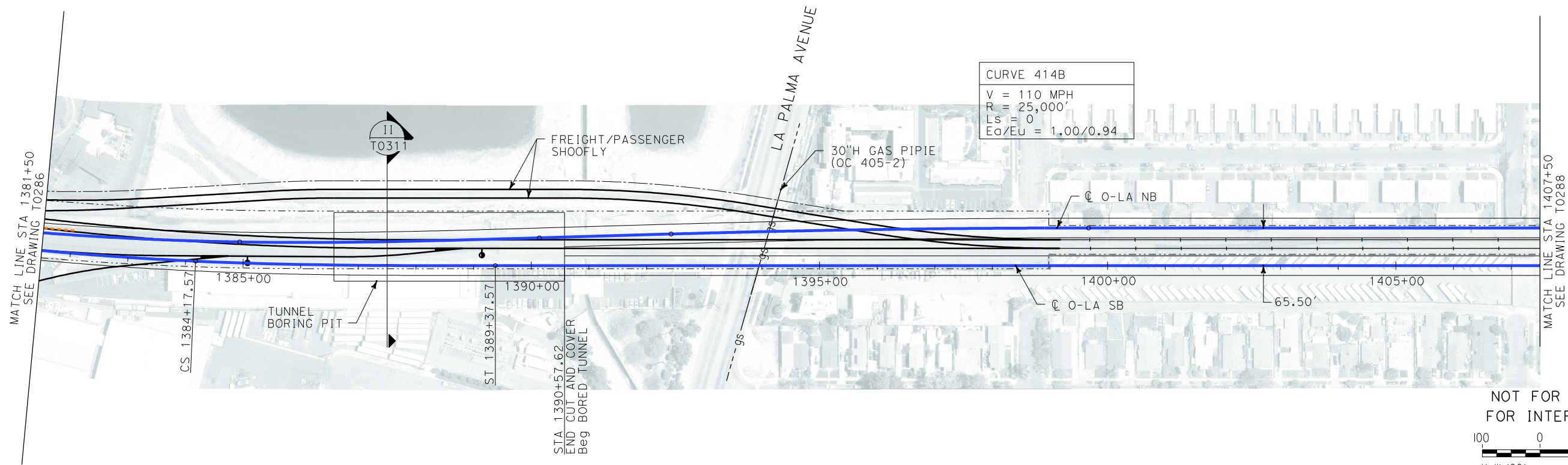
STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



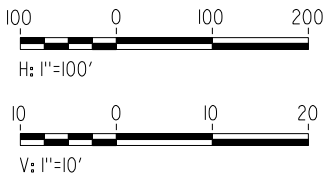
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50'R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

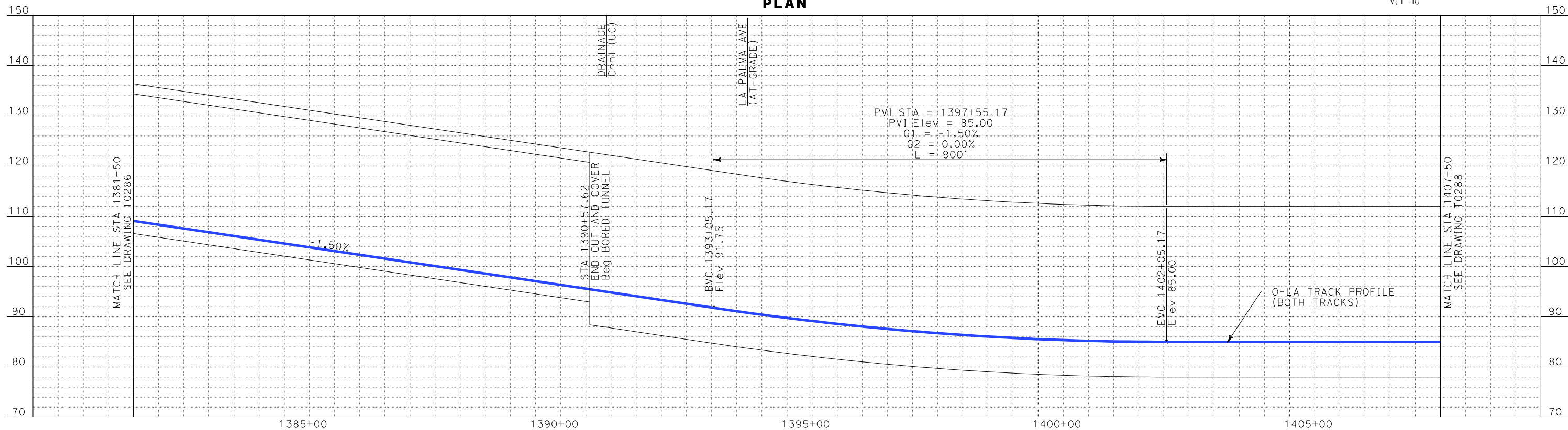
CONTRACT NO.
HSR06-0005
DRAWING NO.
T0286
SCALE
AS SHOWN
SHEET NO.
107 OF 245



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PLAN




PROFILE

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\$\$\$\$\$SYTIME\$\$\$\$\$
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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

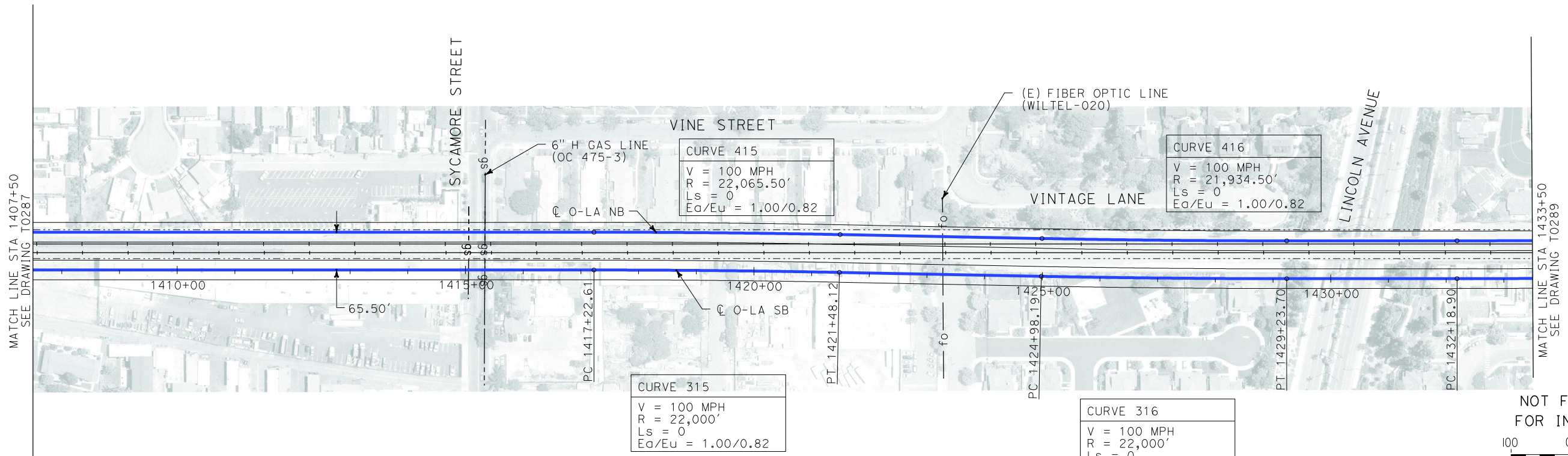


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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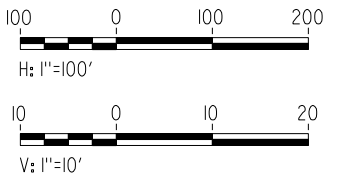
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0287
SCALE
AS SHOWN
SHEET NO.
108 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$
Sperry\k



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

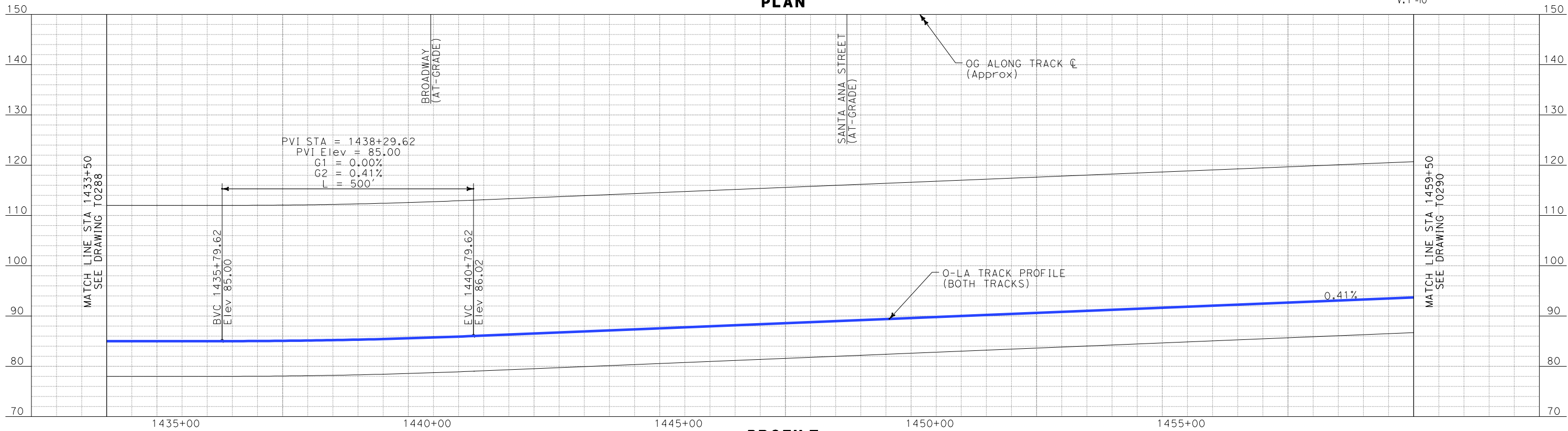
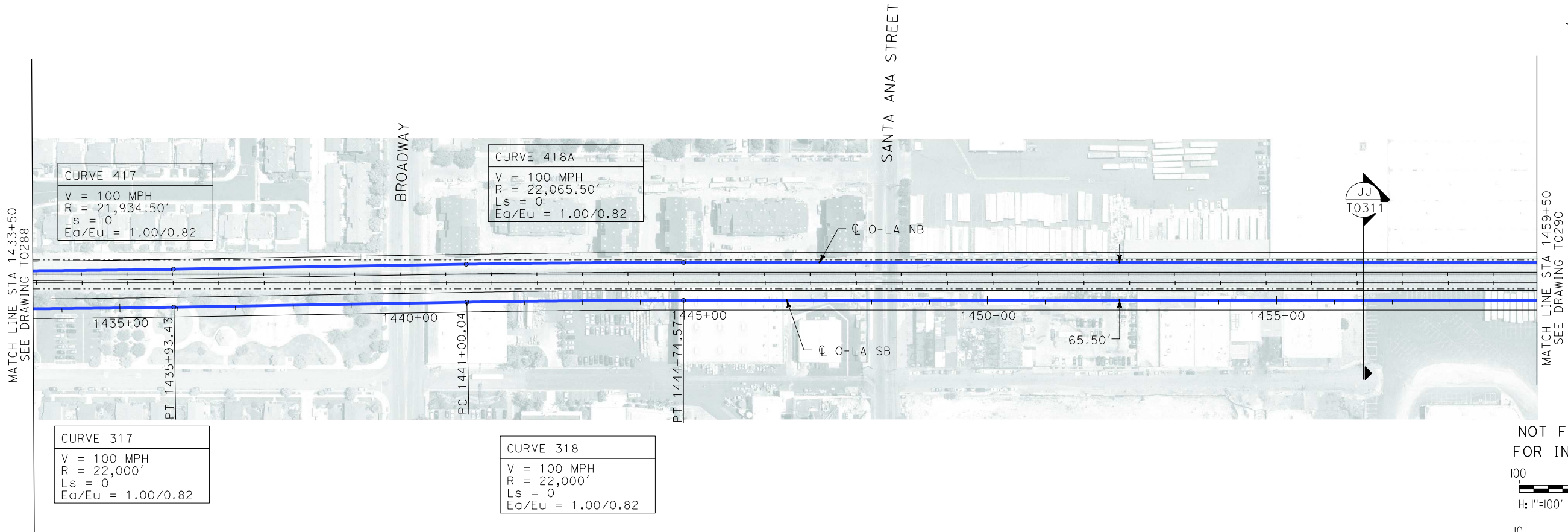
 **STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE


CONTRACT NO. HSR06-0005
DRAWING NO. T0288
SCALE AS SHOWN
SHEET NO. 109 OF 245

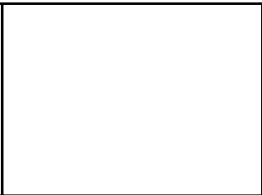
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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

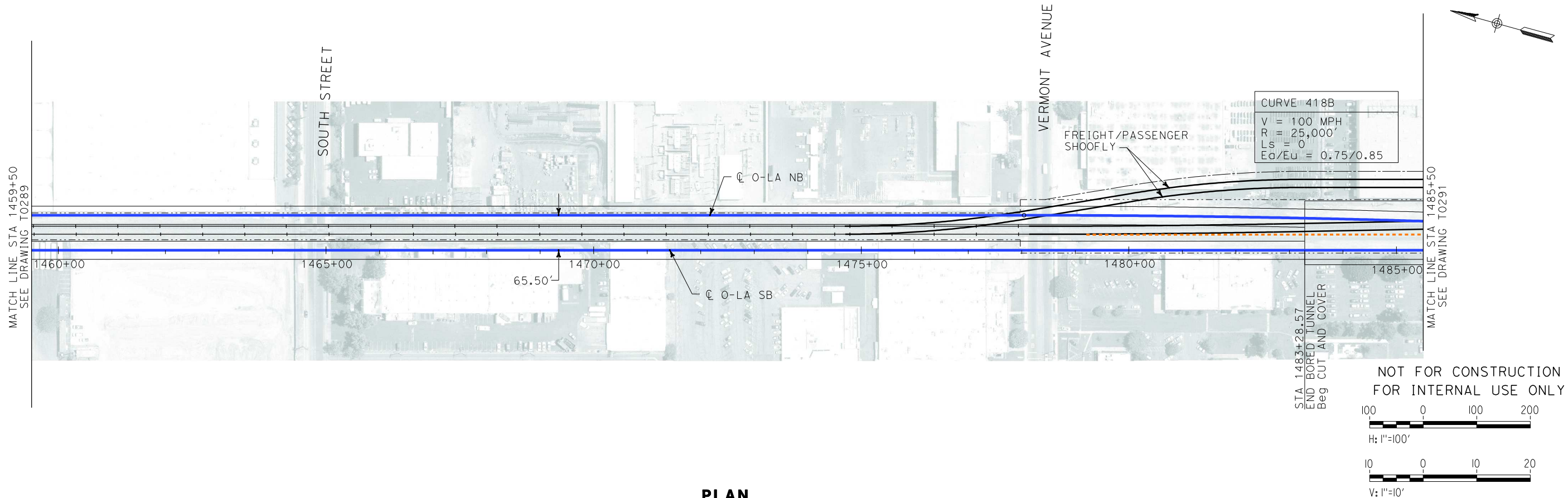


 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
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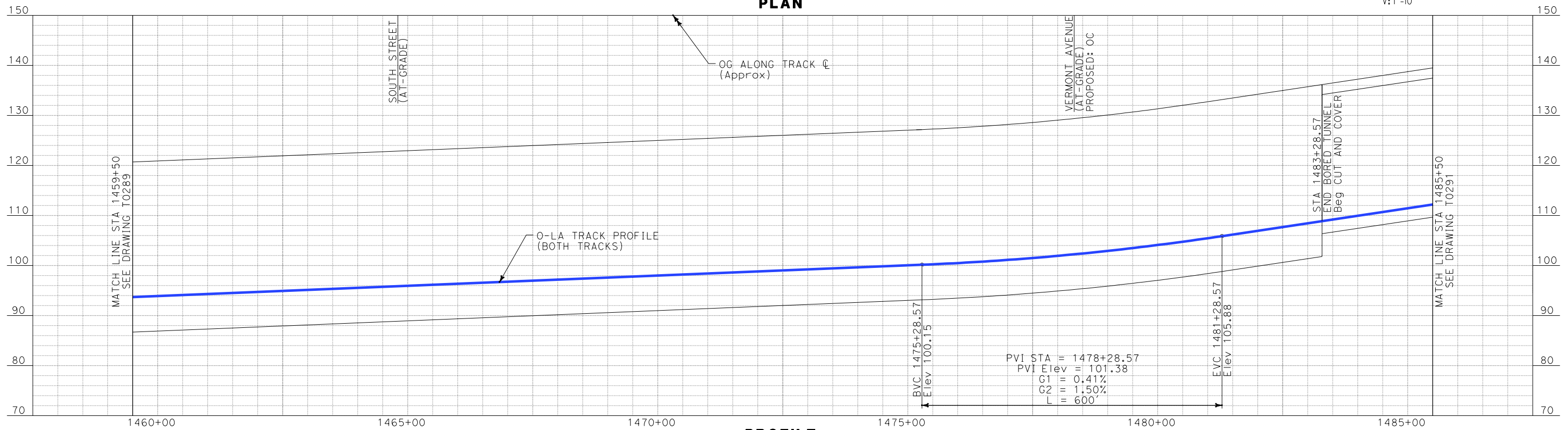
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0289
SCALE AS SHOWN
SHEET NO. 110 OF 245

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PLAN



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DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

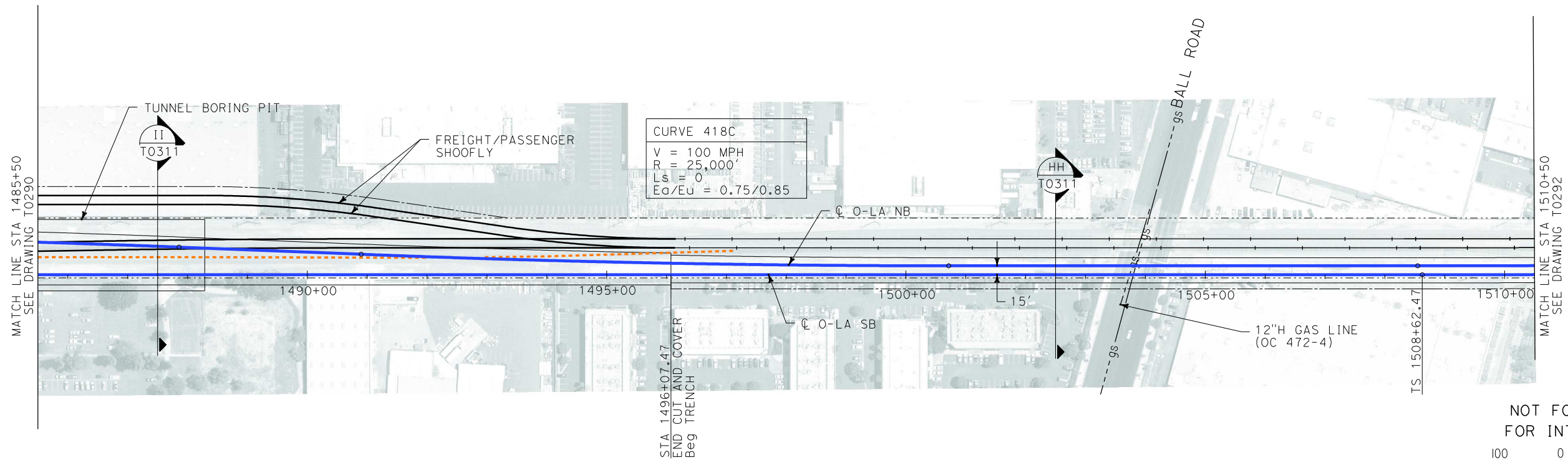
**STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

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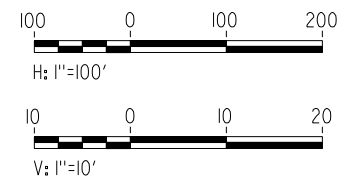
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0290
SCALE AS SHOWN
SHEET NO. 111 OF 245

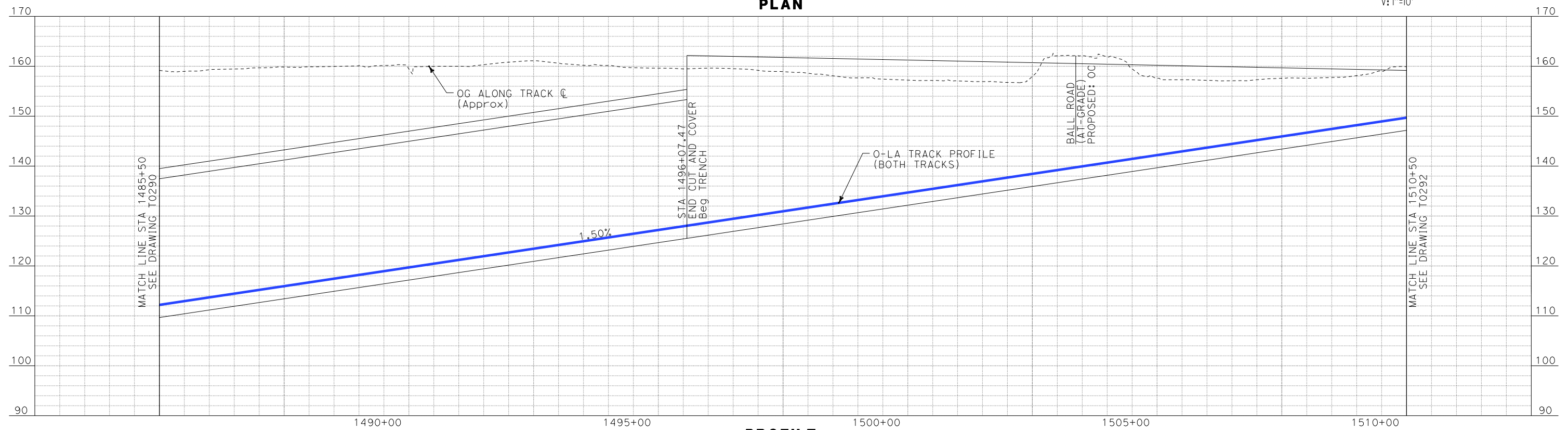
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PROFILE

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY K. SPERRY
DRAWN BY K. SPERRY
CHECKED BY A. SOKOL
IN CHARGE D. THOMSON
DATE 02-13-09

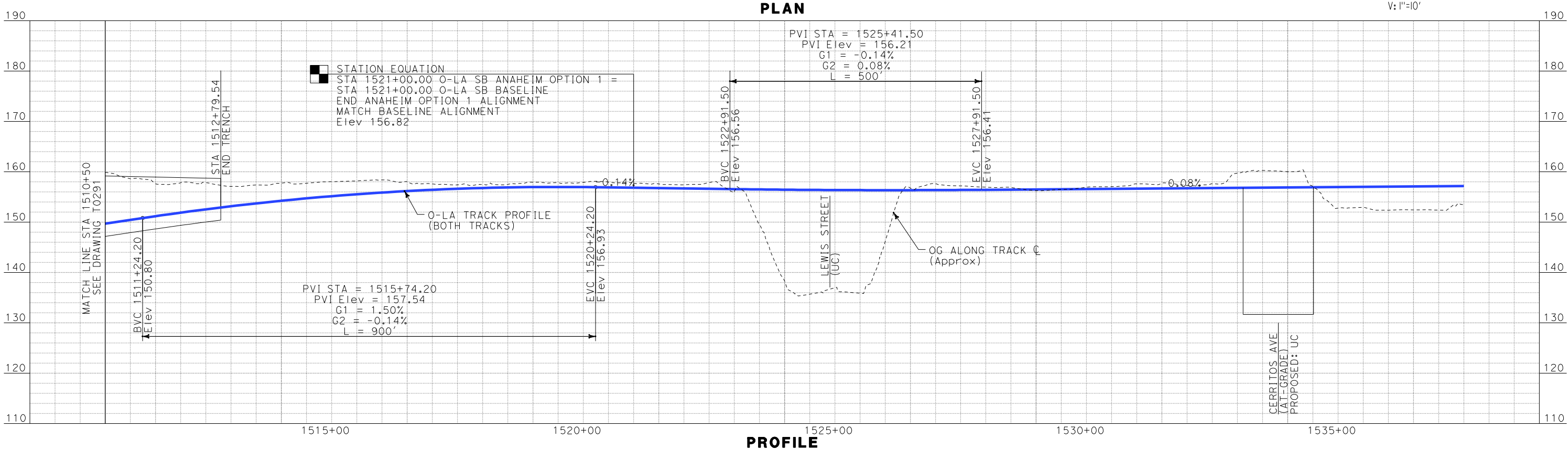
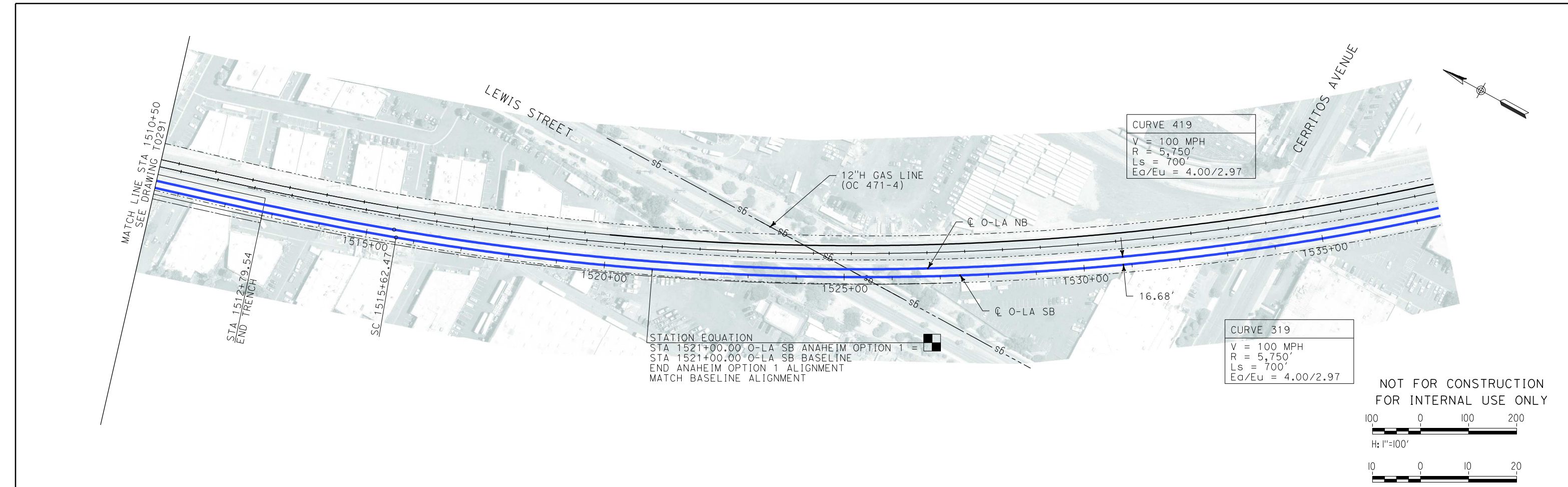
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO. HSR06-0005
DRAWING NO. T0291
SCALE AS SHOWN
SHEET NO. 112 OF 245

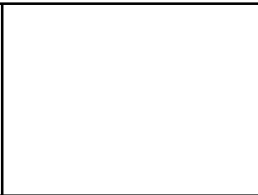
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Sperry\km



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
K. SPERRY
DRAWN BY
K. SPERRY
CHECKED BY
A. SOKOL
IN CHARGE
D. THOMSON
DATE
02-13-09

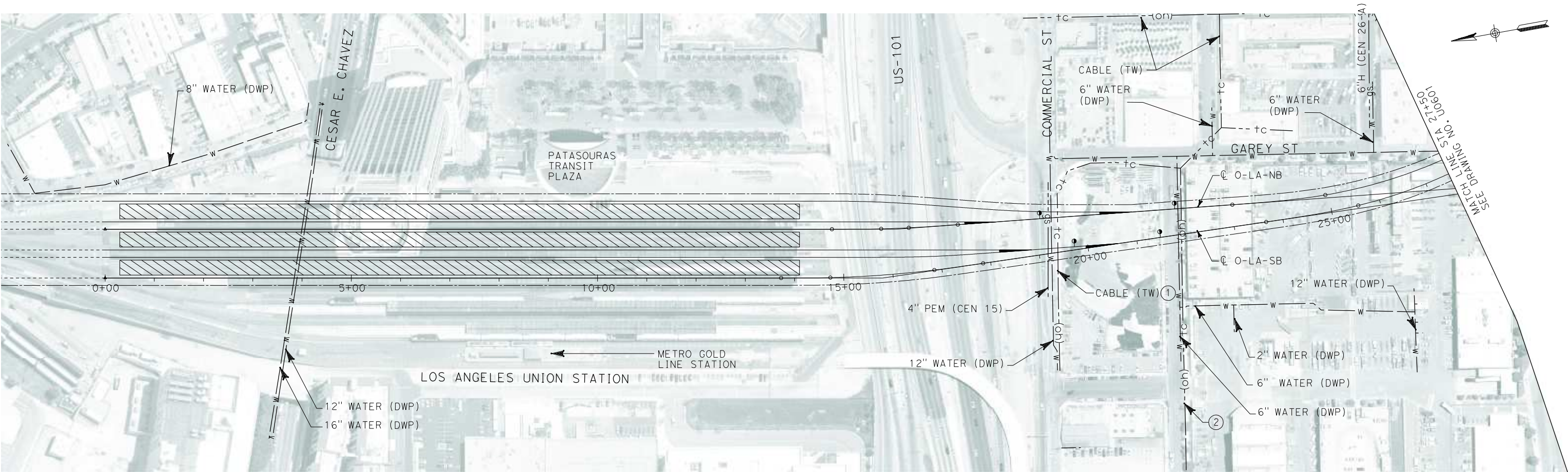
STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ANAHEIM 50' R/W OPTION 1 ALIGNMENT
BORED TUNNEL
PLAN AND PROFILE

CONTRACT NO.
HSR06-0005
DRAWING NO.
T0292
SCALE
AS SHOWN
SHEET NO.
113 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	TIME WARNER	COMMUNICATION	CABLE	STA 19+85	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
②	TIME WARNER	COMMUNICATION	CABLE	STA 22+00	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	

LEGEND

- gs--- GAS LINE
- c--- CABLE TV LINE
- w--- WATER LINE
- fo--- FIBER OPTICS LINE
- e--- OVERHEAD ELECTRICAL LINE
- ✕ ELECTRICAL TOWER
- s--- SEWER LINE
- sd--- STORM DRAIN LINE
- o--- FUEL LINE
- tv--- TELE COMMUNICATION LINE
- o--- OIL LINE
- tc--- COMMUNICATION LINE

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1000200

GRAPHIC SCALE

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01.mxd \$\$\$\$\$\$SYTIME\$\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

STV Incorporated

ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

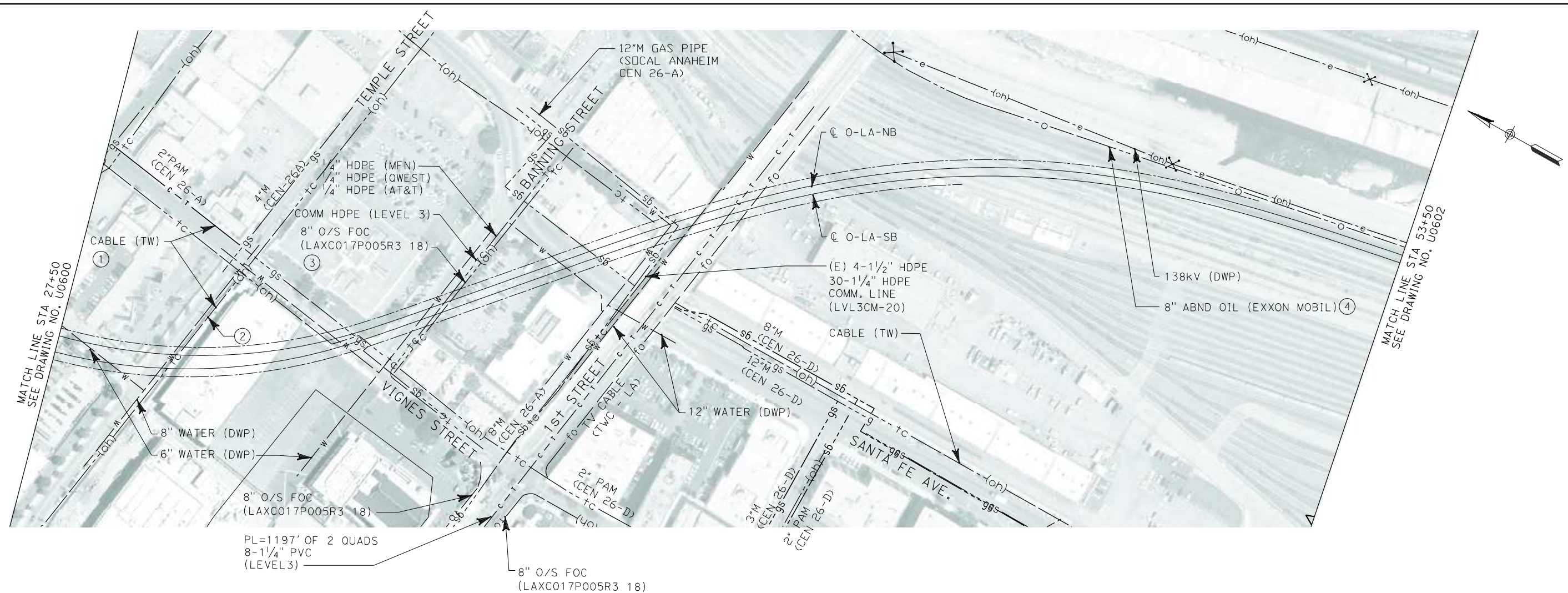
CALIFORNIA HIGH SPEED RAIL AUTHORITY

FLY CALIFORNIA
Without ever leaving the ground.

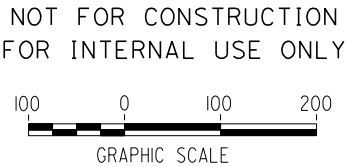
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0600
SCALE 1" = 100'
SHEET NO. 114 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	TIME WARNER	COMMUNICATION	CABLE	STA 29+75	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
②	TIME WARNER	COMMUNICATION	CABLE	STA 33+00	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
③	TIME WARNER	COMMUNICATION	CABLE	STA 35+00	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
④	EXXON MOBIL	OIL	8" ABND OIL	STA 51+30 TO STA 53+50	THE EXISTING OIL LINE IS IN CONFLICT WITH PROPOSED COLUMNS	HIGH RISK	



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REV	DATE	BY	SUB	APP	DESCRIPTION

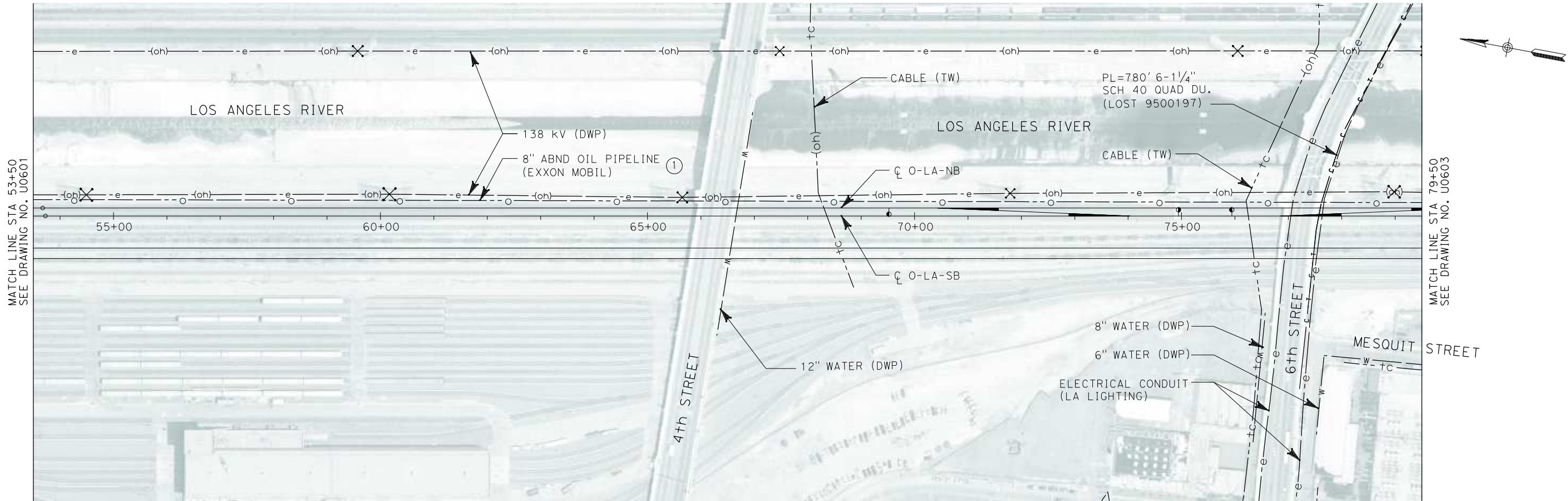
DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



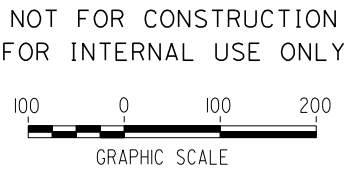
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0601
SCALE 1" = 100'
SHEET NO. 115 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBIL	OIL	8" ABND OIL	STA 53+50 TO STA 63+62	THE EXISTING OIL LINE IS IN CONFLICT WITH PROPOSED COLUMNS	HIGH RISK	



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
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LOS ANGELES, CA 90017-2556



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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

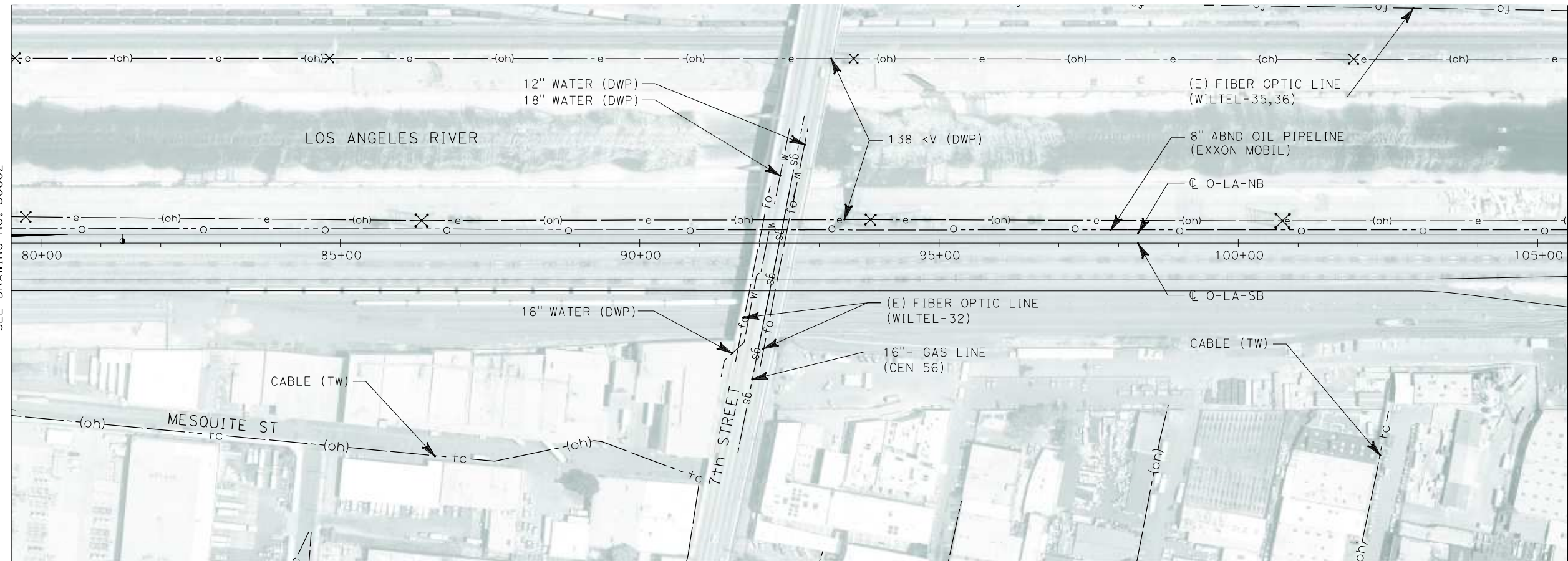
EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0602
SCALE 1" = 100'
SHEET NO. 116 OF 245

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01.mxd

\$\$\$\$\$SYTIME\$\$\$\$\$

MATCH LINE STA 79+50
SEE DRAWING NO. U0602



MATCH LINE STA 105+50
SEE DRAWING NO. U0604



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REV	DATE	BY	SUB	APP	DESCRIPTION

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IN CHARGE D. THOMSON
DATE 02-13-09



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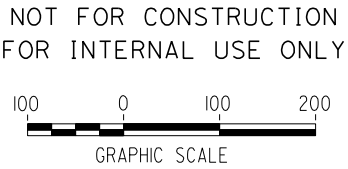
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0603
SCALE 1" = 100'
SHEET NO. 117 OF 245




No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	TRANSMISSION TOWER-138 kV	STA 120+00	THE EXISTING TRANSMISSION TOWER IS TOO CLOSE TO THE PROPOSED NB TRACK	HIGH COST RELOCATION	
②	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138kV	STA 128+75 TO STA 130+87	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED TRACKS, ACCORDING TO CPUC THE MIN. VERTICAL CLEARANCE IS 34 FT. DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED CONFLICT TILL ELEVATION INFORMATION IS RECEIVED.	HIGH COST RELOCATION	



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
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DATE 02-13-09



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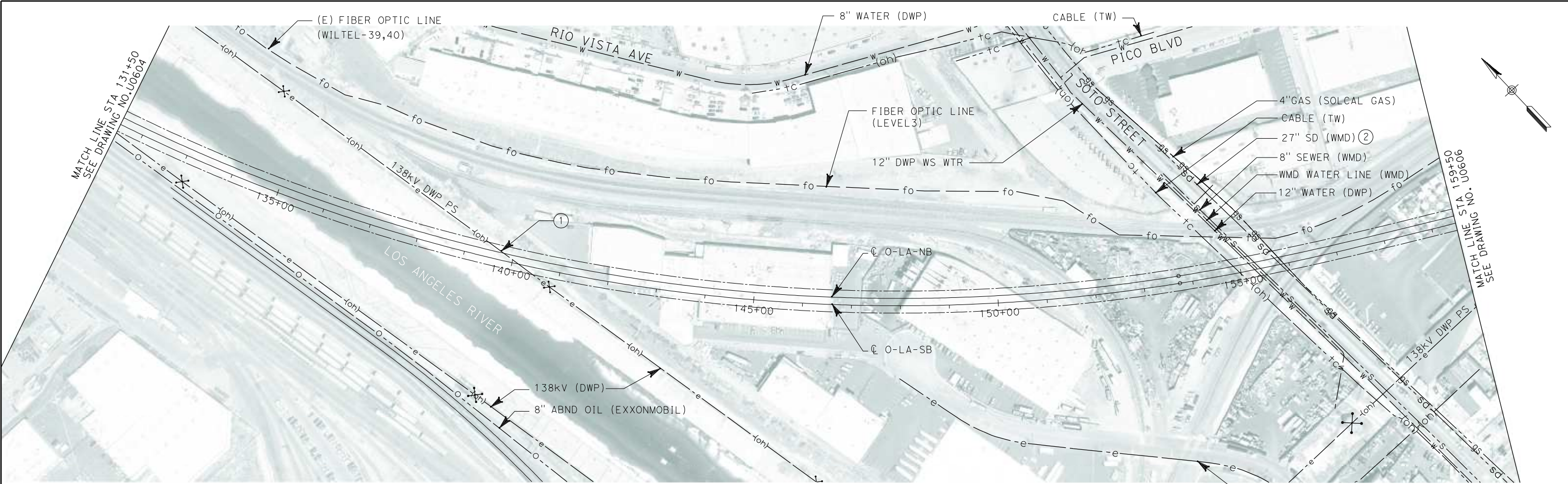


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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0604
SCALE 1" = 100'
SHEET NO. 118 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138kV	STA 139+60 TO STA 140+35	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED TRACKS, ACCORDING TO CPUC THE MIN. VERTICAL CLEARANCE IS 34 FT. DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED CONFLICT TILL ELEVATION INFORMATION IS RECEIVED.	HIGH COST RELOCATION	
②	TIME WARNER	COMMUNICATION	CABLE	STA 155+00	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	

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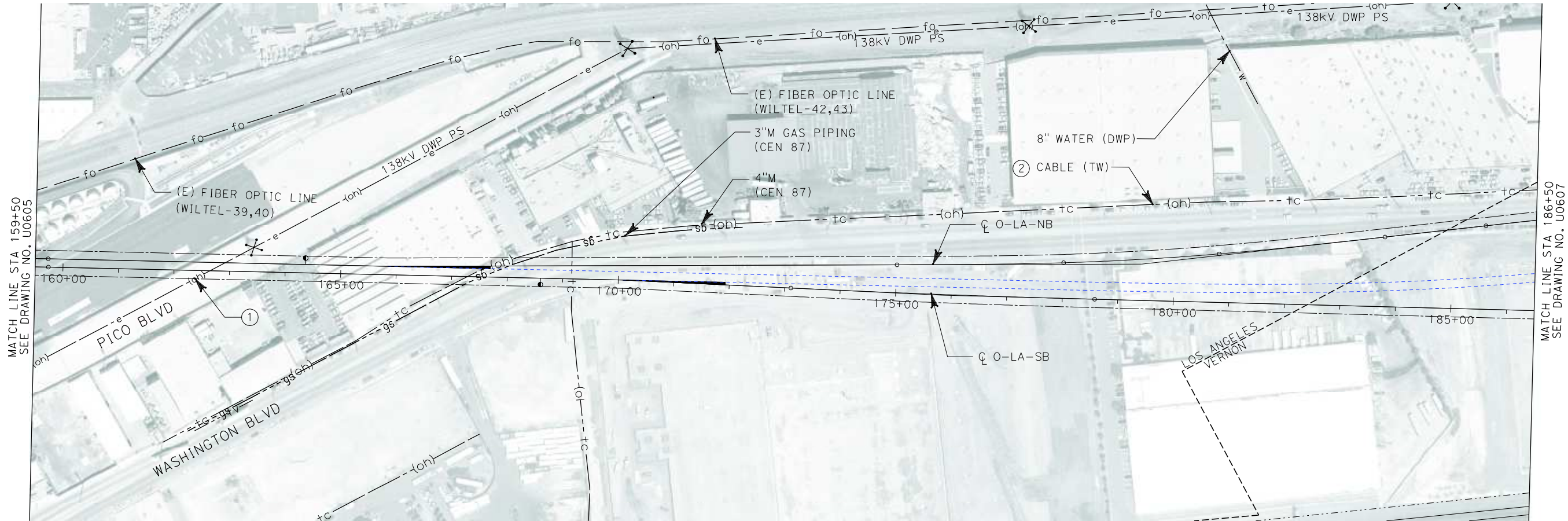
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

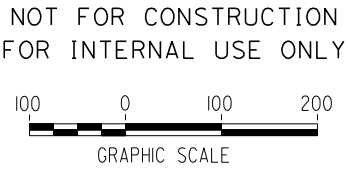
CONTRACT NO. HSR06-0005
DRAWING NO. U0605
SCALE 1" = 100'
SHEET NO. 119 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138kV	PICO BLVD. (STA 163+00)	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED TRACKS, ACCORDING TO CPUC THE MIN. VERTICAL CLEARANCE IS 34 FT. DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	HIGH COST RELOCATION	
②	TIME WARNER	COMMUNICATION	CABLE	STA 167+00	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED TRACKS, DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
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IN CHARGE D. THOMSON
DATE 02-13-09



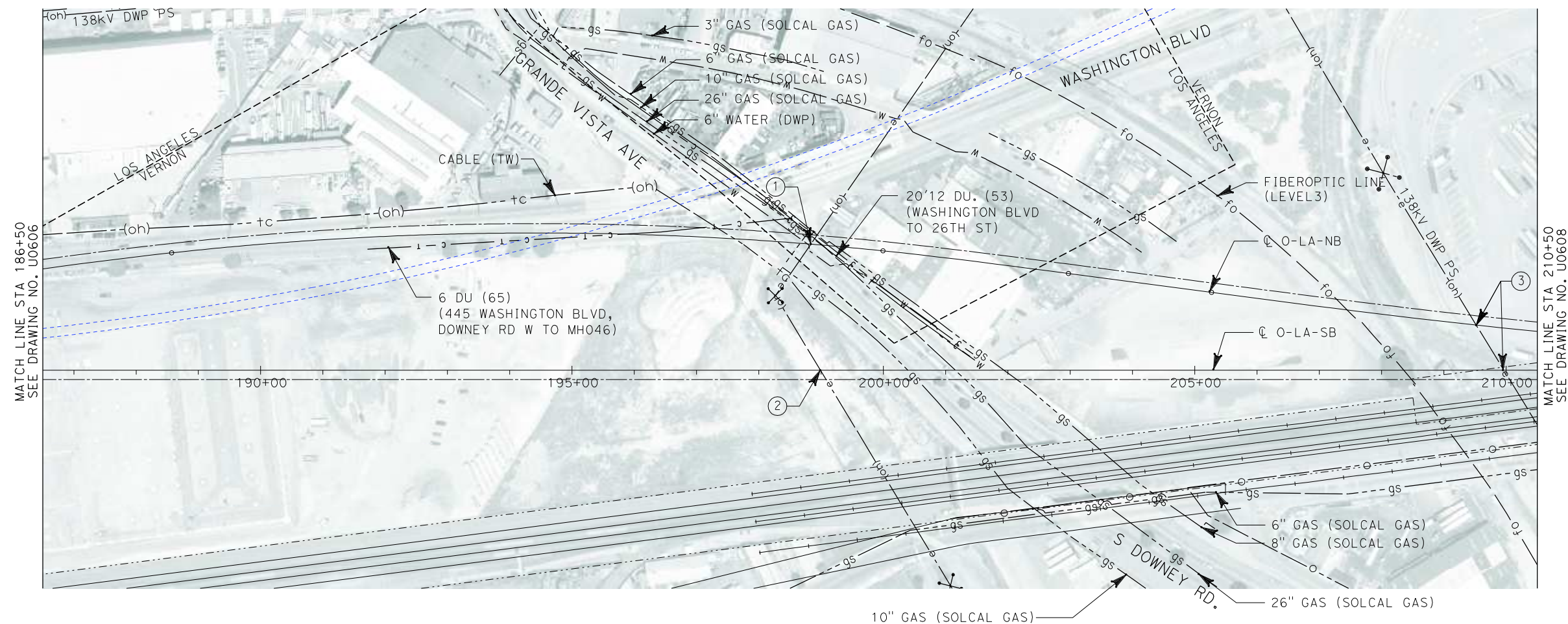
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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0606
SCALE 1" = 100'
SHEET NO. 120 OF 245

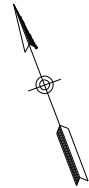
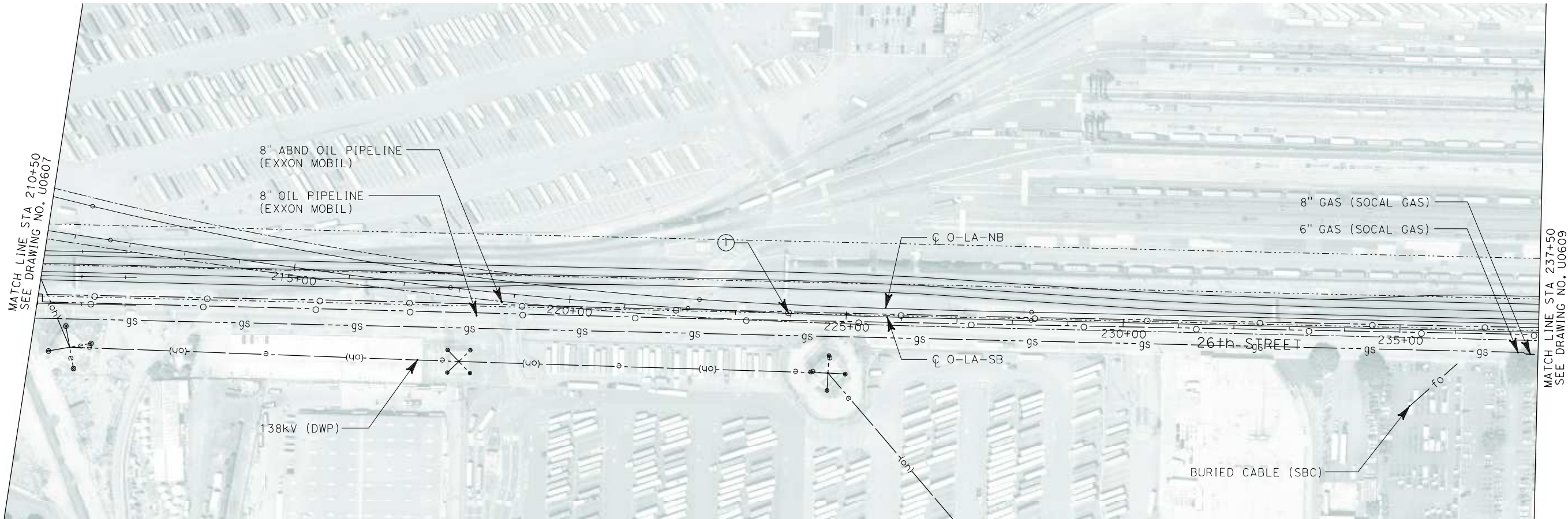


No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	TIME WARNER	COMMUNICATION	CABLE	GRAND VISTA AVE	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS. DUE TO THE LACK OF INFORMATION OF THE ELEVATION OF THE COMMUNICATION LINE, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
②	TIME WARNER	COMMUNICATION	CABLE	STA 195+00 TO STA 196+50	THE EXISTING COMMUNICATION LINE IS CROSSING THE PROPOSED O-LA TRACKS. DUE TO THE LACK OF INFORMATION OF THE ELEVATION OF THE COMMUNICATION LINE, THIS WILL BE CONSIDERED A CONFLICT.	LOW RISK	
③	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138KV	STA 199+00	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE SB PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	
④	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138KV	STA 199+00	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE NB PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	
⑤	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	OVERHEAD ELECTRICAL LINE-138KV	STA 210+00	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	

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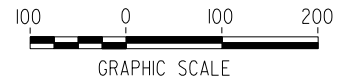


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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBILE	OIL	8" ABND OIL PIPELINE	STA 225+32 TO STA 237+50	THE OIL LINE IS IN CONFLICT WITH A PROPOSED COLUMNS	HIGH RISK	


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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





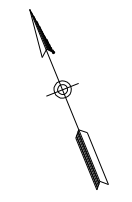
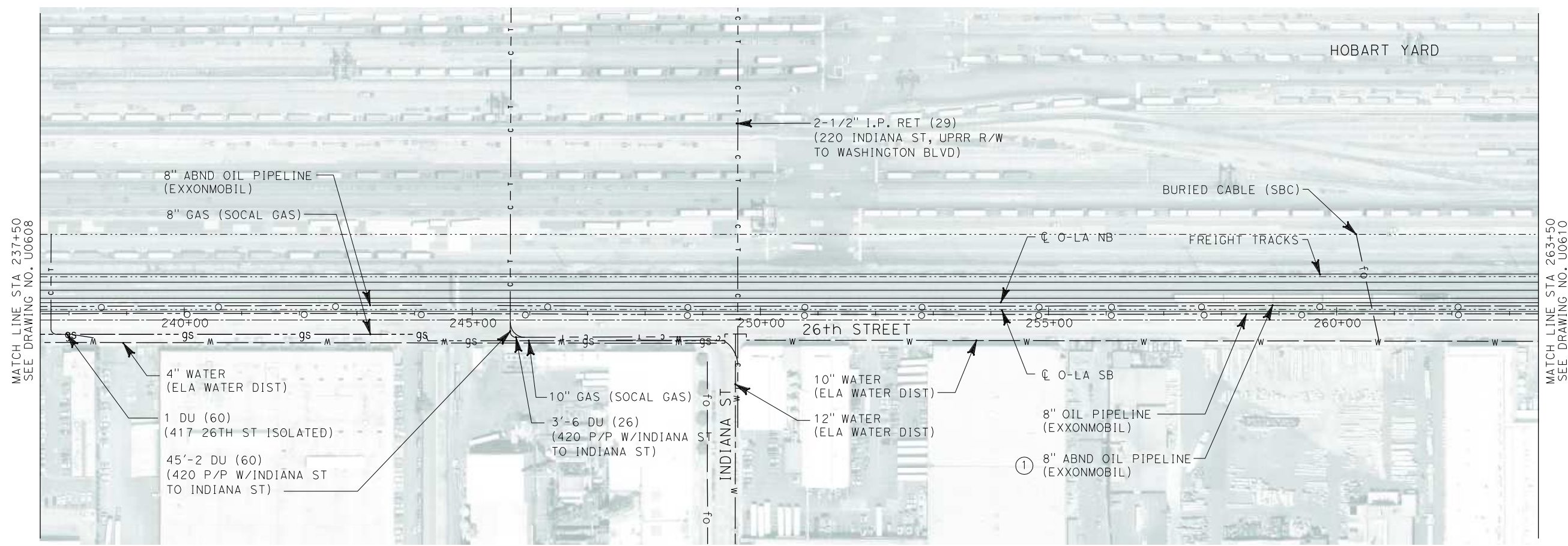
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ORANGE COUNTY TO LOS ANGELES**

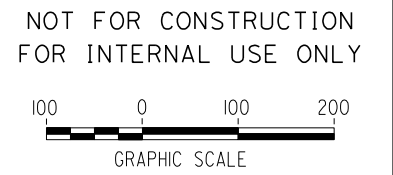
EXISTING COMPOSITE UTILITIES
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CONTRACT NO. HSR06-0005
DRAWING NO. U0608
SCALE 1" = 100'
SHEET NO. 122 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$




No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBILE	OIL	8" ABND OIL PIPELINE	STA 237+50 TO STA 263+50	THE OIL LINE IS IN CONFLICT WITH PROPOSED COLUMNS	HIGH RISK	



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
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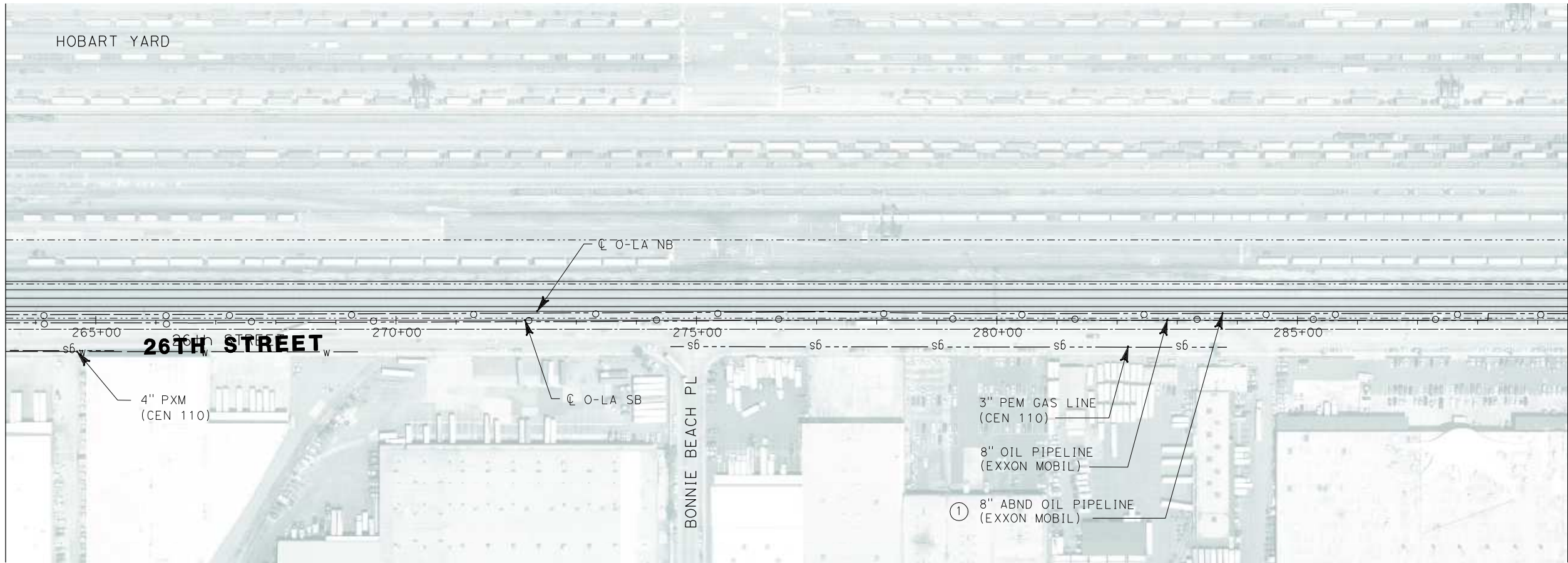
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

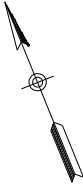
CONTRACT NO. HSR06-0005
DRAWING NO. U0609
SCALE 1" = 100'
SHEET NO. 123 OF 245

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MATCH LINE STA 263+50
SEE DRAWING NO. U0609



MATCH LINE STA 289+50
SEE DRAWING NO. U0611



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBIL	OIL	8" ABND OIL PIPELINE	STA 263+50 TO STA 269+30	THE OIL LINE IS IN CONFLICT WITH PROPOSED COLUMNS	HIGH RISK	

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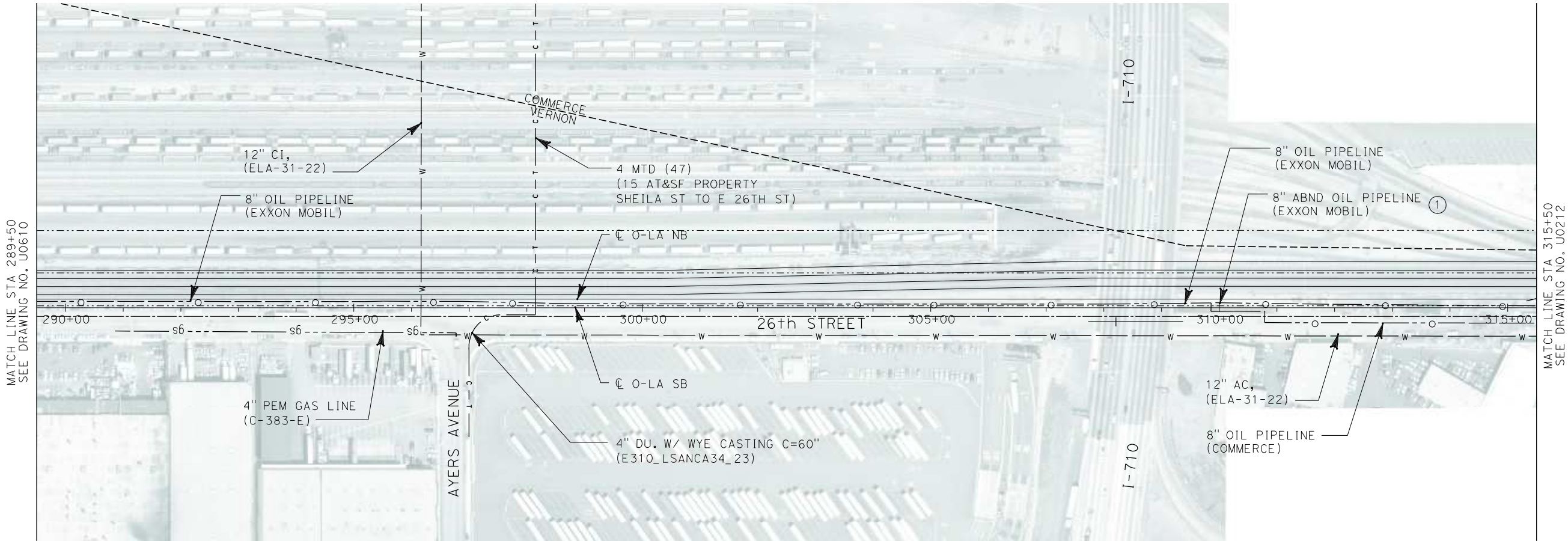
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0610
SCALE 1" = 100'
SHEET NO. 124 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBIL	OIL	8" ABND OIL PIPELINE	STA 297+52 TO STA 313+00	THE OIL LINE IS IN CONFLICT WITH PROPOSED COLUMNS	HIGH RISK	

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REV	DATE	BY	SUB	APP	DESCRIPTION

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DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



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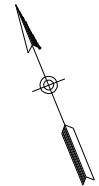
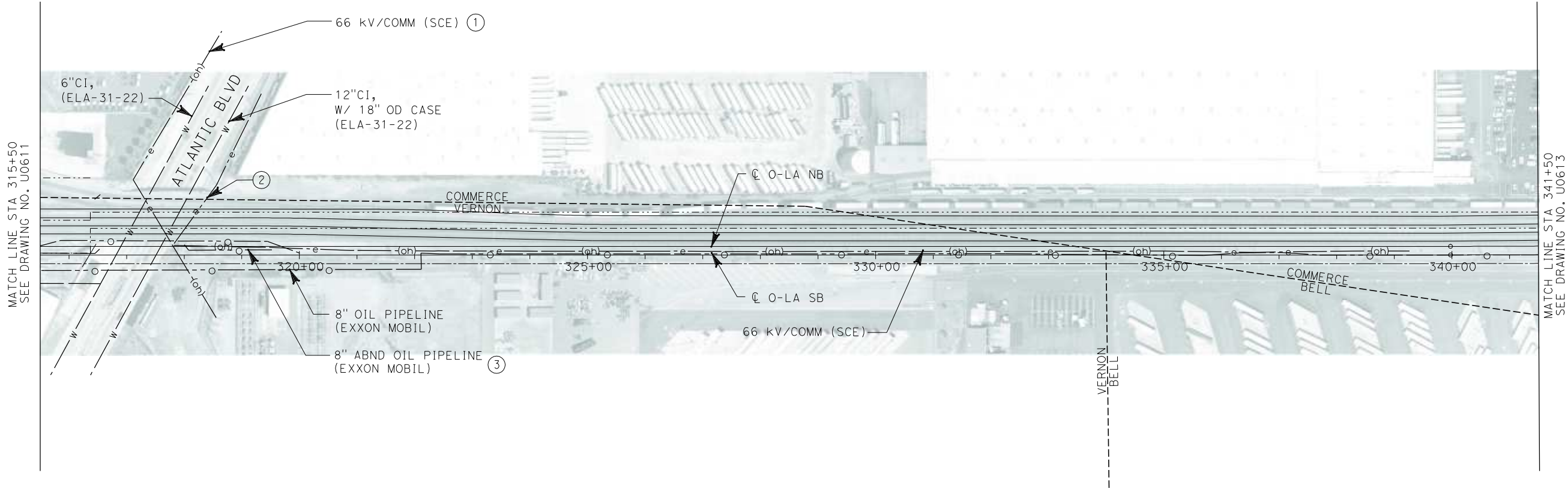
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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0611
SCALE 1" = 100'
SHEET NO. 125 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66 kV	ATLANTIC BLVD	THE EXISTING POWER POLE IS IN CONFLICT WITH THE WIDENING OF THE ATLANTIC BLVD BRIDGE.	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	66 kV	ATLANTIC BLVD	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED TRACKS, ACCORDING TO CPUC THE MIN, VERTICAL CLEARANCE IS 34 FT. DUE TO LACK OF INFORMATION OF THE ELEVATION OF THE OVERHEAD LINES, THIS WILL BE CONSIDERED A CONFLICT.	HIGH COST RELOCATION	
③	SCE	ELECTRICAL	66 kV	STA 317+82 TO STA 339+28	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED TRACKS AND THEY ARE IN CONFLICT WITH PROPOSED COLUMNS.	HIGH COST RELOCATION	

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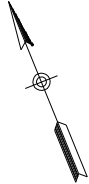
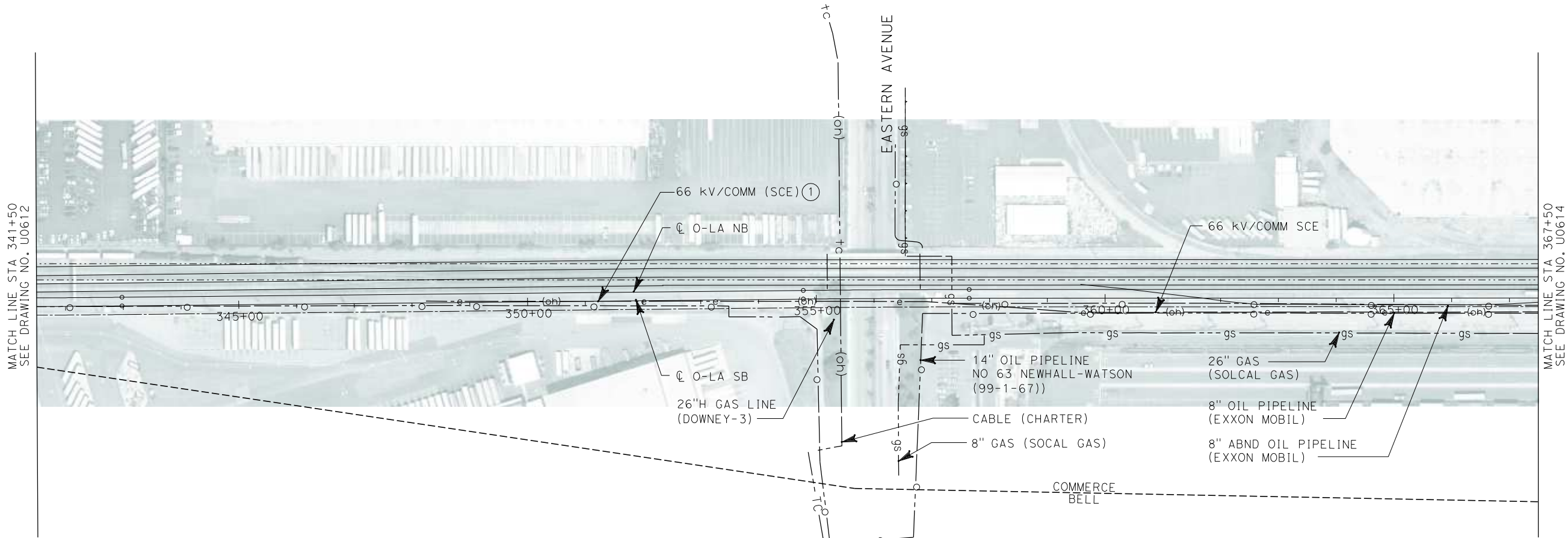
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EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0612
SCALE 1" = 100'
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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66Kv/COMM	STA 348+28 TO STA 356+00	THE OVERHEAD ELECTRICAL LINES ARE TOO CLOSE TO THE NB O-LA PROPOSED TRACK	HIGH COST RELOCATION	

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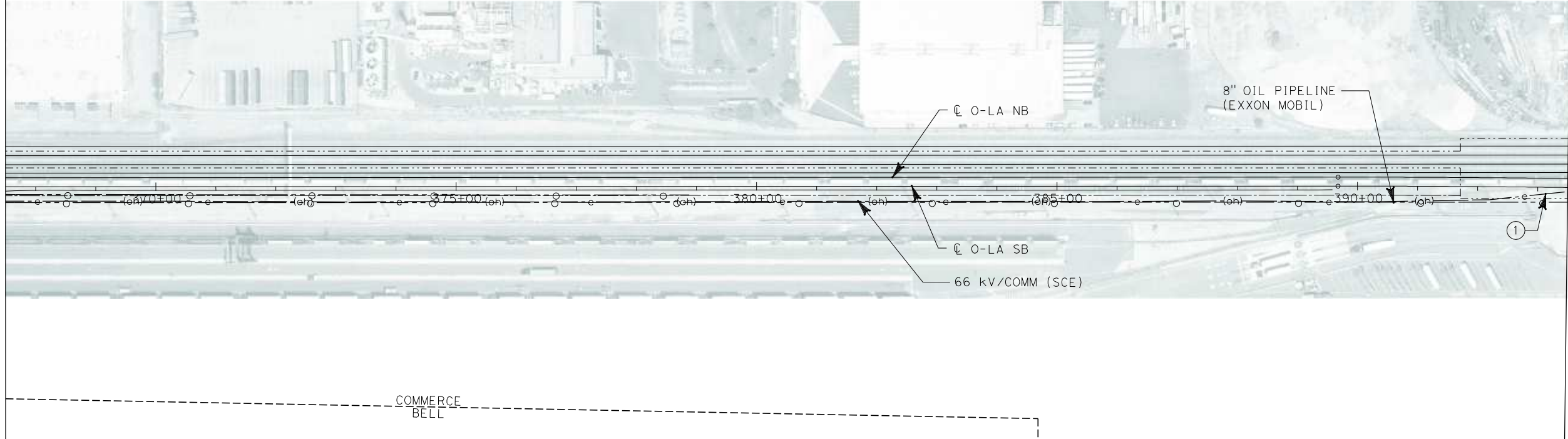
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0613
SCALE 1" = 100'
SHEET NO. 127 OF 245

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MATCH LINE STA 367+50
SEE DRAWING NO. U0613



MATCH LINE STA 393+50
SEE DRAWING NO. U0615



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66kv/COMM	STA 394+00	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE SB PROPOSED O-LA TRACK	HIGH COST RELOCATION	

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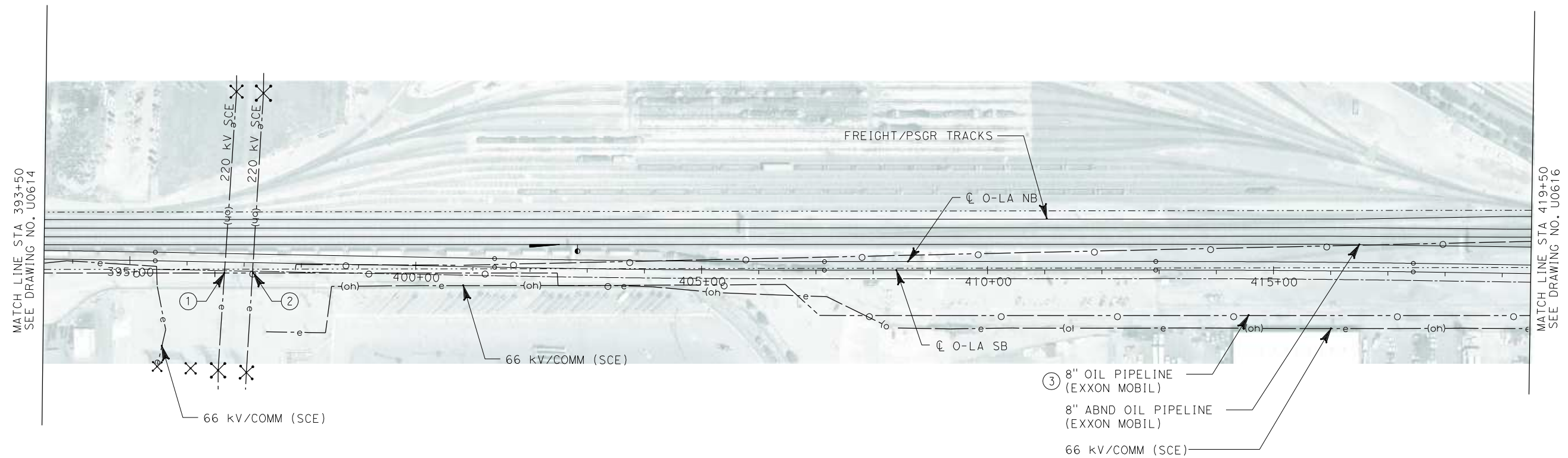


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ORANGE COUNTY TO LOS ANGELES**

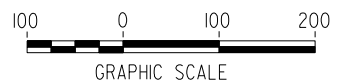
EXISTING COMPOSITE UTILITIES
PLAN



CONTRACT NO. HSR06-0005
DRAWING NO. U0614
SCALE 1" = 100'
SHEET NO. 128 OF 245



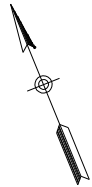
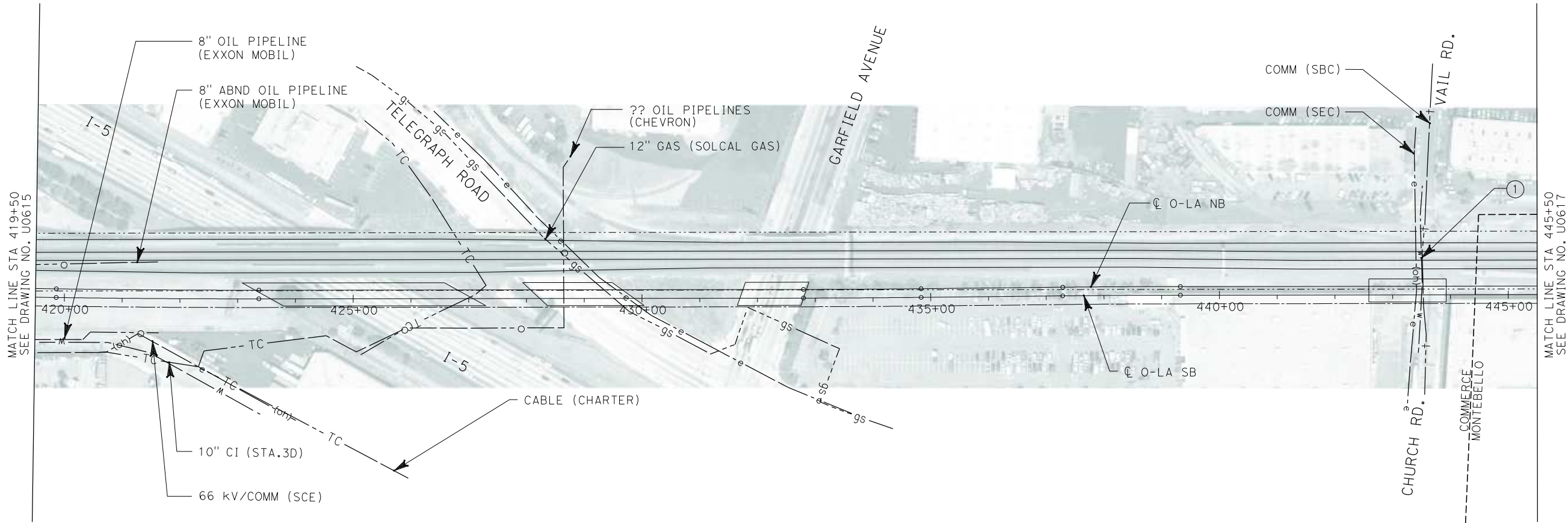
No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	220kV	STA 396+65	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	220kV	STA 397+13	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	
③	EXXON MOBIL	OIL	8" OIL PIPELINE	STA 407+47	THE OIL LINE IS IN CONFLICT WITH THE PROPOSED COLUMN	HIGH RISK	

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						DRAWN BY Z. OHN				DRAWING NO. U0615
						CHECKED BY M. SATISH				SCALE 1" = 100'
						IN CHARGE D. THOMSON				SHEET NO. 129 OF 245
						DATE 02-13-09				
REV	DATE	BY	SUB	APP	DESCRIPTION	EXISTING COMPOSITE UTILITIES PLAN				

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	MWD	WATER	73" MWD WATER	VAIL ROAD	THE WATER LINE WILL NEED TO BE ENCASED BECAUSE THE EXISTING AND PROPOSED TRACKS ARE IN THE SAME ELEVATION.	HIGH COST RELOCATION	

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REV	DATE	BY	SUB	APP	DESCRIPTION

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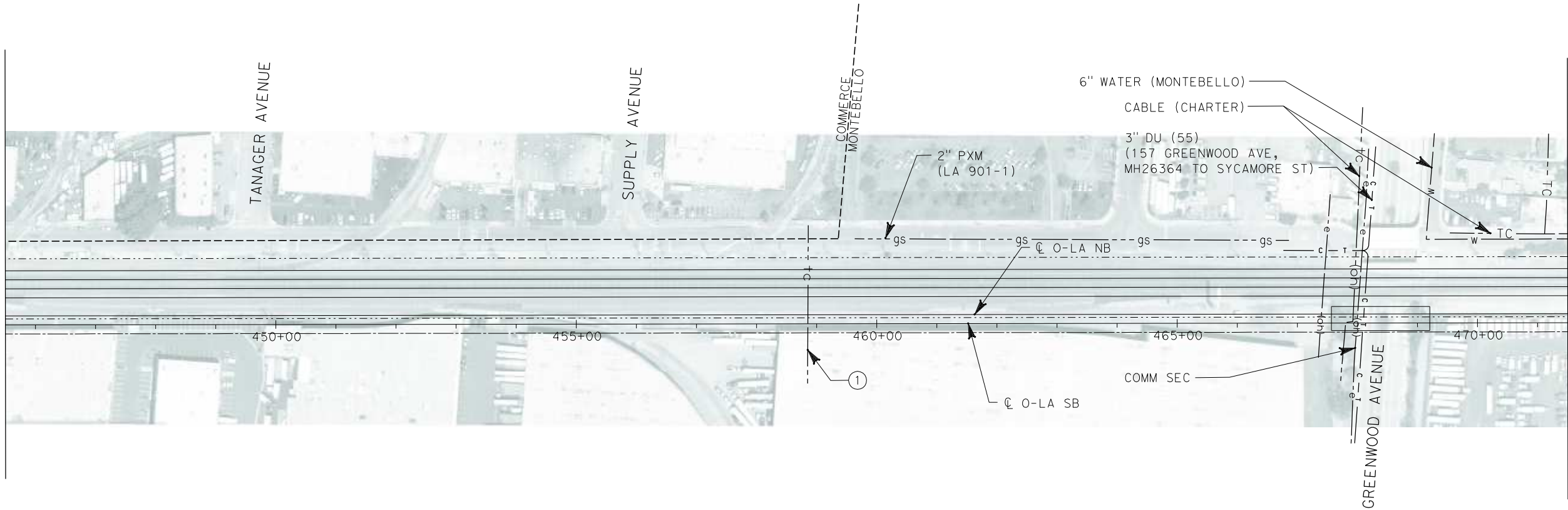
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0616
SCALE 1" = 100'
SHEET NO. 130 OF 245

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MATCH LINE STA 445+50
SEE DRAWING NO. U0616



MATCH LINE STA 471+50
SEE DRAWING NO. U0618

No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CHARTER COMMUNICATION	COMMUNICATION	CABLE	STA 458+85	THE CABLE LINE WILL NEED TO BE ENCASED BECAUSE THE EXISTING AND PROPOSED TRACKS ARE IN THE SAME ELEVATION.	LOW RISK	

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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0617
SCALE 1" = 100'
SHEET NO. 131 OF 245

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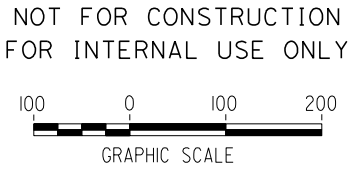
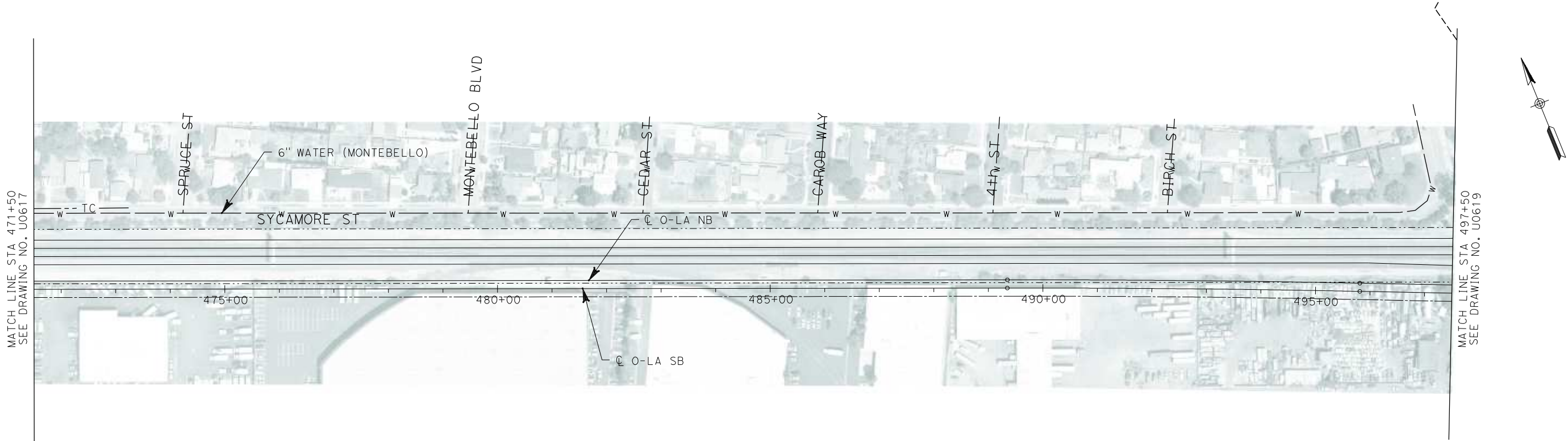


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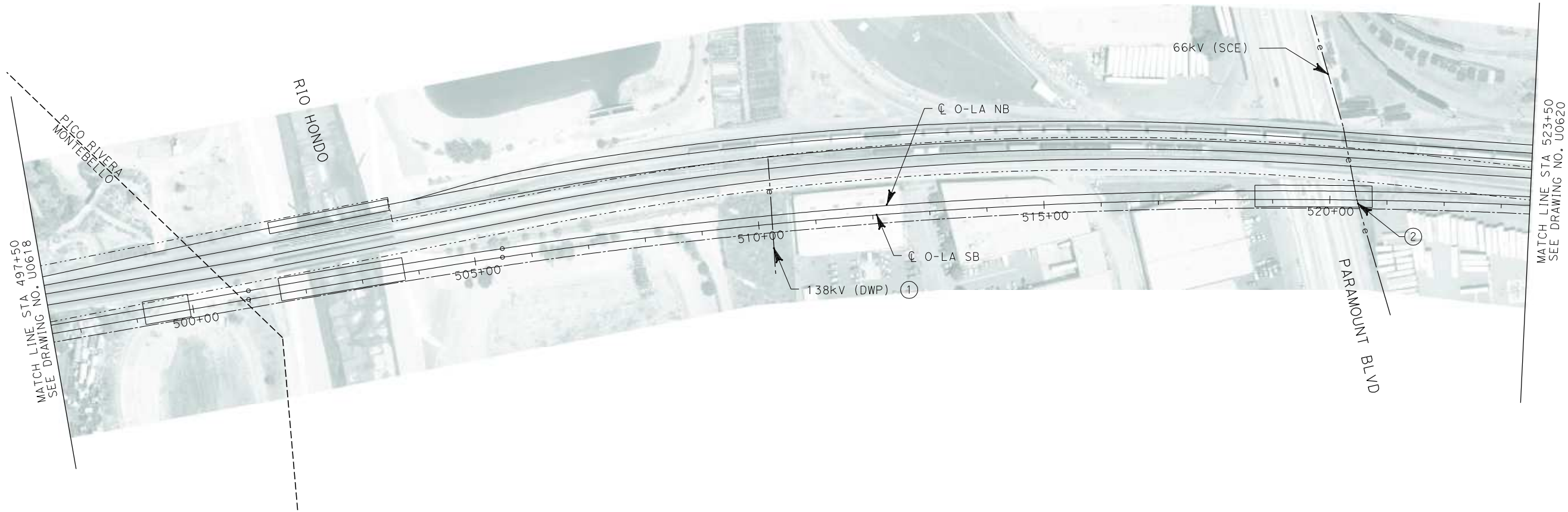
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

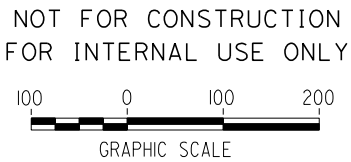
CONTRACT NO. HSR06-0005
DRAWING NO. U0618
SCALE 1" = 100'
SHEET NO. 132 OF 245



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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	DEPARTMENT OF WATER AND POWER - POWER SERVICE	ELECTRICAL	TRANSMISSION TOWER-138kV	STA 510+22	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	66Kv/COMM	PARAMOUNT BLVD.	EXISTING POWER POLE IS IN CONFLICT WITH THE PROPOSED WIDENING OF PARAMOUNT BRIDGE.	HIGH COST RELOCATION	



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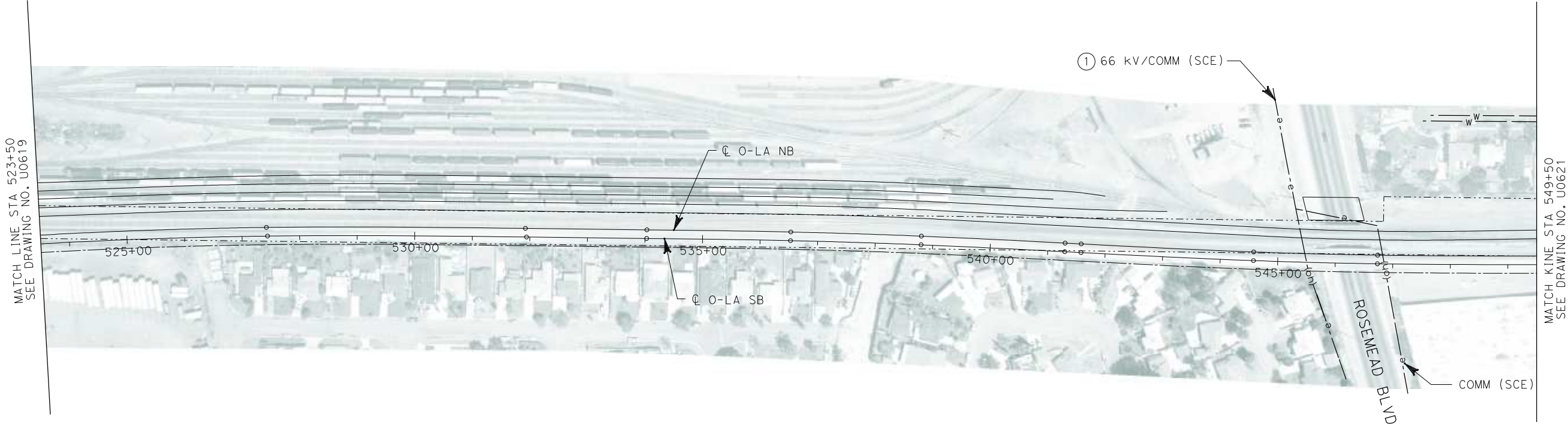


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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0619
SCALE 1" = 100'
SHEET NO. 133 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66Kv/COMM	ROSEMEAD BLVD	THE EXISTING POWER POLE IS IN CONFLICT WITH THE PROPOSED WIDENING OF ROSEMEAD BLVD BRIDGE.	HIGH COST RELOCATION	

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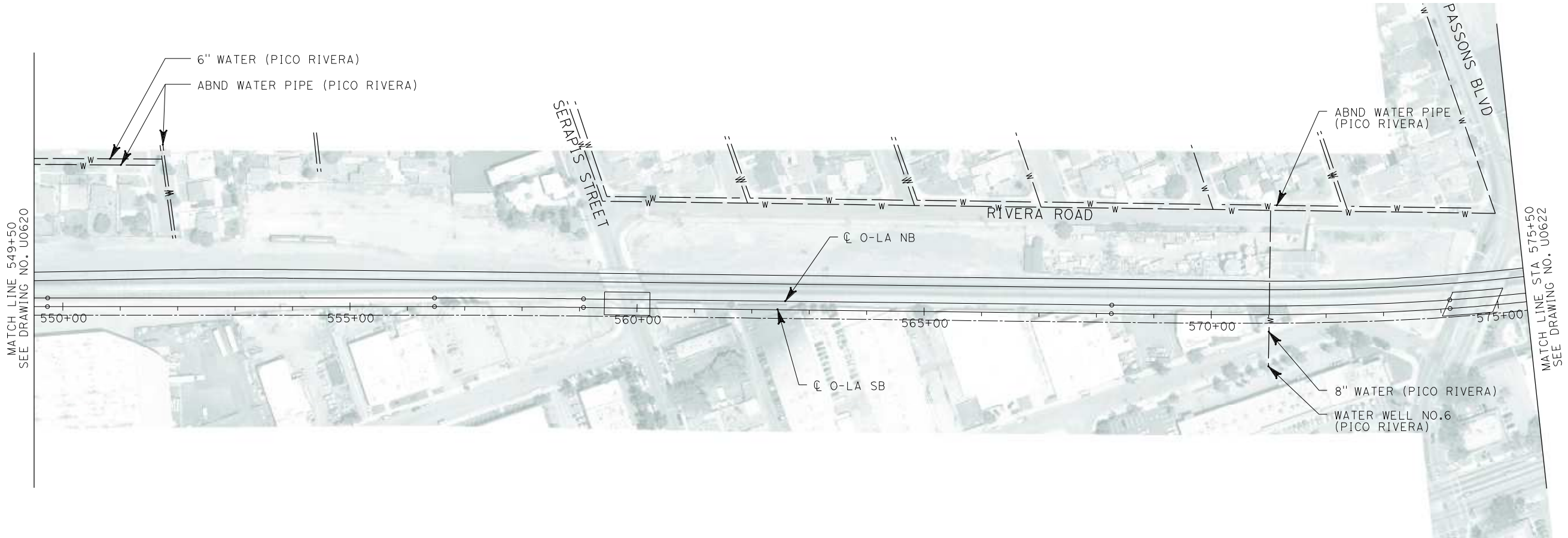
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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0620
SCALE 1" = 100'
SHEET NO. 134 OF 245

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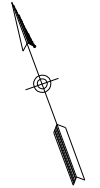
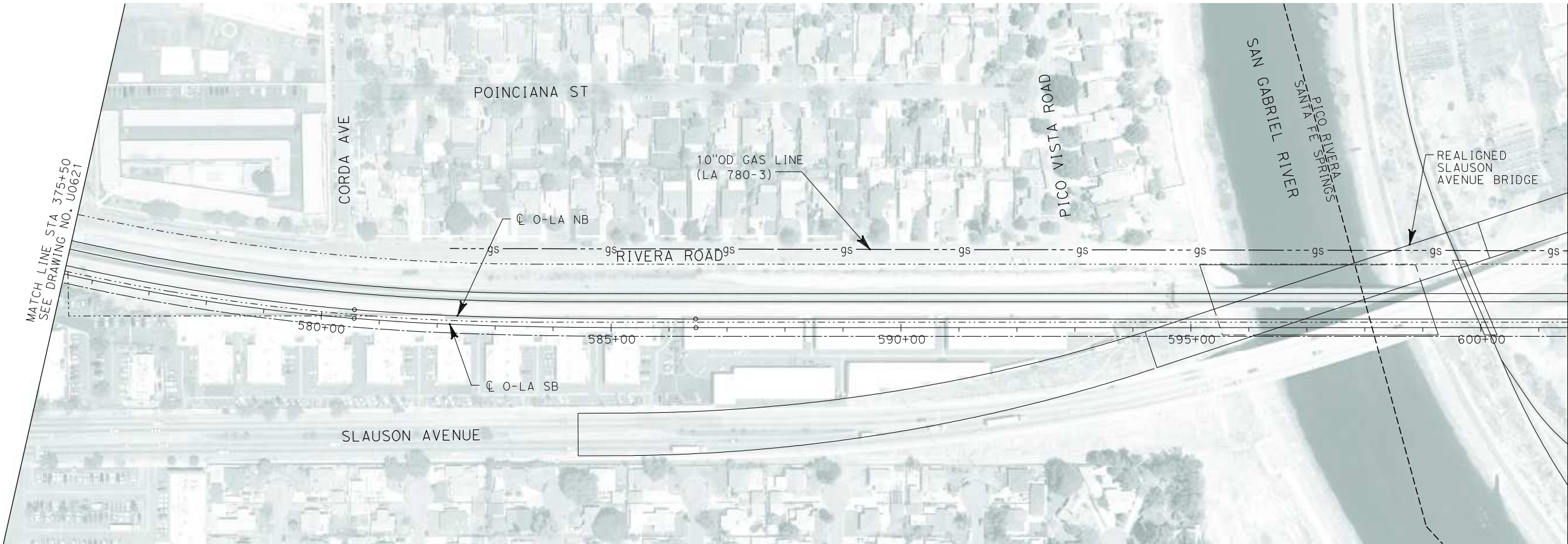
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0621
SCALE 1" = 100'
SHEET NO. 135 OF 245

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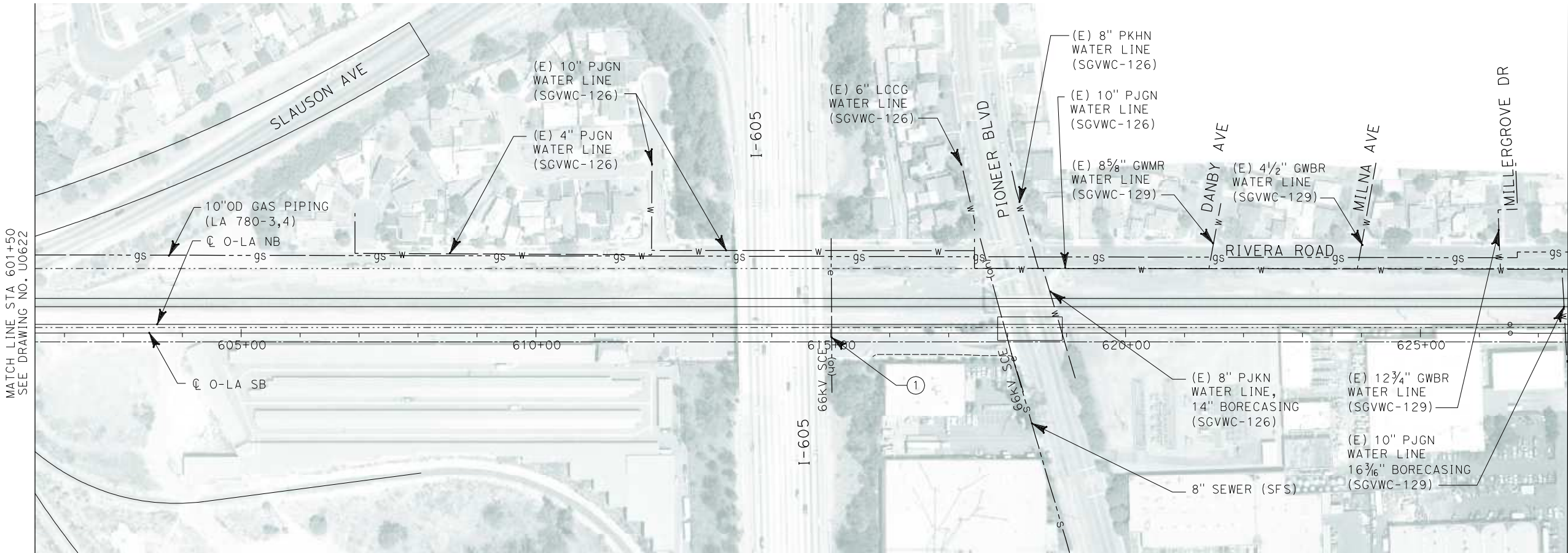
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EXISTING COMPOSITE UTILITIES PLAN		DRAWING NO. U0622
		SCALE 1" = 100'
		SHEET NO. 136 OF 245

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
No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66kV/COMM	I-605	THE PROPOSED STRUCTURE FOR THE O-LA TRACKS IS IN CONFLICT WITH THE EXISTING POWER POLE.	HIGH COST RELOCATION	

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EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0623
SCALE 1" = 100'
SHEET NO. 137 OF 245

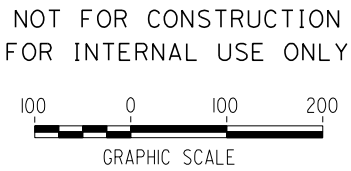
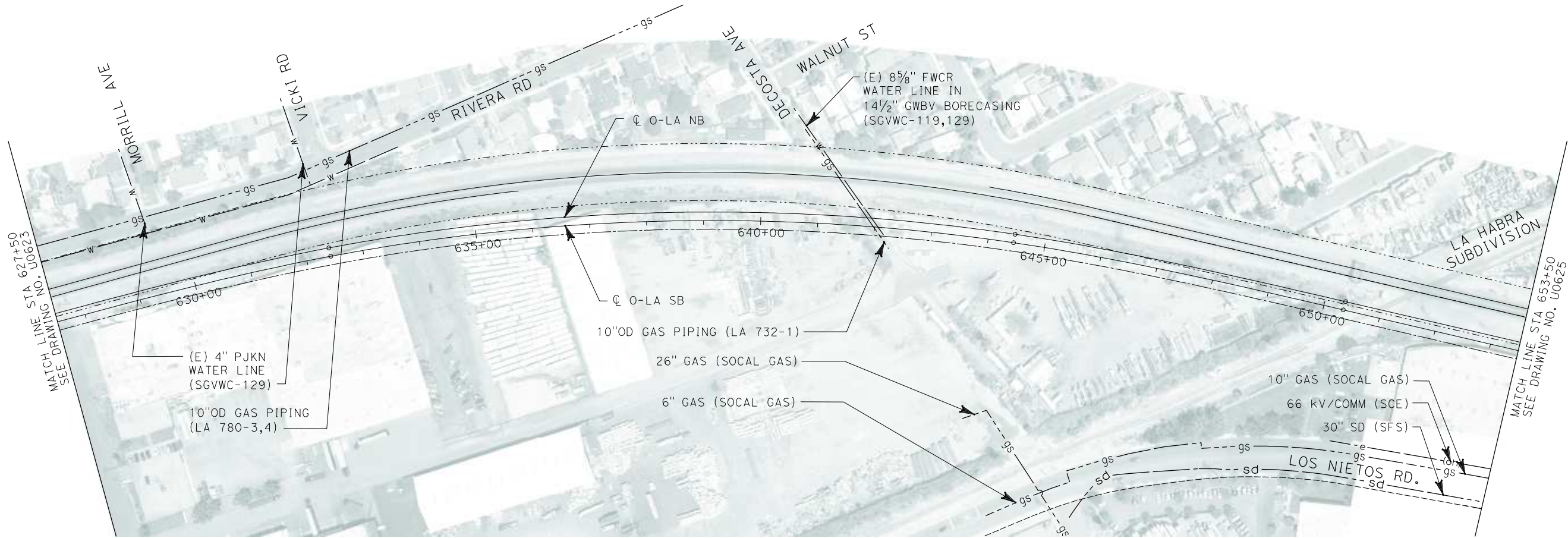
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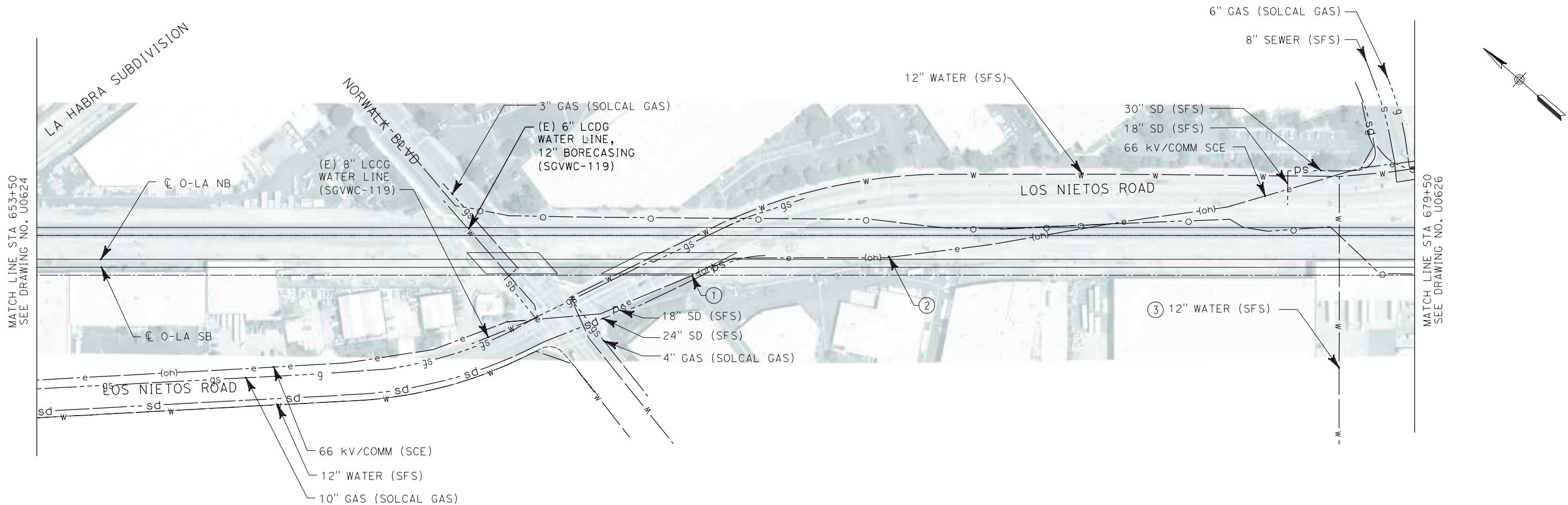
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	DRAWING NO. U0624
	SCALE 1" = 100'
	SHEET NO. 138 OF 245



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
No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66Kv/COMM	STA 666+22 TO STA 666+65	THE EXISTING OVERHEAD ELECTRICAL LINES ARE CROSSING THE PROPOSED O-LA TRACKS.THE EXISTING TRACKS ARE LOWER THAN THE PROPOSED TRACKS. DUE TO THE ELEVATION OF PROPOSED TRACKS, THE OVERHEAD LINES DO NOT MEET THE MINMUM CRITERIA FOR VERTICAL CLEARANCE.	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	66Kv/COMM	STA 666+65 TO STA 669+52	THE EXISTING OVERHEAD ELECTRICAL LINES ARE TOO CLOSE TO THE PROPOSED O-LA TRACKS.	HIGH COST RELOCATION	
③	SANTA FE SPRINGS	WATER	12' WATER	STA 678+08	THE WATER LINE WILL NEED TO BE ENCASED BACAUSE THE EXISTING AND THE PROPOSED O-LA TRACKS ARE IN THE SAME ELEVATION.	LOW RISK	

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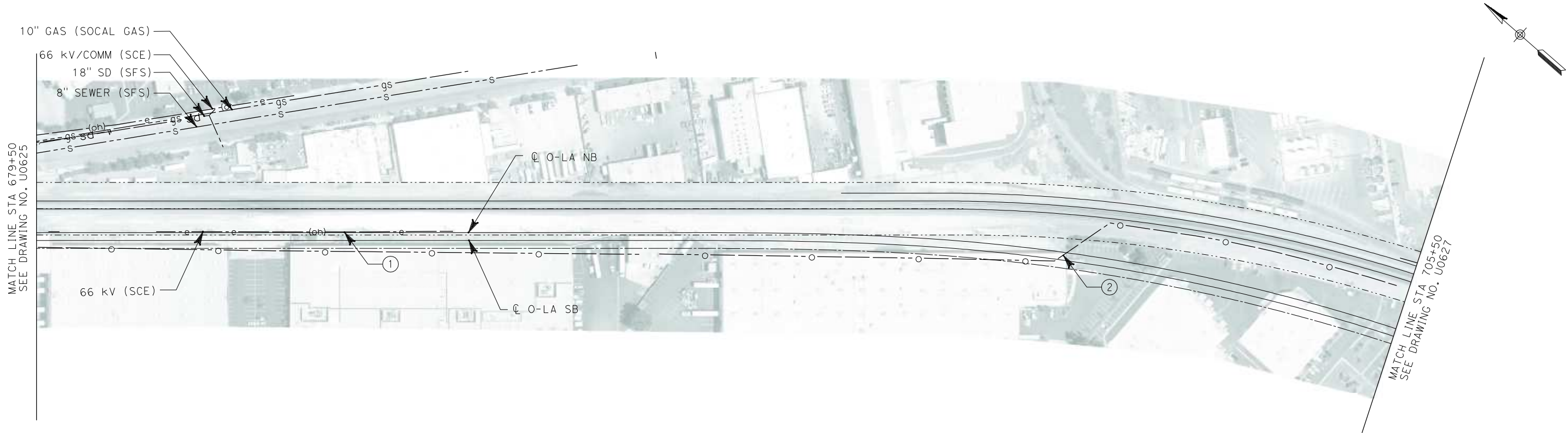


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EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0625
SCALE 1" = 100'
SHEET NO. 139 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66Kv/COMM	STA 681+77 TO STA 687+42	THE EXISTING OVERHEAD ELECTRICAL LINES ARE TOO CLOSE TO THE NB O-LA PROPOSED TRACK	HIGH COST RELOCATION	
②	EXXON MOBIL	OIL	8" ABND OIL PIPELINE	STA 699+00	THE EXISTING OIL LINE WILL NEED TO BE ENCASED BECAUSE THE THE EXISTING AND THE PROPOSED O-LA TRACKS ARE IN THE SAME ELEVATION.	HIGH RISK	


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DATE 02-13-09



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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0626
SCALE 1" = 100'
SHEET NO. 140 OF 245

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PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0627
SCALE 1" = 100'
SHEET NO. 141 OF 245

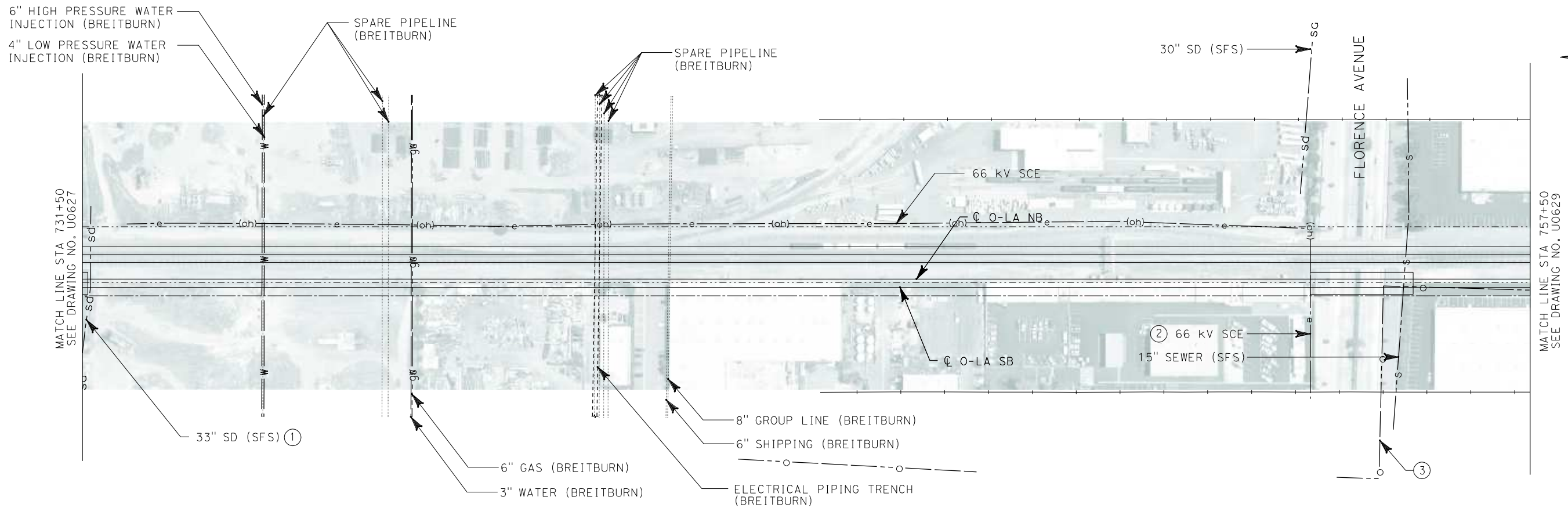


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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CITY OF SANTA FE SPRINGS	STORM DRAIN	33" SD	STA 731+50	THE EXISTING STORM DRAIN LINE IS IN CONFLICT WITH THE PROPOSED BRIDGE OVER TELEGRAPH ROAD.	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	66 kV	FLORENCE AVE	THE EXISTING POWER POLE IS IN CONFLICT DUE TO THE WIDENING OF THE FLORENCE AVE BRIDGE.	HIGH COST RELOCATION	
③	EXXON MOBIL	OIL	8" ABND OIL PIPELINE	STA 755+00 TO STA 757+00	THE EXISTING OIL LINE WILL NEED TO BE ENCASED BECAUSE THE THE EXISTING AND THE PROPOSED O-LA TRACKS ARE IN THE SAME ELEVATION.	HIGH RISK	

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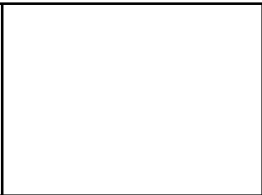


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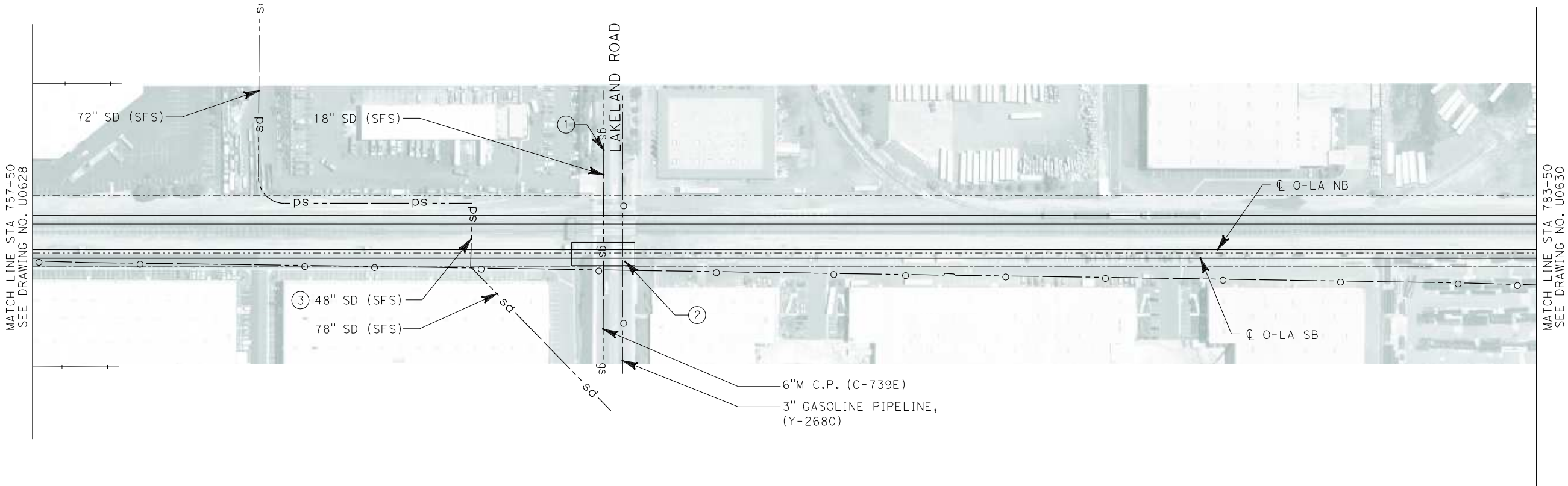
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EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0628
SCALE 1" = 100'
SHEET NO. 142 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SC GAS ANAHEIM	GAS	6" M C.P.	LAKELAND ROAD	THE GAS LINE IS IN CONFLICT DUE TO THE WIDENING OF THE LAKELAND BRIDGE	HIGH RISK	
②	SHELL OIL PIPELINES - SFS	OIL	3" GASOLINE PIPELINE	LAKELAND ROAD	THE WATER LINE IS IN CONFLICT DUE TO THE WIDENING OF THE LAKELAND BRIDGE	HIGH RISK	
③	CITY OF SANTA FE SPRINGS	STORM DRAIN	48" STORM DRAIN	STA 765+00	THE STORM DRAIN LINE NEED TO BE ENCASED BECAUSE THE EXISTING AND PROPOSED O-LA TRACKS ARE IN THE SAME ELEVATION	HIGH COST RELOCATION	

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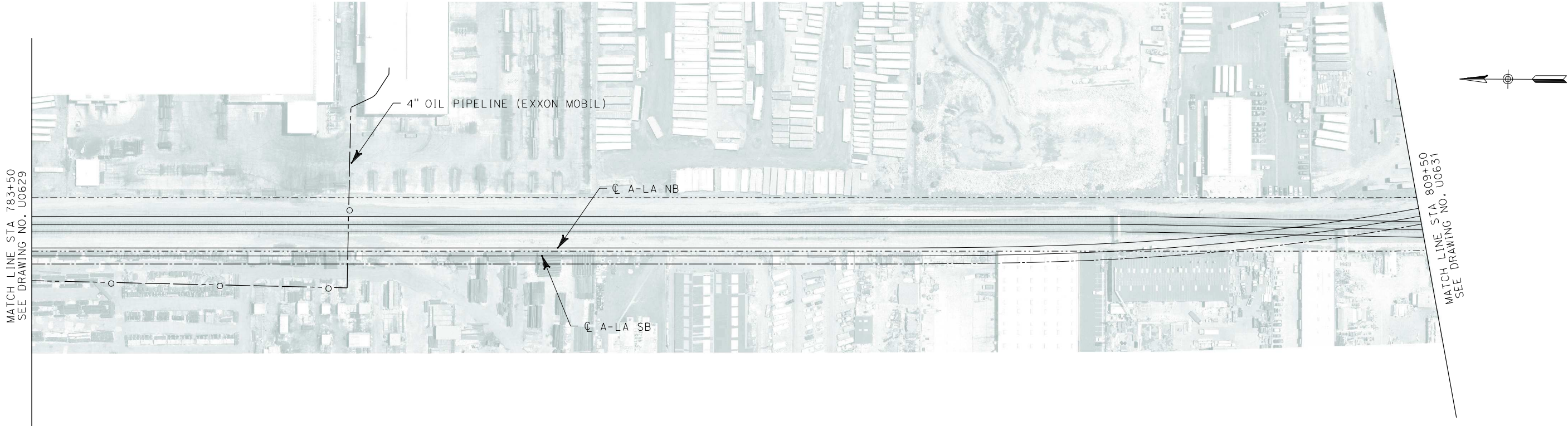


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EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0629
SCALE 1" = 100'
SHEET NO. 143 OF 245

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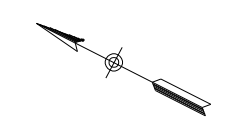
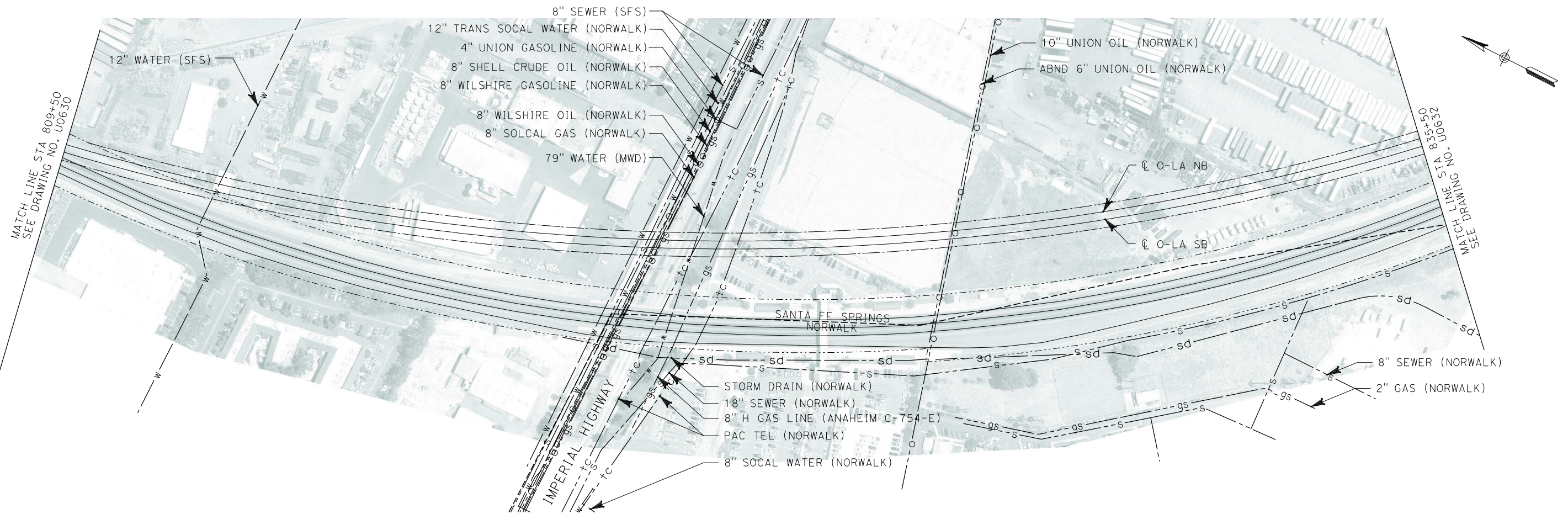


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PLAN

CONTRACT NO. HSR06-0005
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SCALE 1" = 100'
SHEET NO. 144 OF 245



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GRAPHIC SCALE

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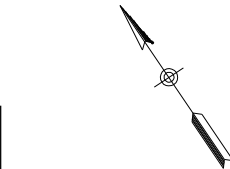
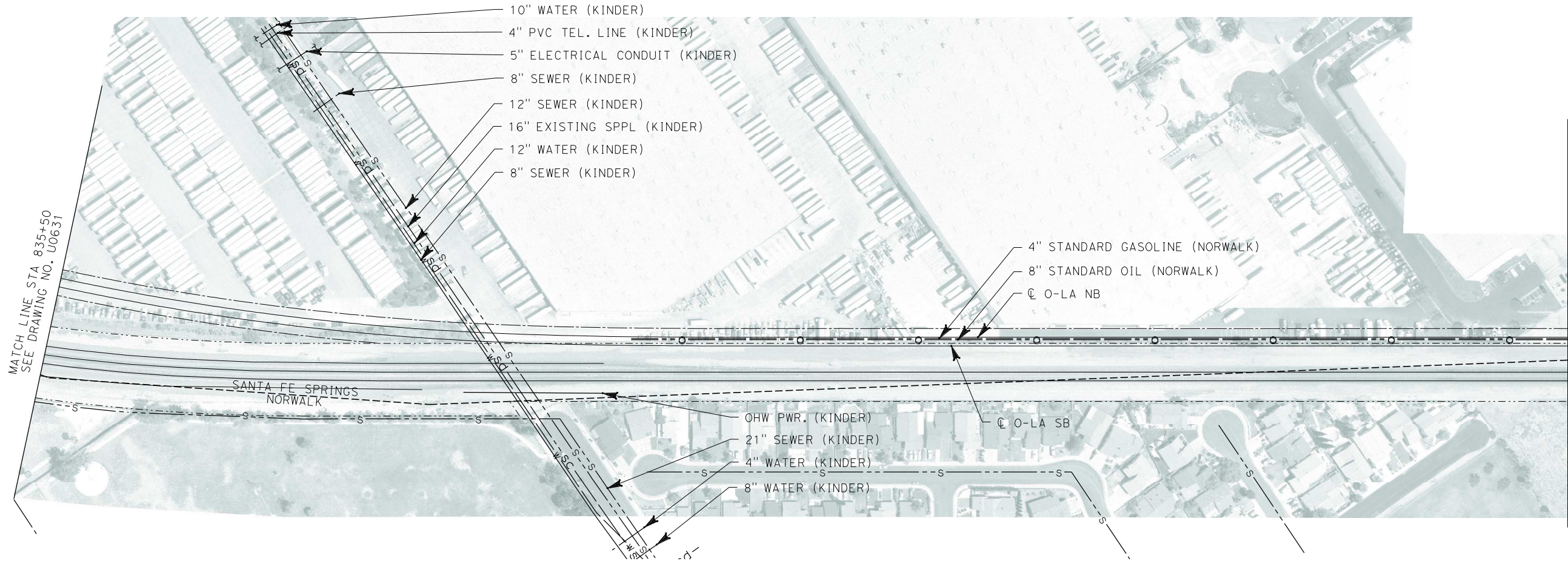
CALIFORNIA HIGH SPEED TRAIN PROJECT

ORANGE COUNTY TO LOS ANGELES

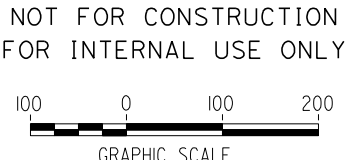
EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0631
SCALE 1" = 100'
SHEET NO. 145 OF 245

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


MATCH LINE STA 861+50
SEE DRAWING NO. U0633

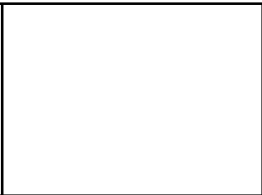


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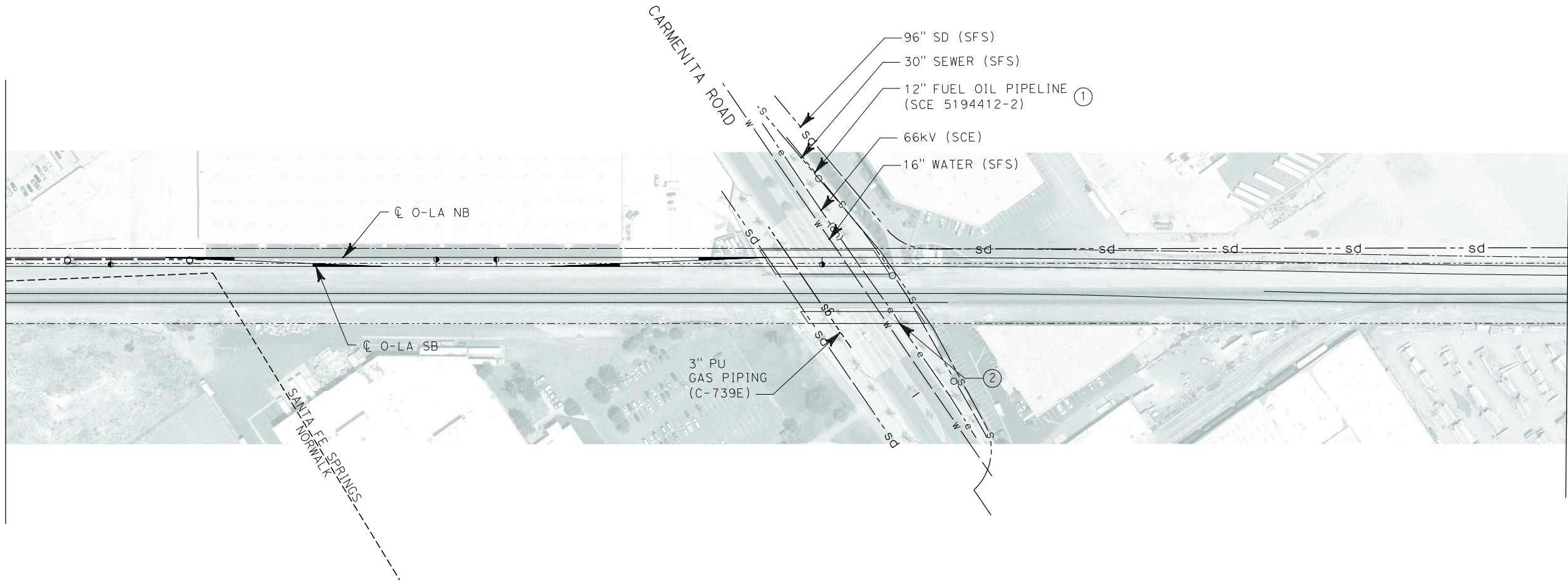
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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0632
SCALE 1" = 100'
SHEET NO. 146 OF 245

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MATCH LINE STA 861+50
SEE DRAWING NO. U0632



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBIL	OIL	12" OIL PIPELINE	CARMENTIA ROAD	THE EXISTING OIL PIPELINE IS IN CONFLICT WITH THE PROPOSED WIDENING OF THE BRIDGE	HIGH RISK	
②	SCE	ELECTRICAL	66 kV	CARMENTIA ROAD	THE EXISTING POWER POLE IS IN CONFLICT DUE TO THE WIDENING OF THE BRIDGE.	HIGH COST RELOCATION	

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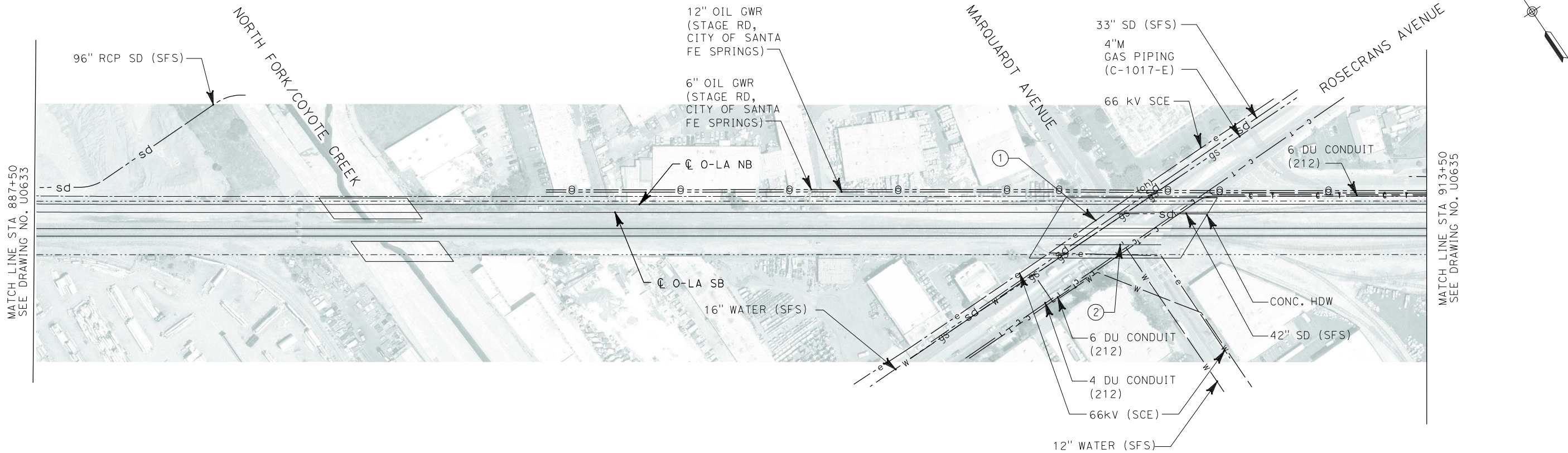


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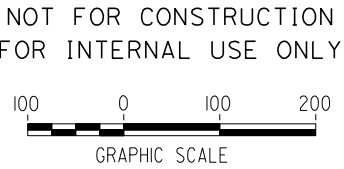
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0633
SCALE 1" = 100'
SHEET NO. 147 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	EXXON MOBILE	OIL	12" OIL PIPELINE	ROSECRANS AVE.	THE EXISTING OIL PIPELINE IS IN CONFLICT WITH THE PROPOSED WIDENING OF THE BRIDGE	HIGH RISK	
②	SCE	ELECTRICAL	66 kV	ROSECRANS AVE.	THE EXISTING POWER POLE IS IN CONFLICT DUE TO THE WIDENING OF THE BRIDGE	HIGH COST RELOCATION	



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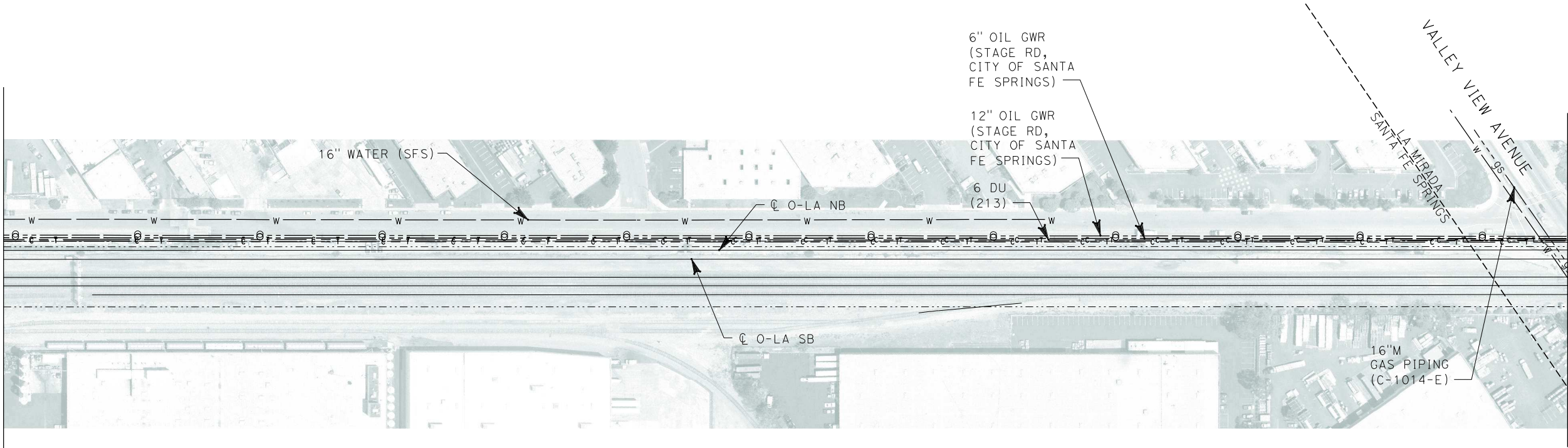
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0634
SCALE 1" = 100'
SHEET NO. 148 OF 245

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MATCH LINE STA 913+50
SEE DRAWING NO. U0634




MATCH LINE STA 939+50
SEE DRAWING NO. U0636

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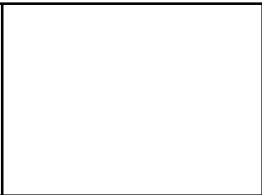


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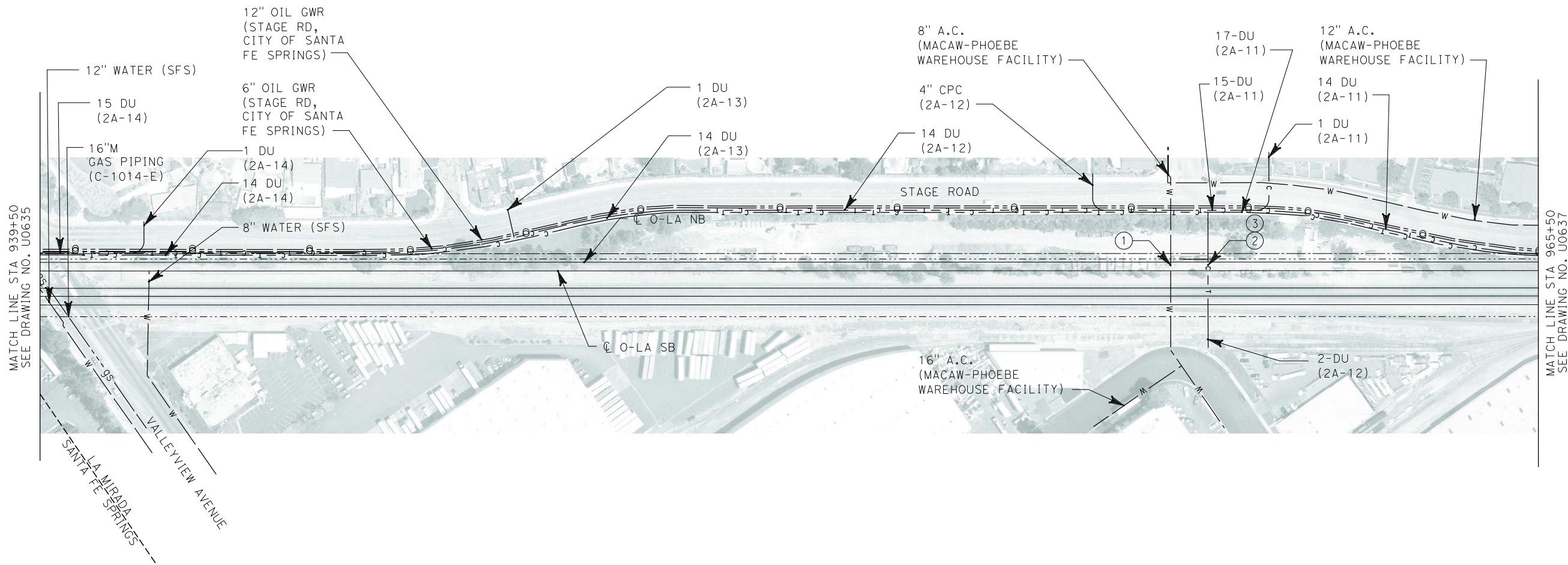


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CONTRACT NO. HSR06-0005
DRAWING NO. U0635
SCALE 1" = 100'
SHEET NO. 149 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	WATER L.A.	WATER	8" AC	STA 959+12	THE PROFILE OF THE PROPOSED O-LA TRACKS ARE LOWER THAN EXISTING TRACKS, THEREFORE THE WATER LINE WILL BE IN CONFLICT	LOW RISK	
②	CONDUIT - OC	COMMUNICATION	2-DU	STA 959+76	THE PROFILE OF THE PROPOSED O-LA TRACKS ARE LOWER THAN EXISTING TRACKS, THEREFORE THE CABLE-TV LINE WILL BE IN CONFLICT	LOW RISK	
③	CONDUIT - OC	COMMUNICATION	2-DU	STA 959+74 TO STA 959+76	THE CABLE TV LINE IS TOO CLOSE TO THE PROPOSED O-LA NB TRACK	LOW RISK	

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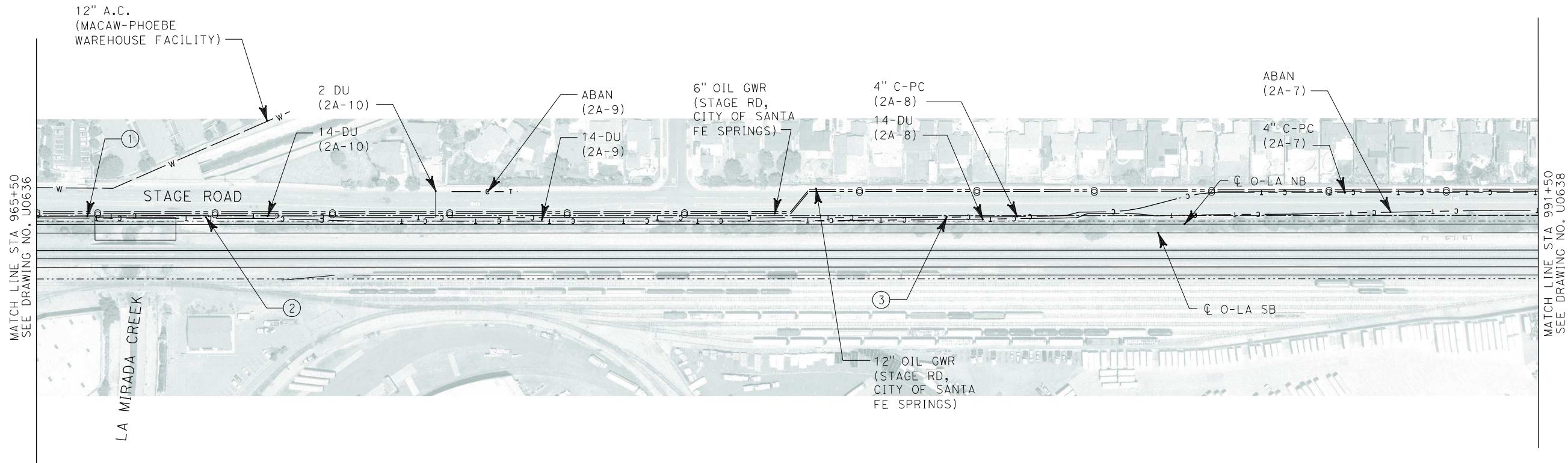


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CONTRACT NO. HSR06-0005
DRAWING NO. U0636
SCALE 1" = 100'
SHEET NO. 150 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	WATER L.A.	WATER	8" AC	STA 966+20	THE PROFILE OF THE PROPOSED O-LA TRACKS ARE LOWER THAN EXISTING TRACKS, THEREFORE THE WATER LINE WILL BE IN CONFLICT	LOW RISK	
②	CONDUIT - OC	COMMUNICATION	2-DU	LA MIRADA CREEK	THE PROFILE OF THE PROPOSED O-LA TRACKS ARE LOWER THAN EXISTING TRACKS, THEREFORE THE CABLE-TV LINE WILL BE IN CONFLICT	LOW RISK	
③	CONDUIT - OC	COMMUNICATION	2-DU	STA 971+00 TO STA 971+85	THE CABLE TV LINE IS TOO CLOSE TO THE PROPOSED O-LA NB TRACK	LOW RISK	

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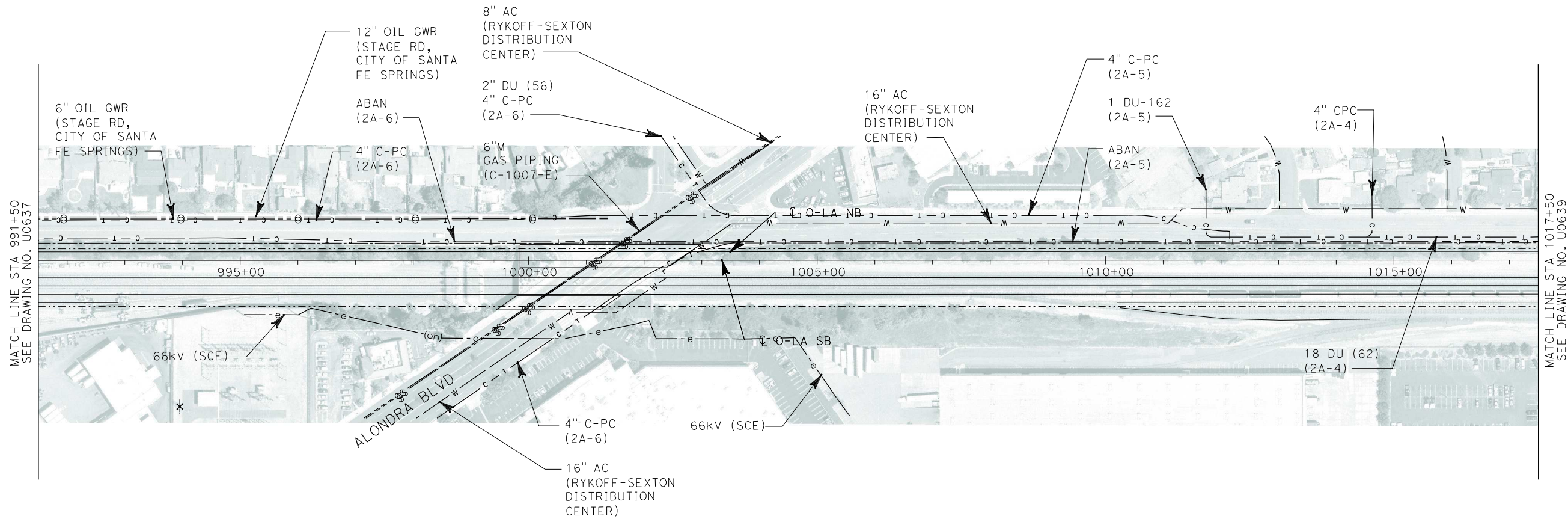
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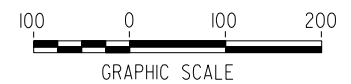
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PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0637
SCALE 1" = 100'
SHEET NO. 151 OF 245

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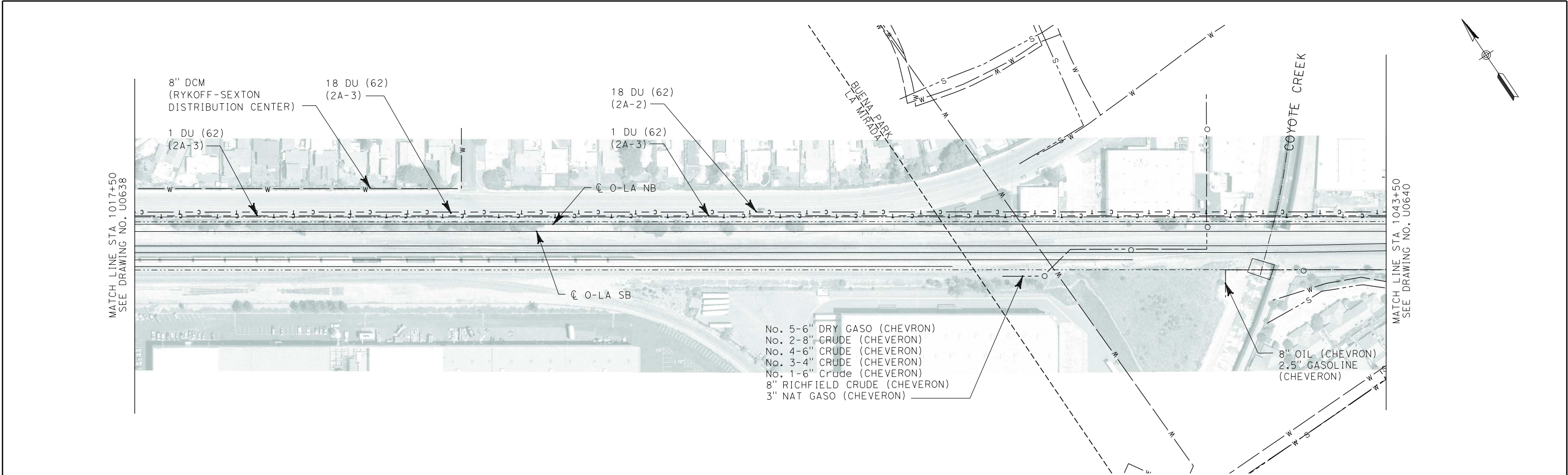


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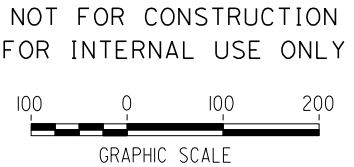
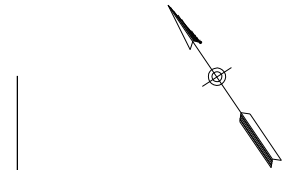
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PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0638
SCALE 1" = 100'
SHEET NO. 152 OF 245

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- No. 5-6" DRY GASO (CHEVRON)
- No. 2-8" CRUDE (CHEVERON)
- No. 4-6" CRUDE (CHEVERON)
- No. 3-4" CRUDE (CHEVERON)
- No. 1-6" CRUDE (CHEVERON)
- 8" RICHFIELD CRUDE (CHEVERON)
- 3" NAT GASO (CHEVERON)



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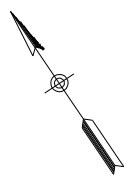
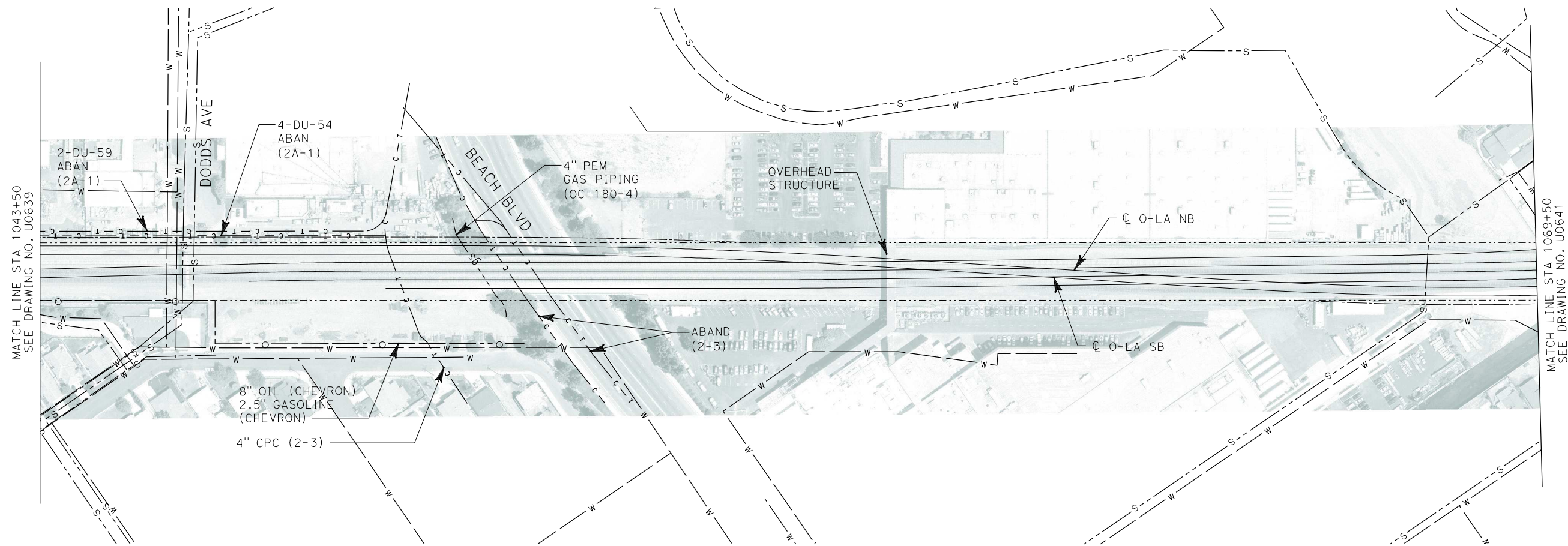
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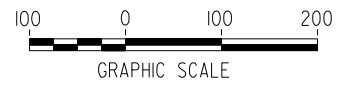
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DRAWING NO. U0639
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SHEET NO. 153 OF 245

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CONTRACT NO. HSR06-0005
DRAWING NO. U0640
SCALE 1" = 100'
SHEET NO. 154 OF 245

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
No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	BUENA PARK	WATER	12" WATER	DALE STREET	THE EXISTING WATER LINE IS IN CONFLICT DUE TO THE WIDENING OF THE BRIDGE AT DALE STREET	LOW RISK	

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REV	DATE	BY	SUB	APP	DESCRIPTION

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CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



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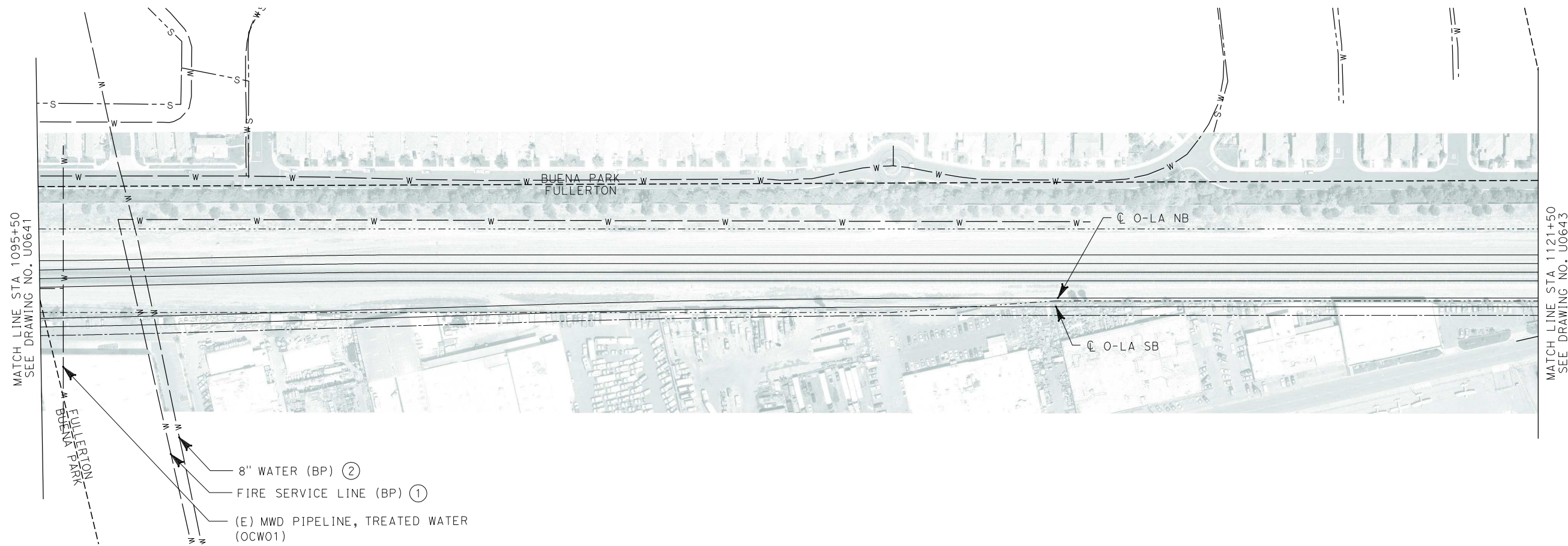
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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0641
SCALE 1" = 100'
SHEET NO. 155 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	BUENA PARK	WATER	8" WATER	STA 1097+30	THE EXISTING WATER LINE IS IN CROSSING THE PROPOSED TRACKS THEREFORE THE WATER LINE WILL BE ENCASED	LOW RISK	
②	BUENA PARK	WATER	8" WATER	STA 1097+60	THE EXISTING WATER LINE IS IN CROSSING THE PROPOSED TRACKS THEREFORE THE WATER LINE WILL BE ENCASED	LOW RISK	

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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY	L. HOROWITZ
DRAWN BY	Z. OHN
CHECKED BY	M. SATISH
IN CHARGE	D. THOMSON
DATE	02-13-09

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CALIFORNIA HIGH SPEED TRAIN PROJECT

ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

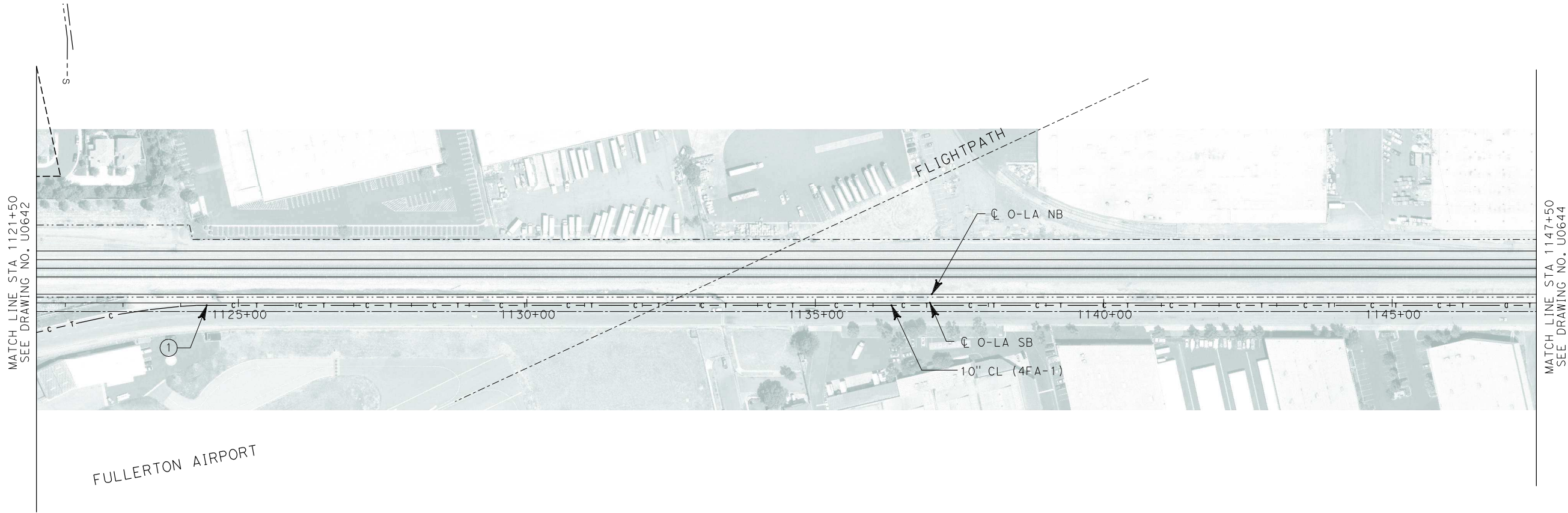
CONTRACT NO.
HSR06-0005

DRAWING NO.
U0642

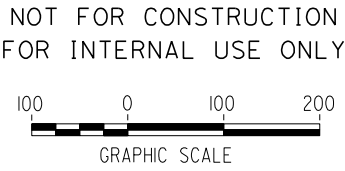
SCALE
1" = 100'

SHEET NO.
156 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CONDUIT - OC	COMMUNICATION	10" CL	STA 1124+00 TO STA 1147+50	THE CABLE TV LINE IS TOO CLOSE TO THE PROPOSED O-LA NB TRACK	LOW RISK	



REV	DATE	BY	SUB	APP	DESCRIPTION

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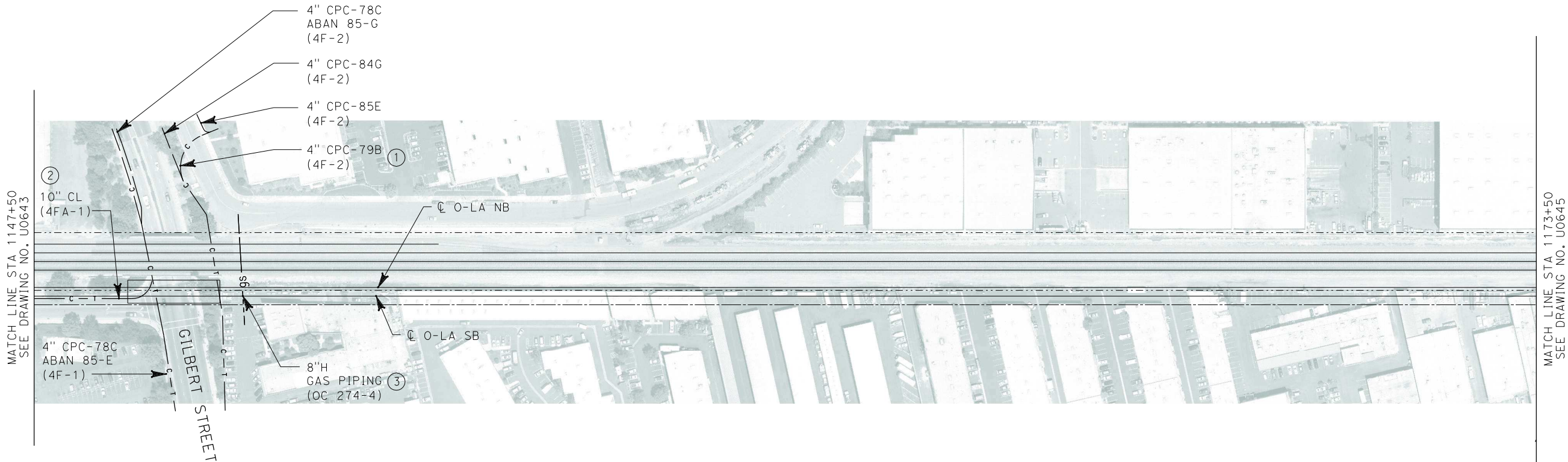


**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0643
SCALE 1" = 100'
SHEET NO. 157 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CONDUIT - OC	COMMUNICATION	4" CPC	GILBERT STREET	THE EXISTING COMMUNICATION LINE IS IN CONFLICT WITH THE WIDENING OF THE BRIDGE AT GILBERT STREET	LOW RISK	
②	CONDUIT - OC	COMMUNICATION	10" CL	STA 1147+50 TO STA 1150+00	CABLE TV IS TOO CLOSE TO THE PROPOSE TRACKS	LOW RISK	
③	THE GAS COMPANY	GAS	8" H GAS PIPINS	STA 1151+12	THE PROFILE OFF THE PROPOSED O-LA TRACKS ARE LOWER THAN EXISTING TRACKS, THEREFORE THE GAS LINE WILL BE IN CONFLICT.	HIGH RISK	

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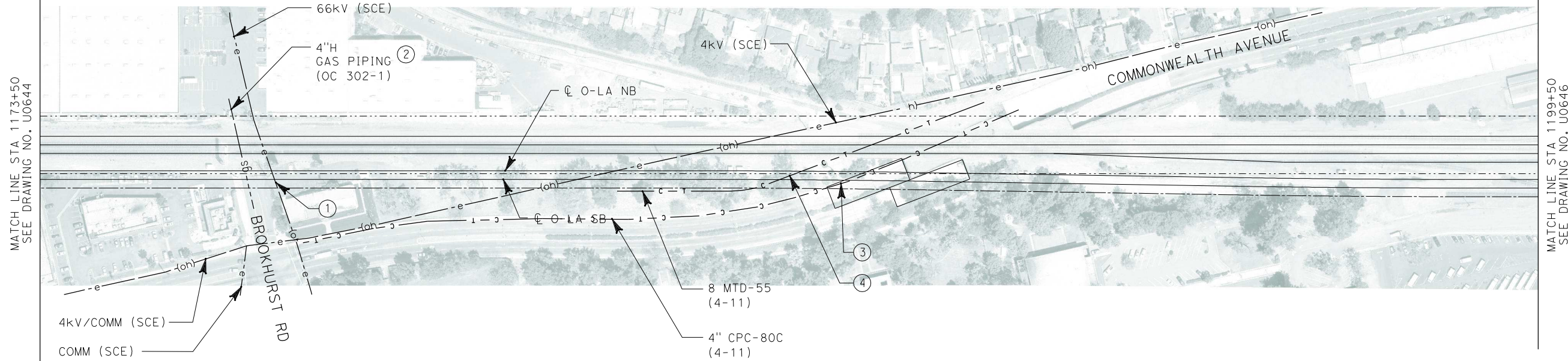


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0644
SCALE 1" = 100'
SHEET NO. 158 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66kv/COMM	BROOKHURST ROAD	THE EXISTING POWER POLE IS IN CONFLICT DUE TO THE WIDENING OF THE BROOKHURST BRIDGE	HIGH COST RELOCATION	
②	THE GAS COMPANY	GAS	4" H GAS PIPING	BROOKHURST ROAD	THE EXISTING GAS LINE IS CROSSING THE PROPOSED TRACKS THEREFORE THEY NEED TO BE ENCASED	LOW RISK	
③	CONDUIT - OC	COMMUNICATION	8 MTD	COMMONWEALTH AVE	THE EXISTING CABLE-TV LINE IS IN CONFLICT DUE TO THE WIDENING OF COMMONWEALTH BRIDGE	LOW RISK	
④	CONDUIT - OC	COMMUNICATION	8 MTD	COMMONWEALTH AVE	THE EXISTING CABLE-TV LINE IS CROSSING THE PROPOSED TRACKS THEREFORE THEY NEED TO BE ENCASED	LOW RISK	

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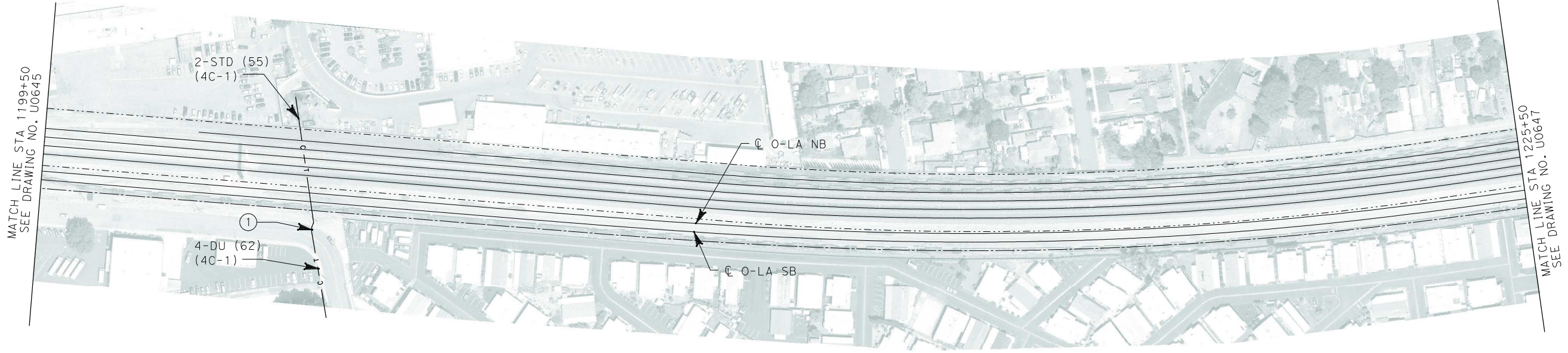
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0645
SCALE 1" = 100'
SHEET NO. 159 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CONDUIT - OC	COMMUNICATION	4-DU	STA 1204+30	THE EXISTING CABLE-TV LINE IS CROSSING THE PROPOSED TRACKS THEREFORE THEY NEED TO BE ENCASED	LOW RISK	

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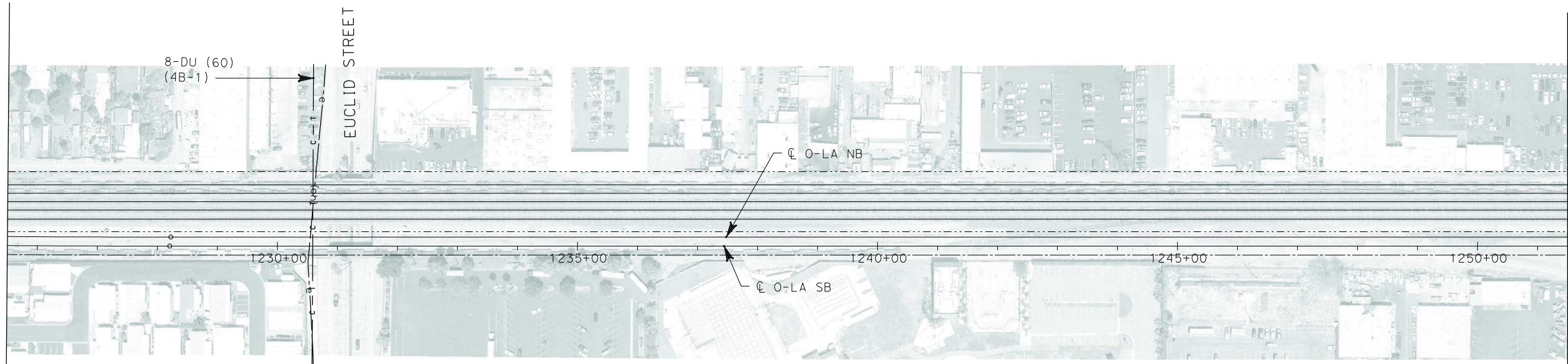
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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0646
SCALE 1" = 100'
SHEET NO. 160 OF 245

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MATCH LINE STA 1225+50
SEE DRAWING NO. U0646




MATCH LINE STA 1251+50
SEE DRAWING U0648

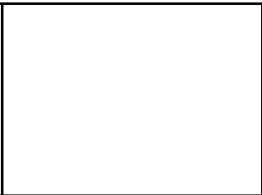
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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0647
SCALE 1" = 100'
SHEET NO. 161 OF 245



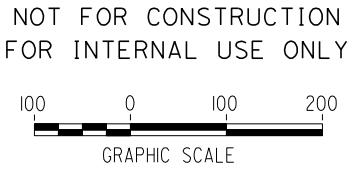
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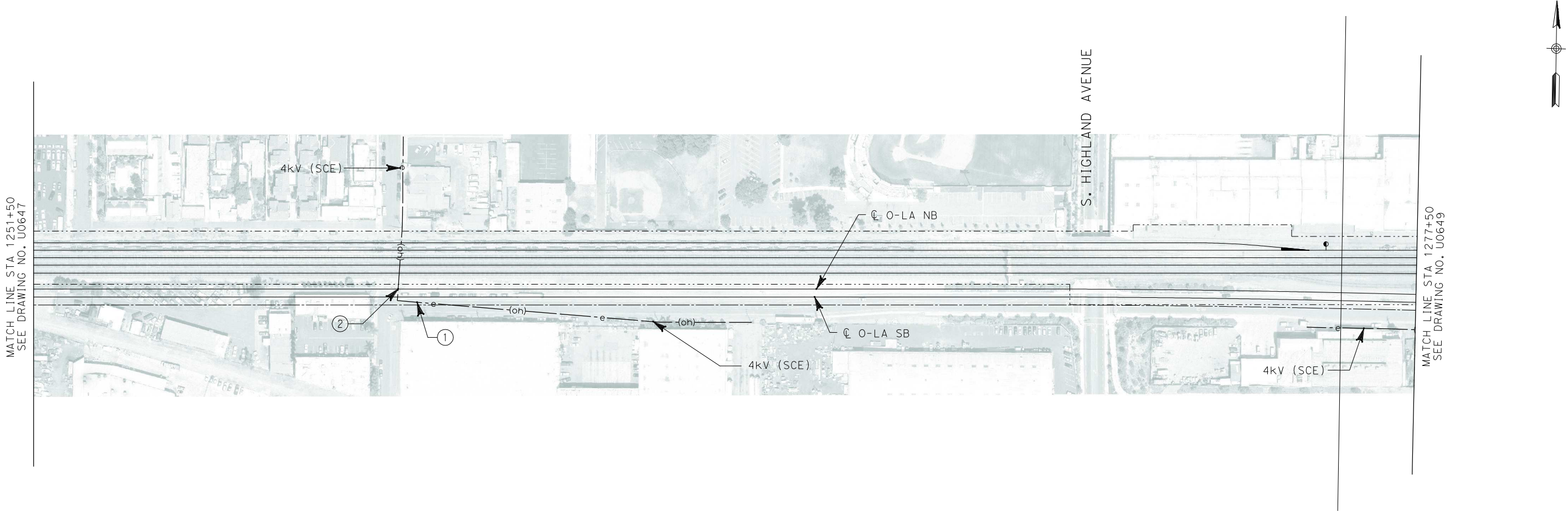
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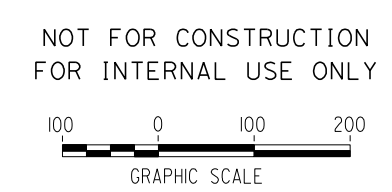
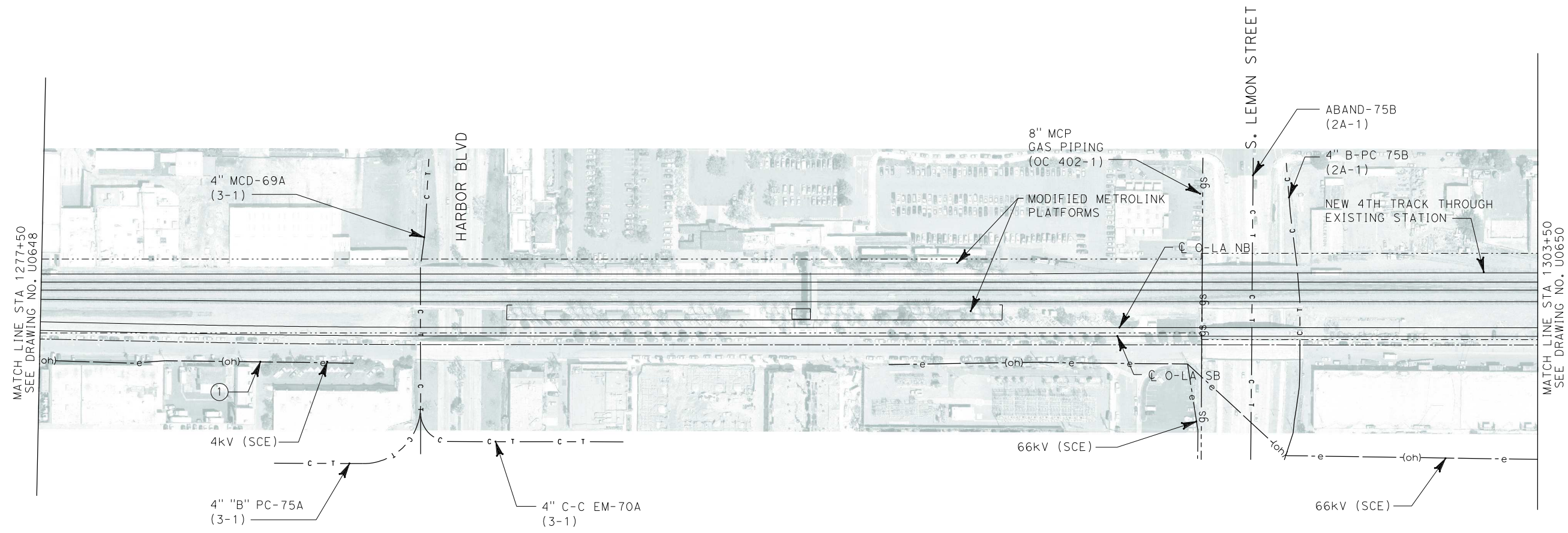


<div>CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES</div> <div>EXISTING COMPOSITE UTILITIES PLAN</div>	CONTRACT NO. HSR06-0005
	DRAWING NO. U0648
	SCALE 1" = 100'
	SHEET NO. 162 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	4 kV	STA 1258+35	THE EXISTING OVERHEAD LINES ARE IN CONFLICT WITH THE PROPOSED FULLERTON METROLINK STATION	HIGH COST RELOCATION	
②	SCE	ELECTRICAL	4 kV	STA 1258+35	THE EXISTING OVERHEAD LINES ARE TOO CLOSE TO THE PROPOSED O-LA TRACKS	HIGH COST RELOCATION	






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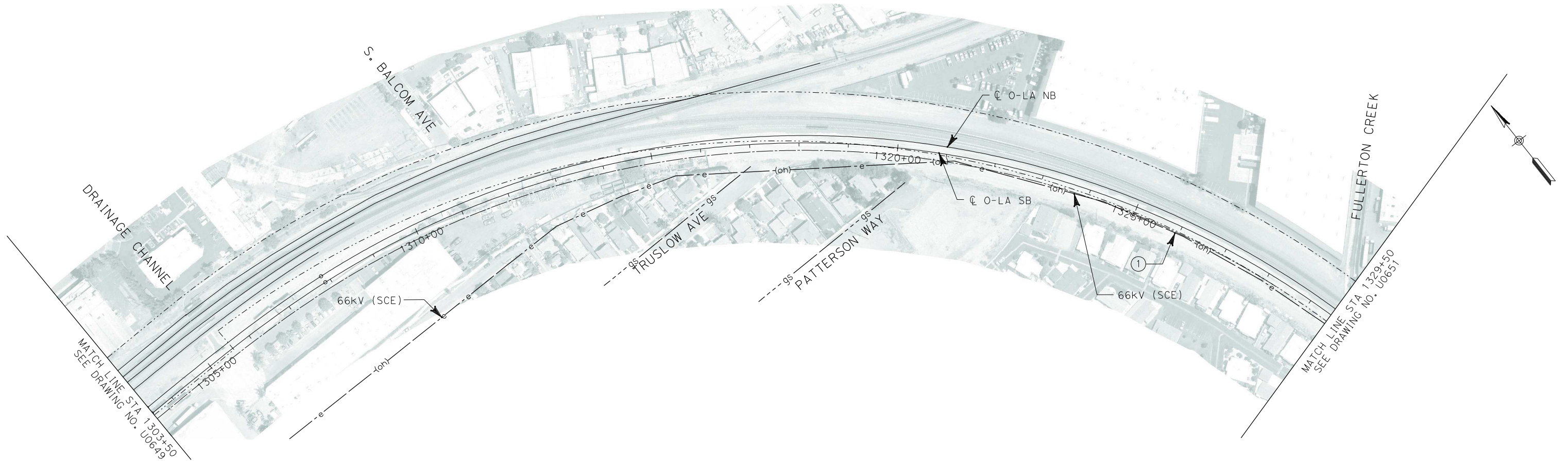
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0649
SCALE 1" = 100'
SHEET NO. 163 OF 245

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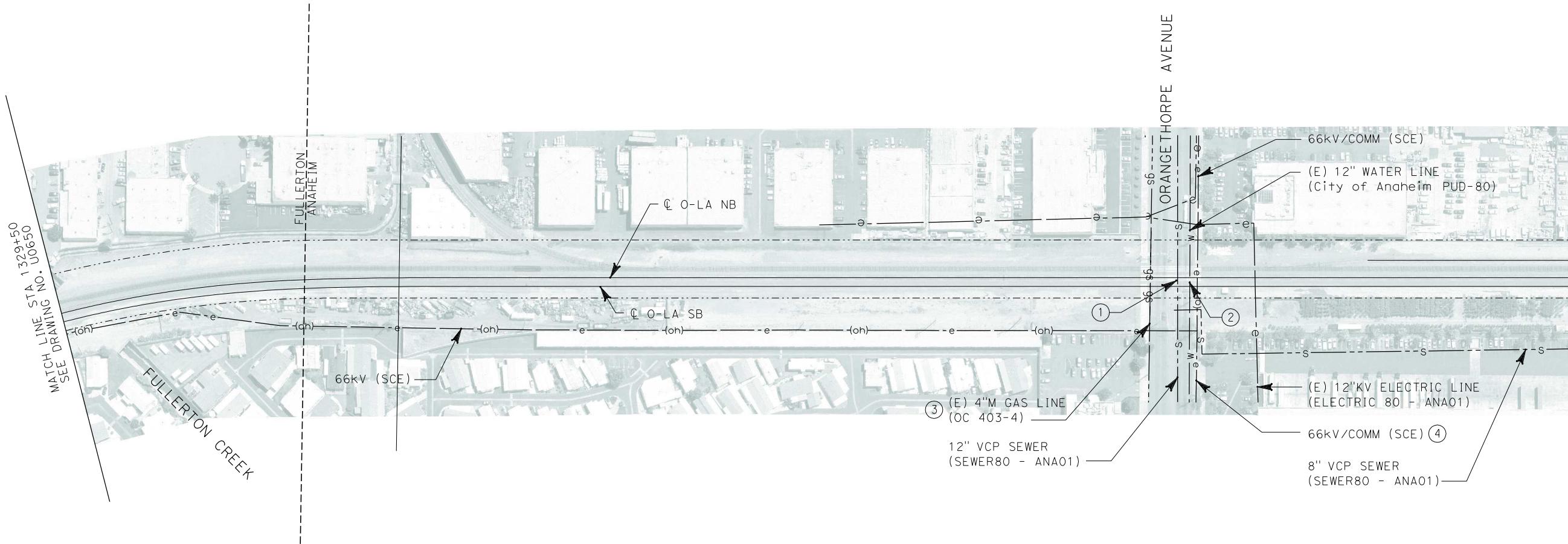
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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0650
SCALE 1" = 100'
SHEET NO. 164 OF 245

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MATCH LINE STA 1355+50
SEE DRAWING NO. U0652



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SEWER ANAHEIM	SEWER	12" VCP	ORANGETHORPE AVE	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE THE ORANGETHORPE WILL BE GRADE SEPARATED	LOW RISK	
②	CITY OF ANAHEIM	WATER	12" WATER LINE	ORANGETHORPE AVE	THE ADDITION OF THE PROPOSED O-LA TRACKS ASIDE THE EXISTING TRACKS, MIGHT CONFLICT WITH THE WATER LINE	LOW RISK	
③	THE GAS COMPANY	GAS	4" M GAS	ORANGETHORPE AVE	THE EXISTING GAS LINE IS IN CONFLICT BECAUSE THE ORANGETHROPE WILL BE GRADE SEPARATED	LOW RISK	
④	SCE	ELECTRICAL	66 kV	ORANGETHORPE AVE	THE EXISTING OVERHEAD LINE IS IN CONFLICT BECAUSE THE ORANGETHROPE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	

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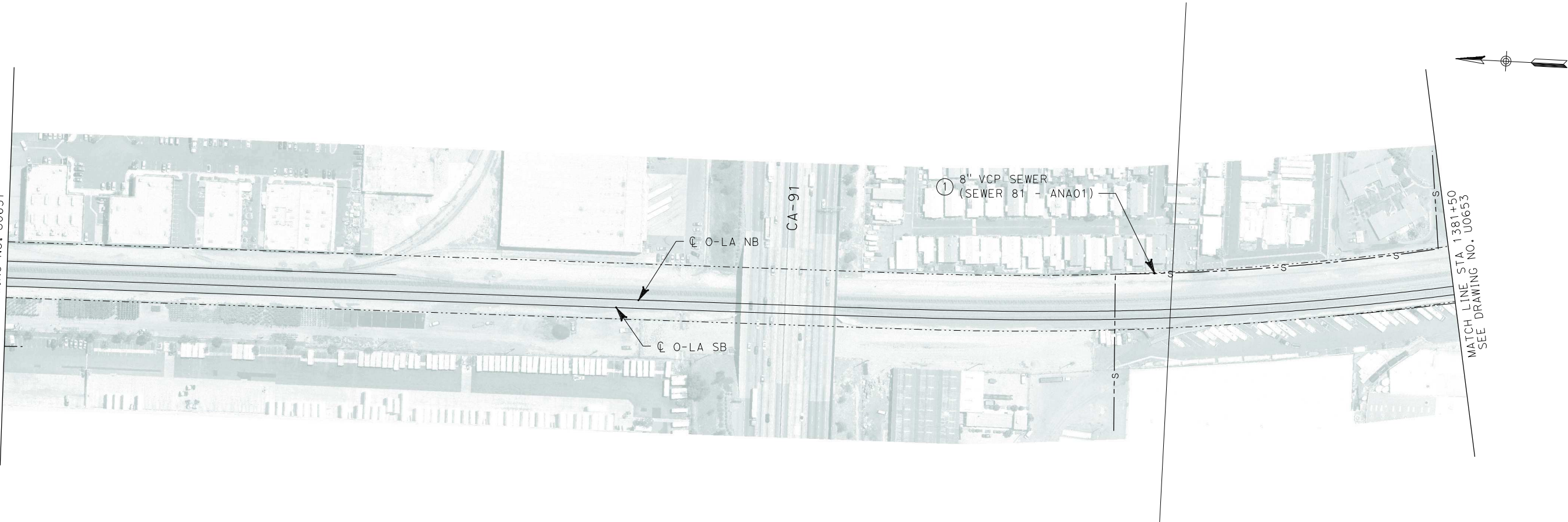
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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0651
SCALE 1" = 100'
SHEET NO. 165 OF 245

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MATCH LINE STA 1355+50
SEE DRAWING NO. U0651



MATCH LINE STA 1381+50
SEE DRAWING NO. U0653


No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CITY OF ANAHEIM	SEWER	8" VCP	STA 1375+40	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE NEED TO BE ENCASED	LOW RISK	

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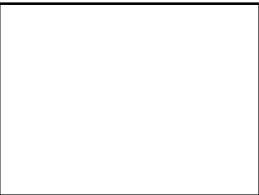


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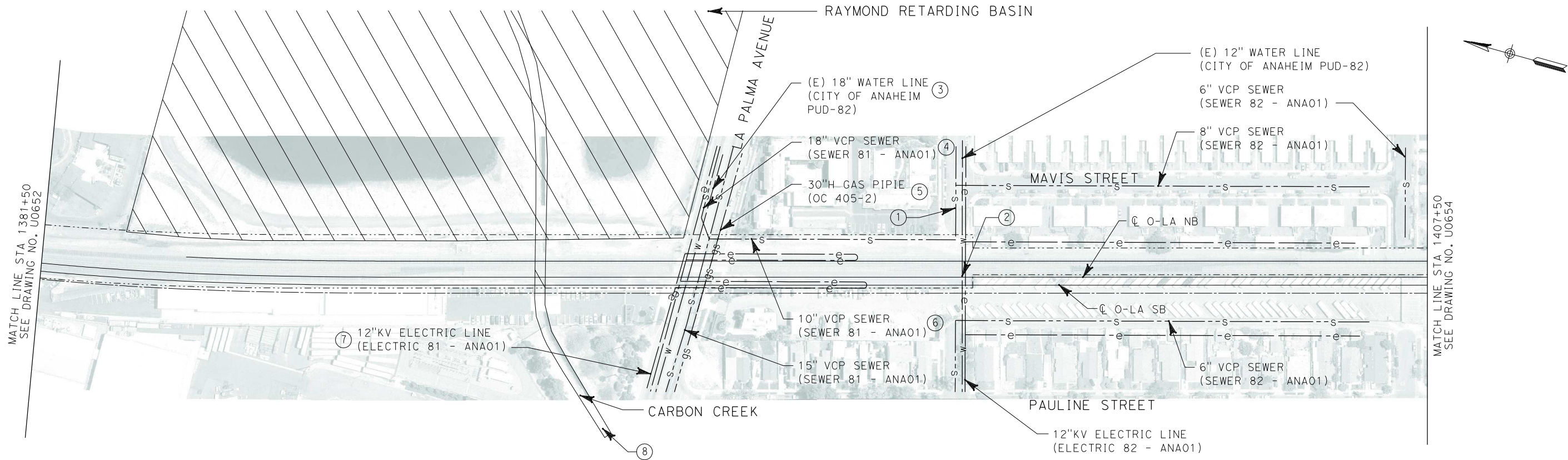


CALIFORNIA HIGH SPEED RAIL AUTHORITY
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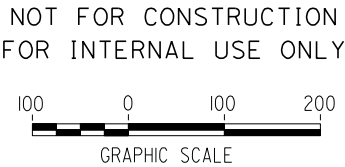
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0652
SCALE 1" = 100'
SHEET NO. 166 OF 245




No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SEWER ANAHEIM	SEWER	8" VCP	LA PALMA AVE	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE NEED TO BE ENCASED	LOW RISK	
②	CITY OF ANAHEIM	WATER	12" WATER LINE	LA PALMA AVE	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE NEED TO BE ENCASED	LOW RISK	
③	CITY OF ANAHEIM	WATER	18" WATER LINE	LA PALMA AVE	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	LOW RISK	
④	CITY OF ANAHEIM	SEWER	18" VCP	LA PALMA AVE	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	LOW RISK	
⑤	THE GAS COMPANY	GAS	30" H	LA PALMA AVE	THE EXISTING GAS LINE IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	HIGH RISK	
⑥	CITY OF ANAHEIM	SEWER	10" VCP	LA PALMA AVE	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	LOW RISK	
⑦	CITY OF ANAHEIM	ELECTRICAL	12" KV ELECTRICAL LINE	LA PALMA AVE	THE EXISTING ELECTRICAL LINE IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	LOW RISK	
⑧	OCFCD - CARBON CREEK	STORM DRAIN	FLOOD CONTROL CHANNEL	LA PALMA AVE	EXISTING CHANNEL IS IN CONFLICT BECAUSE LA PALMA AVE WILL BE GRADE SEPARATED	LOW RISK/HIGH RISK	



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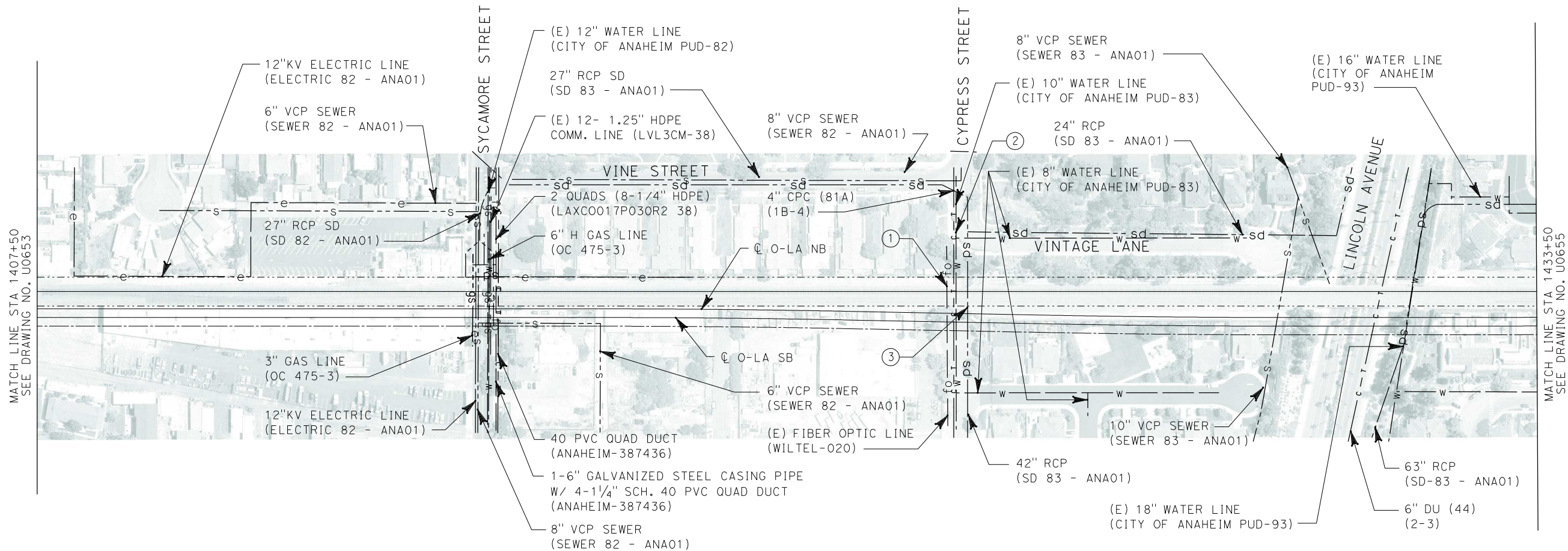


**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0653
SCALE 1" = 100'
SHEET NO. 167 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CONDUIT - OC	COMMUNICATION	FIBER OPTIC	CYPRESS STREET	THE EXISTING FIBER OPTIC LINE IS CROSSING THE PROPOSED TRACKS, THEREFORE NEED TO BE ENCASED	LOW RISK	
②	CITY OF ANAHEIM	WATER	10" WATER LINE	CYPRESS STREET	THE EXISTING WATER LINE IS CROSSING THE PROPOSED TRACKS, THEREFORE NEED TO BE ENCASED	LOW RISK	
③	CITY OF ANAHEIM	STORM DRAIN	42" RCP	CYPRESS STREET	THE EXISTING STORM DRAIN IS CROSSING THE PROPOSED TRACKS, THEREFORE NEED TO BE ENCASED	HIGH COST RELOCATION	

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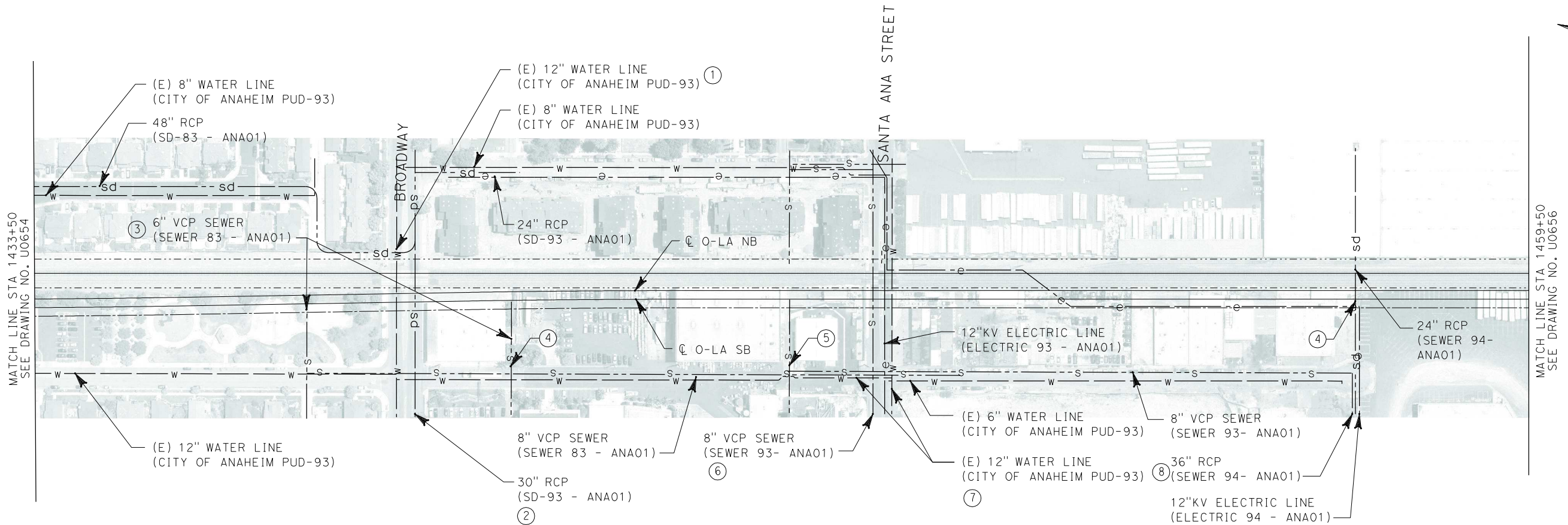
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0654
SCALE 1" = 100'
SHEET NO. 168 OF 245

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
No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CITY OF ANAHEIM	WATER	12" WATER	BROADWAY STREET	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE BROADWAY STREET WILL BE GRADE SEPARATED	LOW RISK	
②	CITY OF ANAHEIM	STORM DRAIN	30" STROM DRAIN	BROADWAY STREET	THE EXISTING STORM DRAIN IS IN CONFLICT BECAUSE BROADWAY STREET WILL BE GRADE SEPARATED	LOW RISK	
③	CITY OF ANAHEIM	SEWER	6" VCP SEWER	STA 1438+20	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
④	CITY OF ANAHEIM	SEWER	6" VCP SEWER	STA 1441+77	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
⑤	CITY OF ANAHEIM	SEWER	6" VCP SEWER	STA 1446+63	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
⑥	CITY OF ANAHEIM	SEWER	8" VCP SEWER	SANTA ANA STREET	THE EXISTING SEWER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
⑦	CITY OF ANAHEIM	WATER	12" WATER	SANTA ANA STREET	THE EXISTING WATER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
⑧	CITY OF ANAHEIM	STORM DRAIN	36" RCP	STA 1456+88	THE EXISTING STORM DRAIN LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	

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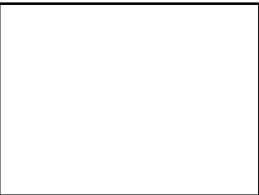


REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
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CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



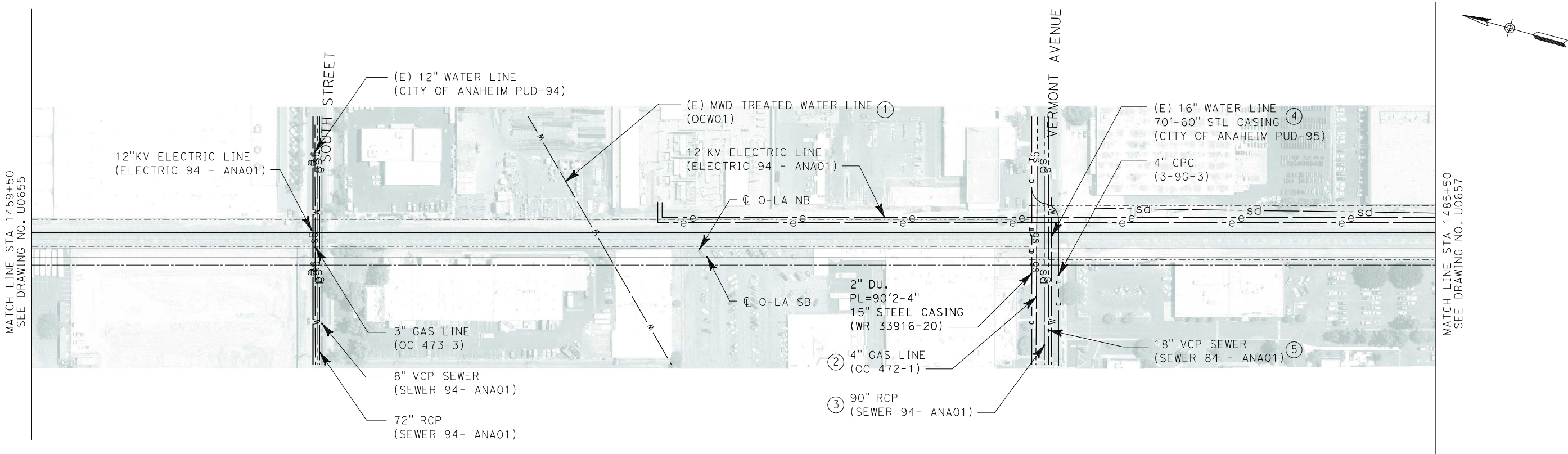
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



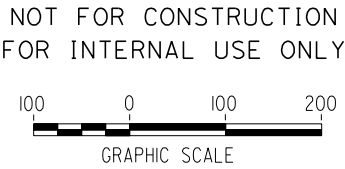
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0655
SCALE 1" = 100'
SHEET NO. 169 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	MWD	WATER	TREATED WATER LINE	STA 1470+00	THE EXISTING WATER LINE IS CROSSING THE PROPOSED TRACKS THEREFORE NEED TO BE ENCASED	LOW RISK	
②	THE GAS COMPANY	GAS	4" GAS	VERMONT AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE VERMONT AVE WILL BE GRADE SEPARATED	LOW RISK	
③	CITY OF ANAHEIM	STORM DRAIN	90" RCP	VERMONT AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE VERMONT AVE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
④	CITY OF ANAHEIM	WATER	16" WATER	VERMONT AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE VERMONT AVE WILL BE GRADE SEPARATED	LOW RISK	
⑤	CITY OF ANAHEIM	SEWER	18" VCP SEWER	VERMONT AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE VERMONT AVE WILL BE GRADE SEPARATED	LOW RISK	



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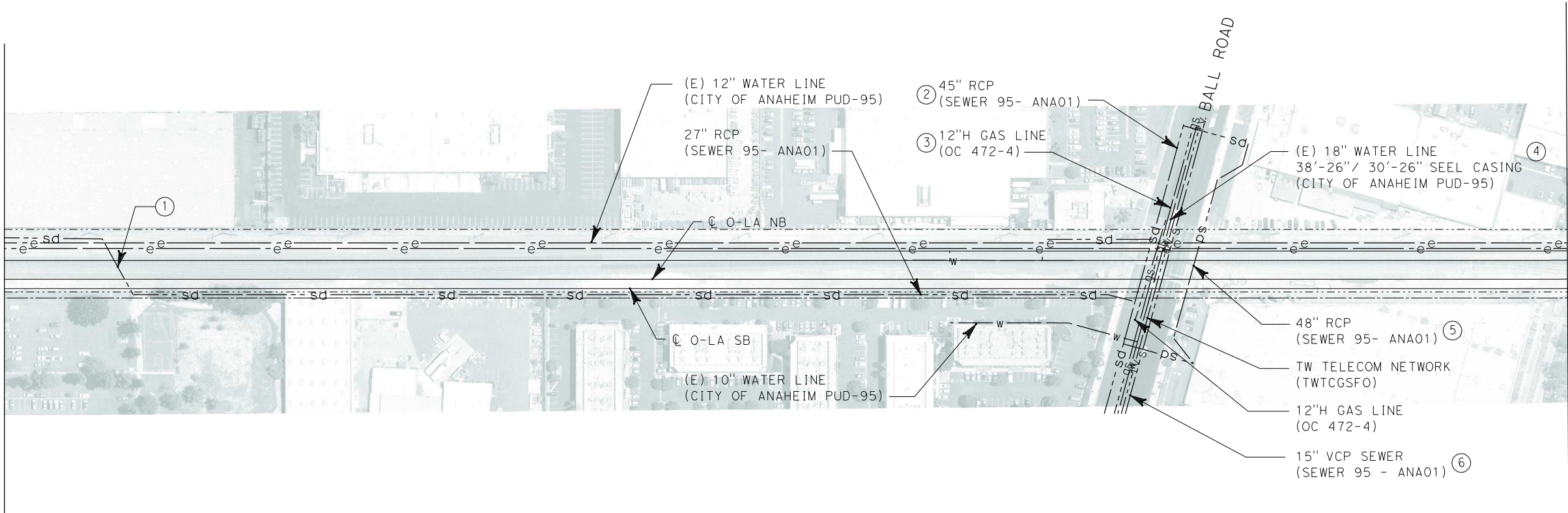
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0656
SCALE 1" = 100'
SHEET NO. 170 OF 245

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MATCH LINE STA 1485+50
SEE DRAWING NO. U0656



MATCH LINE STA 1510+50
SEE DRAWING NO. U0658



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CITY OF ANAHEIM	STORM DRAIN	27" RCP	STA 1487+40	THE EXISTING STORM DRAIN LINE IS CROSSING THE PROPOSED TRACKS THEREFORE WILL NEED TO BE ENCASED	LOW RISK	
②	CITY OF ANAHEIM	STORM DRAIN	45" RCP	BALL ROAD	THE EXISTING STORM DRAIN IS IN CONFLICT BECAUSE BALL ROAD WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
③	THE GAS COMPANY	GAS	12" H GAS	BALL ROAD	THE EXISTING GAS LINE IS IN CONFLICT BECAUSE BALL ROAD WILL BE GRADE SEPARATED	HIGH RISK	
④	CITY OF ANAHEIM	WATER	18" WATER	BALL ROAD	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE BALL ROAD WILL BE GRADE SEPARATED	LOW RISK	
⑤	CITY OF ANAHEIM	STORM DRAIN	45" RCP	BALL ROAD	THE EXISTING STORM DRAIN IS IN CONFLICT BECAUSE BALL ROAD WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
⑥	CITY OF ANAHEIM	SEWER	15" VCP SEWER	BALL ROAD	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE BALL ROAD WILL BE GRADE SEPARATED	LOW RISK	

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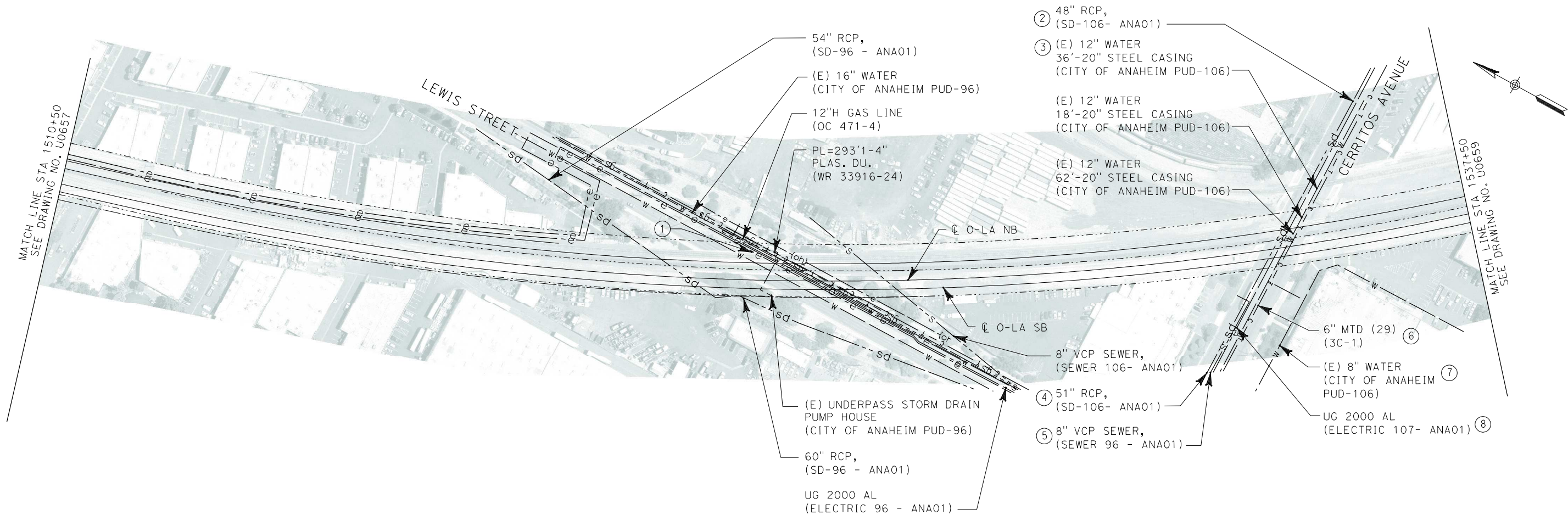
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LOS ANGELES, CA 90017-2556



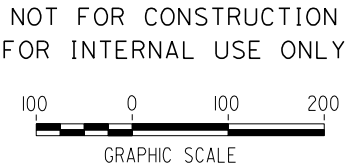
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0657
SCALE 1" = 100'
SHEET NO. 171 OF 245



No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	SCE	ELECTRICAL	66 kV	LEWIS STREET	THE EXISTING ELECTRICAL LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
②	CITY OF ANAHEIM	STORM DRAIN	48" RCP	CERRITOS AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
③	CITY OF ANAHEIM	WATER	12" WATER	CERRITOS AVE	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	LOW RISK	
④	CITY OF ANAHEIM	STORM DRAIN	51" RCP	CERRITOS AVE	THE EXISTING STORM DRAIN LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	
⑤	CITY OF ANAHEIM	SEWER	8" VCP	CERRITOS AVE	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	LOW RISK	
⑥	CONDUIT - OC	COMMUNICATION	6" MTD	CERRITOS AVE	THE EXISTING COMMUNICATION LINE IS IN CONFLICT BECAUSE CERRITOS AVE WILL BE GRADE SEPARATED	LOW RISK	
⑦	CITY OF ANAHEIM	WATER	8" WATER	CERRITOS AVE	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE CERRITO AVE WILL BE GRADE SEPARATED	LOW RISK	
⑧	CITY OF ANAHEIM	ELECTRICAL	UG 2000 AL	CERRITOS AVE	THE EXISTING ELECTRICAL LINE IS IN CONFLICT BECAUSE CERRITO AVE WILL BE GRADE SEPARATED	HIGH COST RELOCATION	



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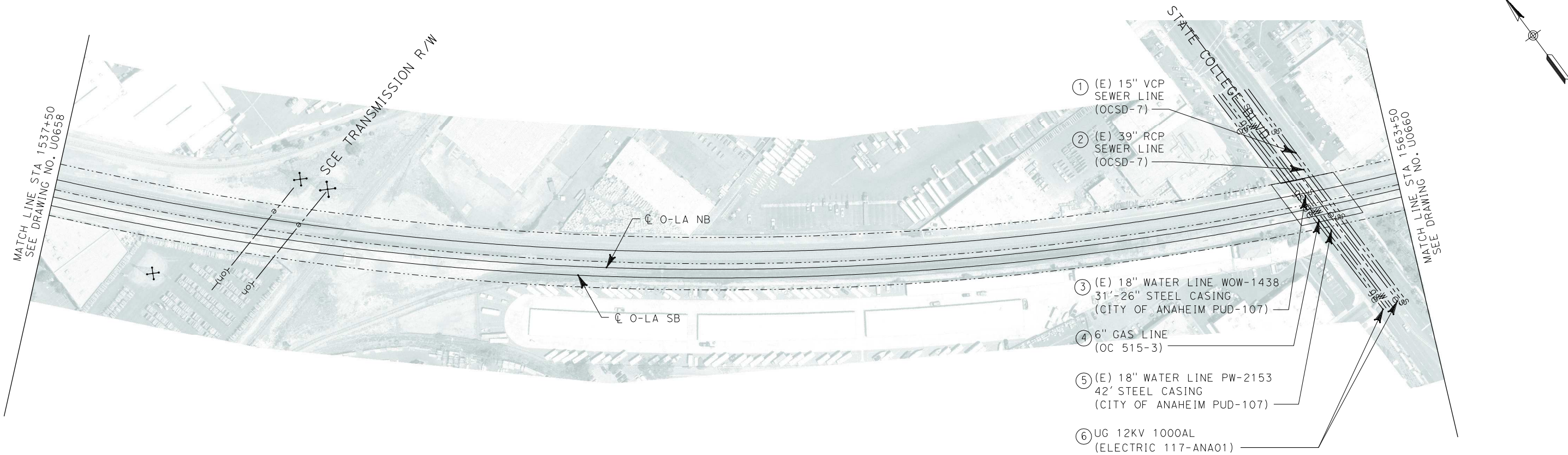


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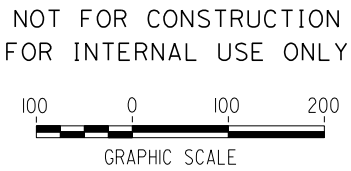
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0658
SCALE 1" = 100'
SHEET NO. 172 OF 245




No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	ORANGE COUNTY SANTITATION DISTRICT	SEWER	15" VCP	STATE COLLEGE BLVD	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	LOW RISK	
②	ORANGE COUNTY SANTITATION DISTRICT	SEWER	39" RCP	STATE COLLEGE BLVD	THE EXISTING SEWER LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	LOW RISK	
③	CITY OF ANAHEIM	WATER	18" WATER	STATE COLLEGE BLVD	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	LOW RISK	
④	THE GAS COMPANY	GAS	6" GAS	STATE COLLEGE BLVD	THE EXISTING GAS LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	HIGH RISK	
⑤	CITY OF ANAHEIM	WATER	18" WATER	STATE COLLEGE BLVD	THE EXISTING WATER LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	LOW RISK	
⑥	CITY OF ANAHEIM	ELECTRICAL	UG 12 KV 1000 AL	STATE COLLEGE BLVD	THE EXISTING ELECTRICAL LINE IS IN CONFLICT BECAUSE STATE COLLEGE BLVD WILL BE GRADE SEPARATED	HIGH COST RELOCATION	



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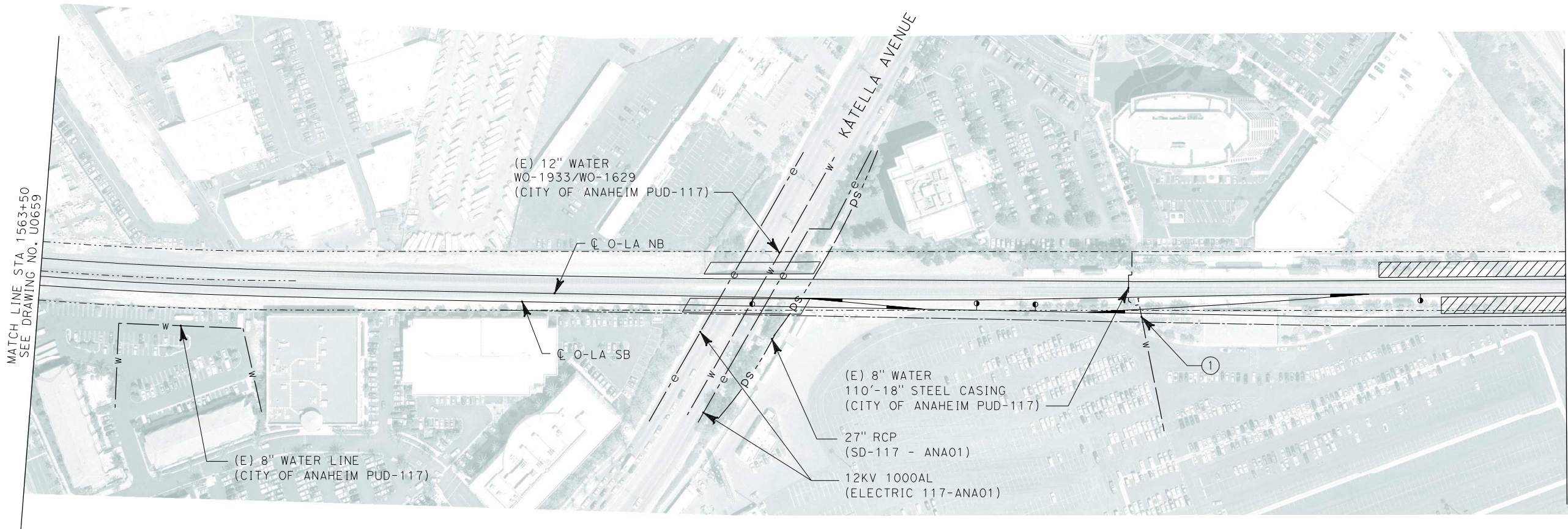
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ORANGE COUNTY TO LOS ANGELES

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0659
SCALE 1" = 100'
SHEET NO. 173 OF 245

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No.	COMPANY	TYPE OF UTILITY	DESCRIPTION	LOCATION	CONFLICT	TYPE OF CONFLICT	DISPOSITION
①	CITY OF ANAHEIM	WATER	8" WATER LINE	STA 1582+30	THE EXISTING WATER LINE IS CROSSING THE PROPOSED TRACKS, THEREFORE NEED TO BE ENCASED	LOW RISK	

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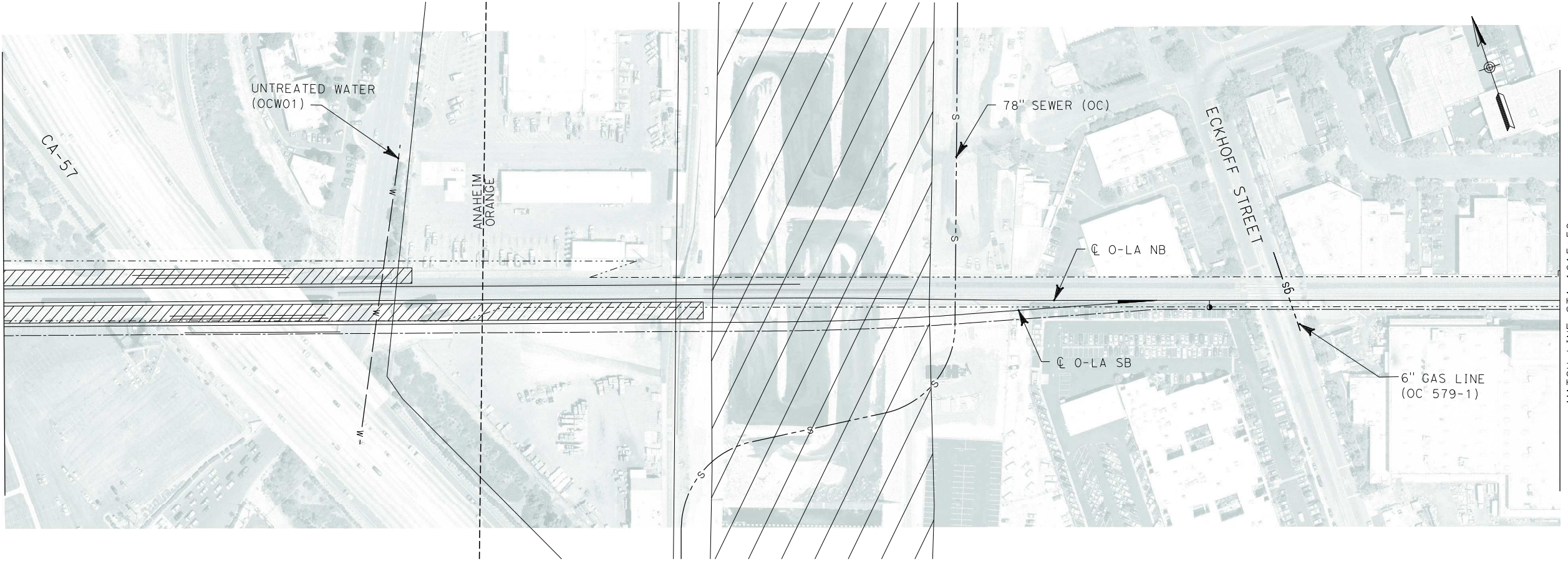
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0660
SCALE 1" = 100'
SHEET NO. 174 OF 245

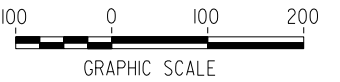
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SEE DRAWING NO. U0660




MATCH LINE STA 1615+50
SEE DRAWING NO. U0662

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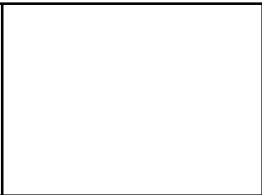


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ORANGE COUNTY TO LOS ANGELES**

EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0661
SCALE 1" = 100'
SHEET NO. 175 OF 245

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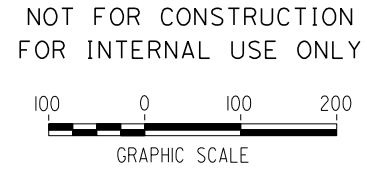
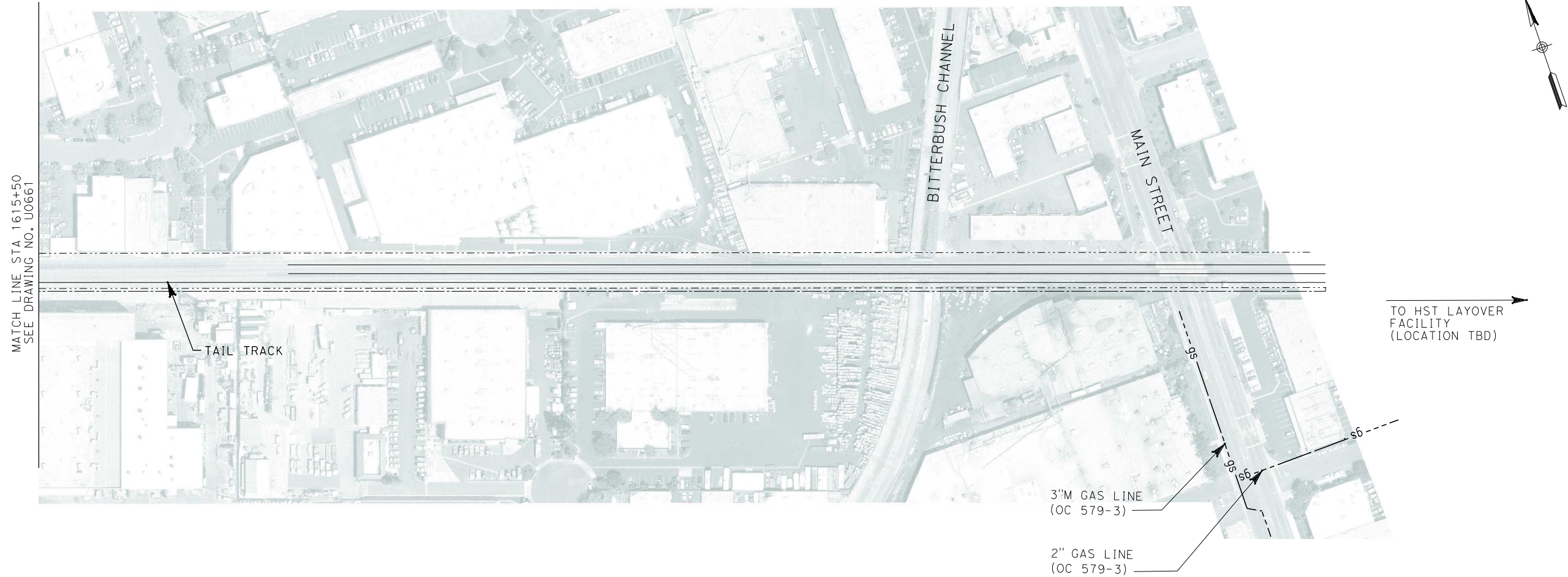
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ORANGE COUNTY TO LOS ANGELES

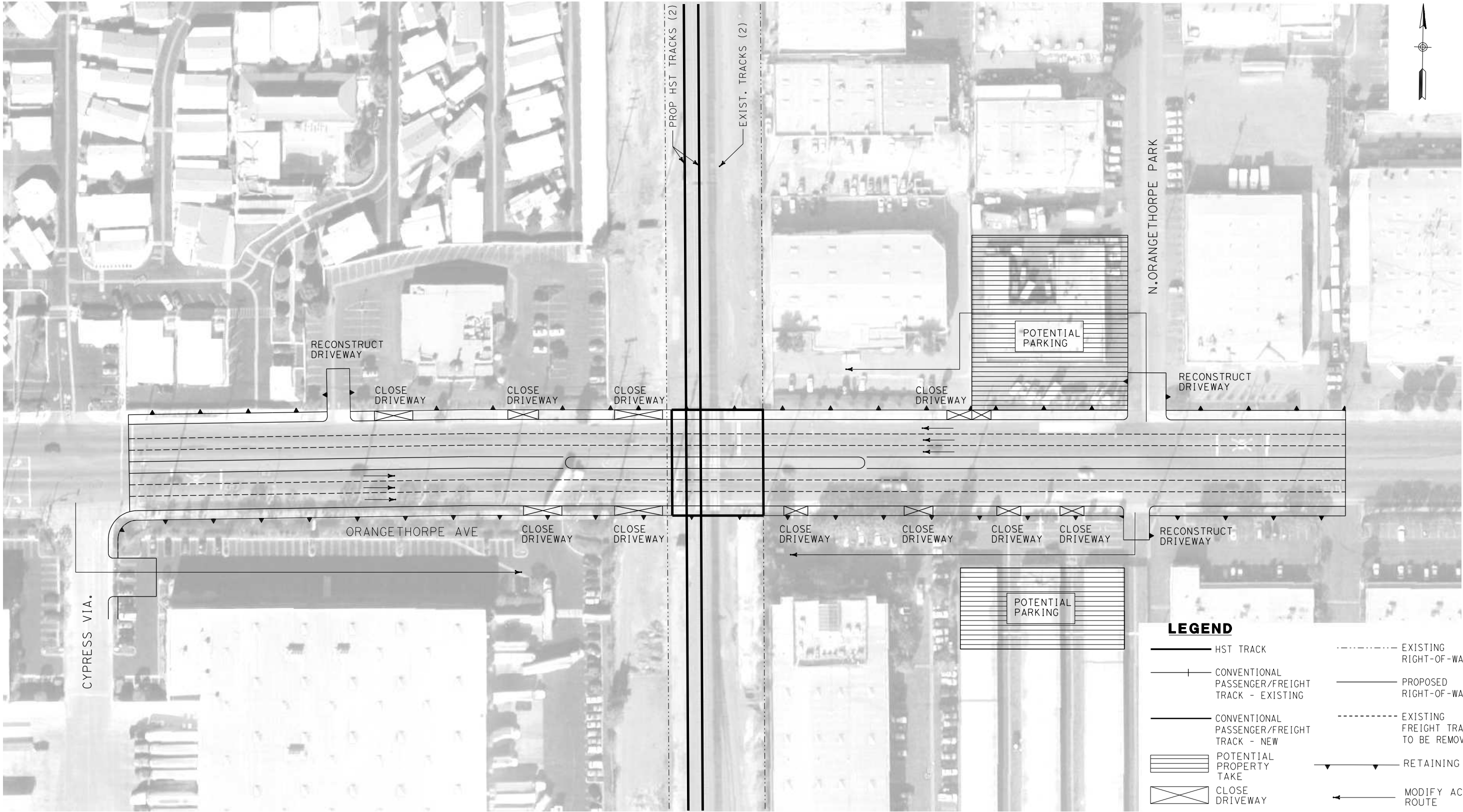
EXISTING COMPOSITE UTILITIES
PLAN

CONTRACT NO. HSR06-0005
DRAWING NO. U0662
SCALE 1" = 100'
SHEET NO. 176 OF 245



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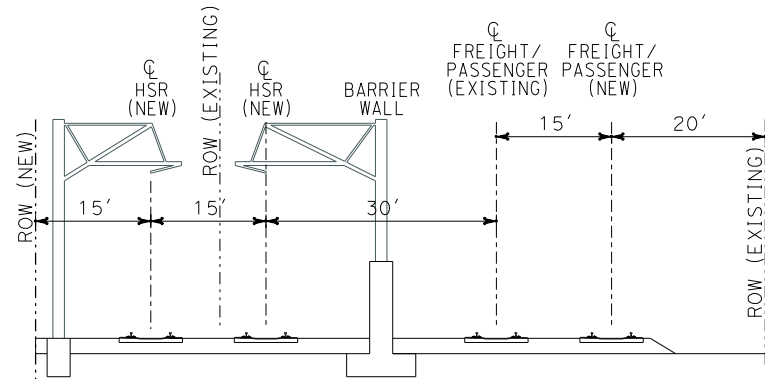
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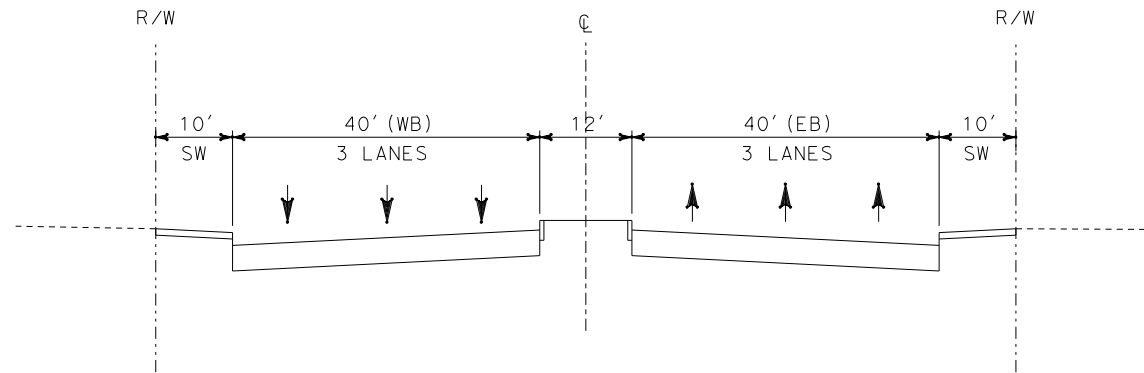
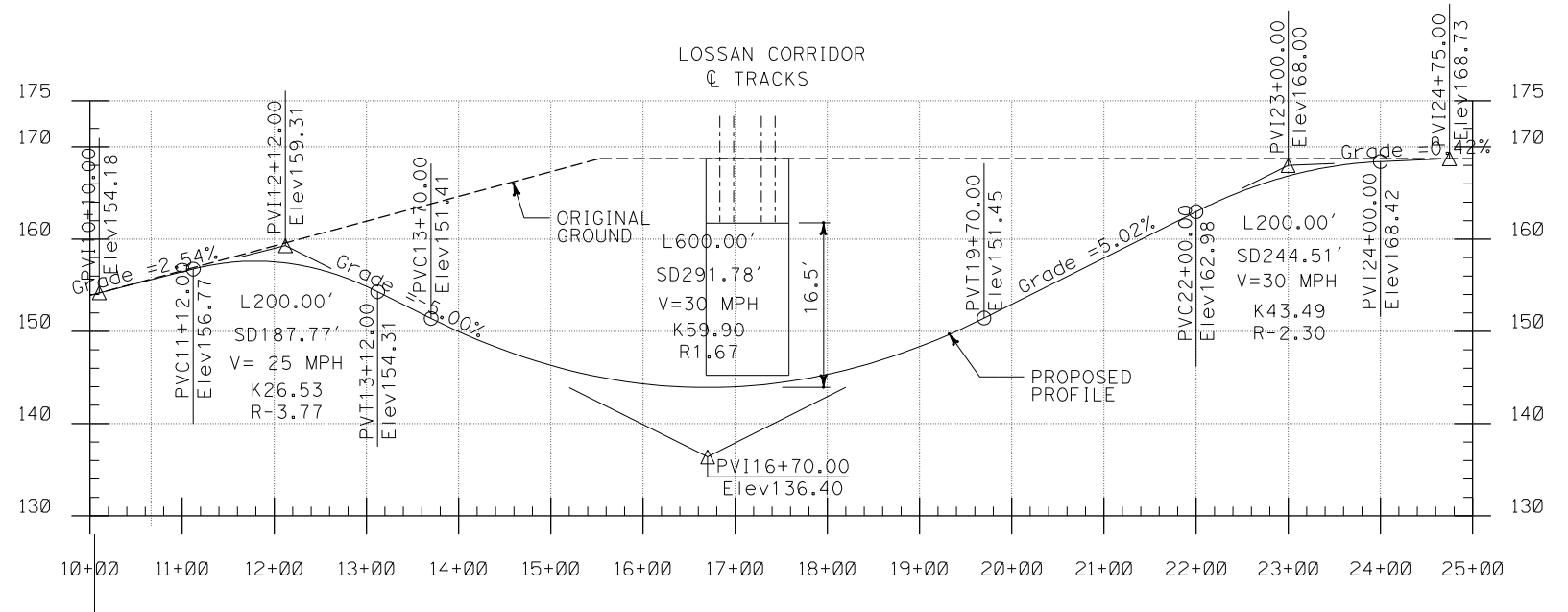
**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

ORANGETHORPE AVE. GRADE SEPARATION
PLAN

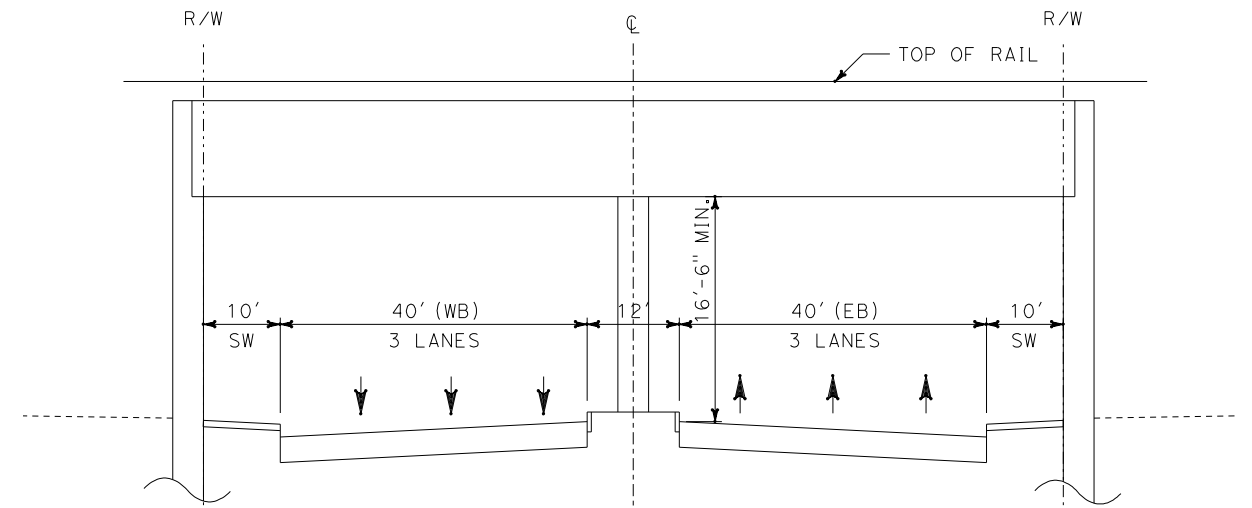
CONTRACT NO. HSR06-0005
DRAWING NO. C0410
SCALE AS SHOWN
SHEET NO. 177 OF 245



DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



**EXISTING ORANGETHORPE - EAST AND WEST OF CROSSING
(LOOKING EAST)**



**PROPOSED ORANGETHORPE - AT CROSSING
(LOOKING EAST)**

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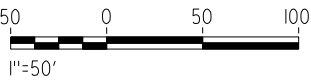
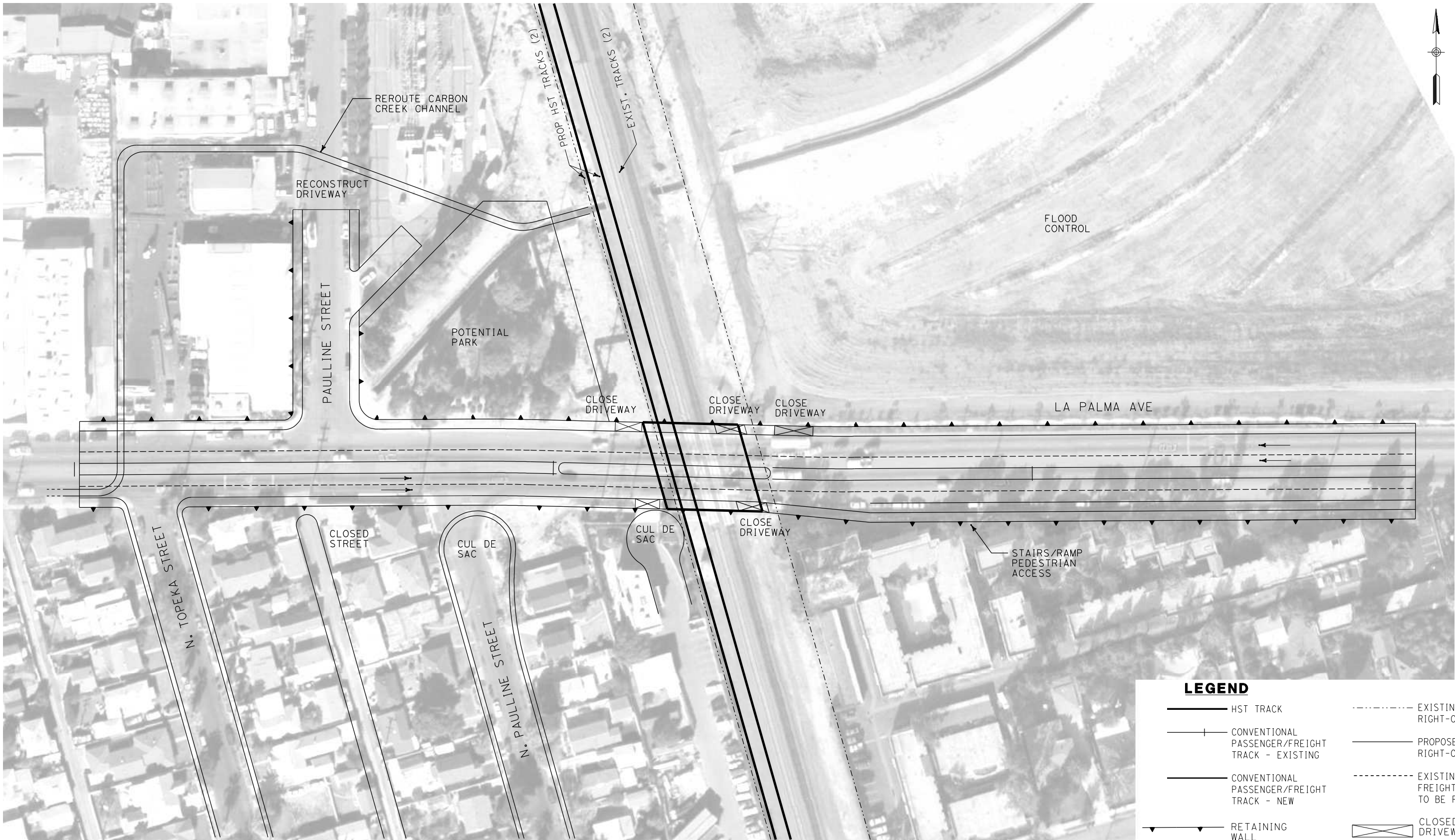
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ORANGE COUNTY TO LOS ANGELES**
ORANGETHORPE AVE. GRADE SEPARATION
PROFILE AND SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. C0300
SCALE AS SHOWN
SHEET NO. 178 OF 245


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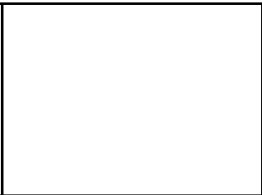


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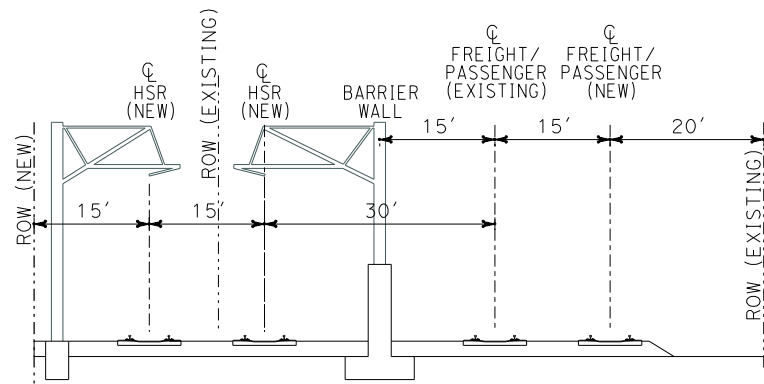
REV	DATE	BY	SUB	APP	DESCRIPTION

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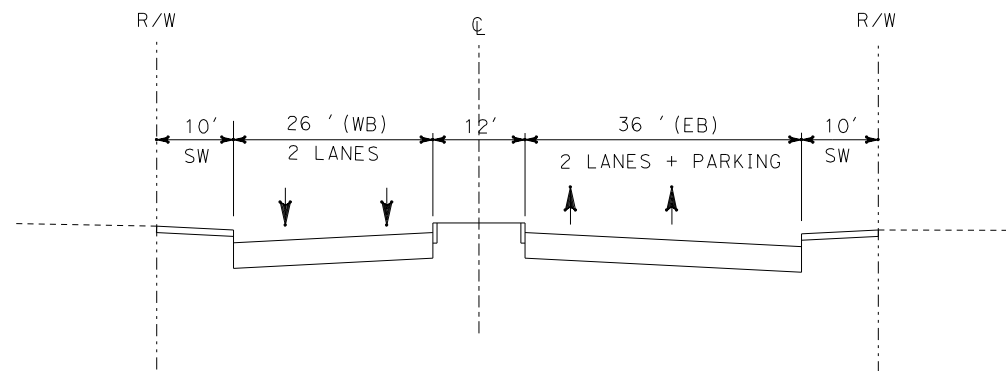
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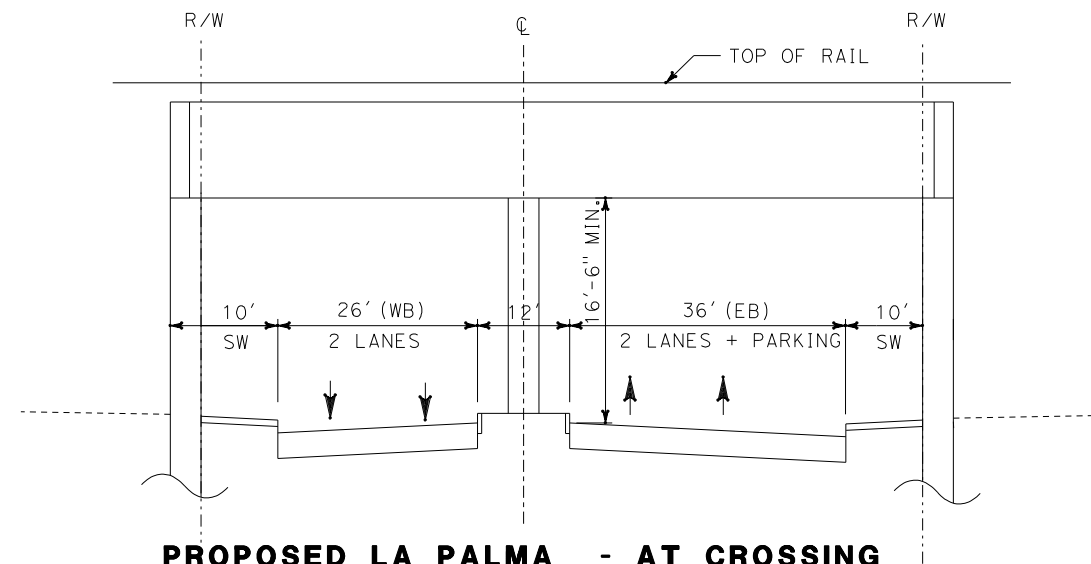
CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES		CONTRACT NO. HSR06-0005
LA PALMA AVE. GRADE SEPARATION PLAN		DRAWING NO. C0411
		SCALE AS SHOWN
		SHEET NO. 179 OF 245



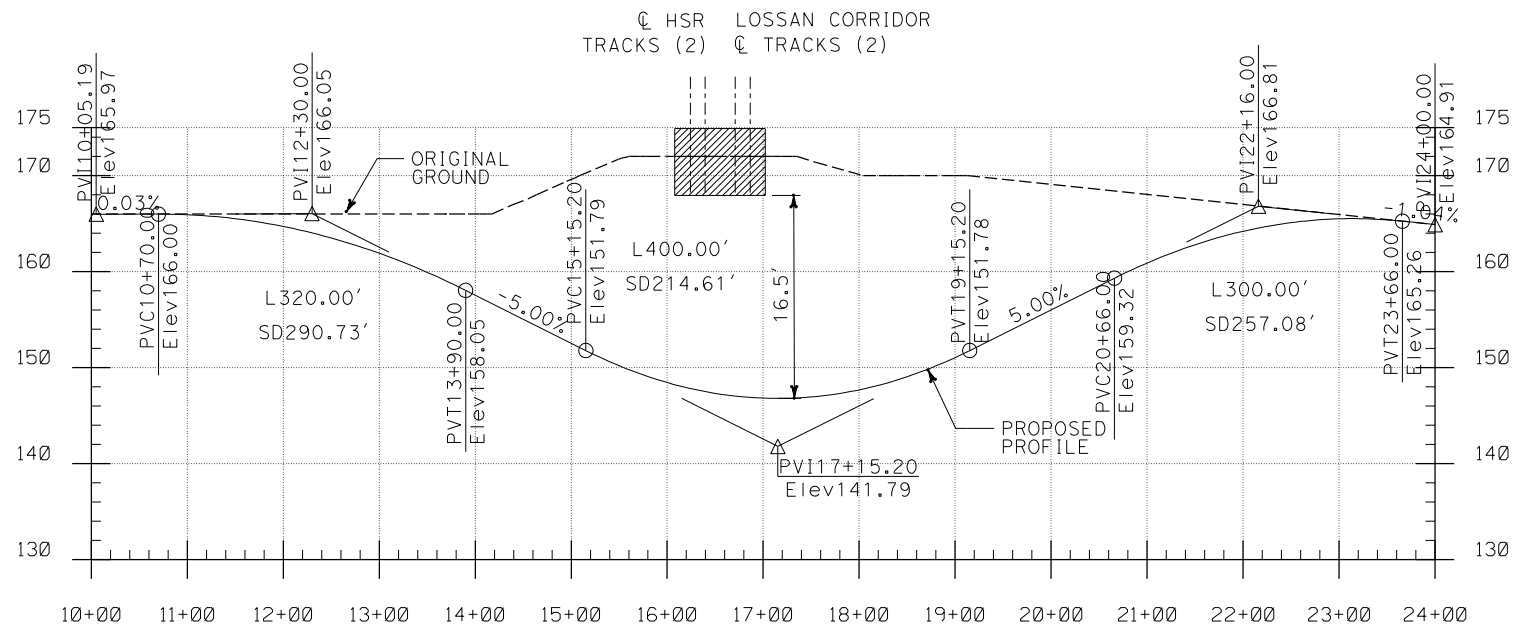
DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



**EXISTING LA PALMA - EAST AND WEST OF CROSSING
(LOOKING EAST)**



**PROPOSED LA PALMA - AT CROSSING
(LOOKING EAST)**



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**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

LA PALMA AVE. GRADE SEPARATION
PROFILE AND SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. C0301
SCALE AS SHOWN
SHEET NO. 180 OF 245

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\$\$\$\$\$SYTIME\$\$\$\$\$



REV	DATE	BY	SUB	APP	DESCRIPTION

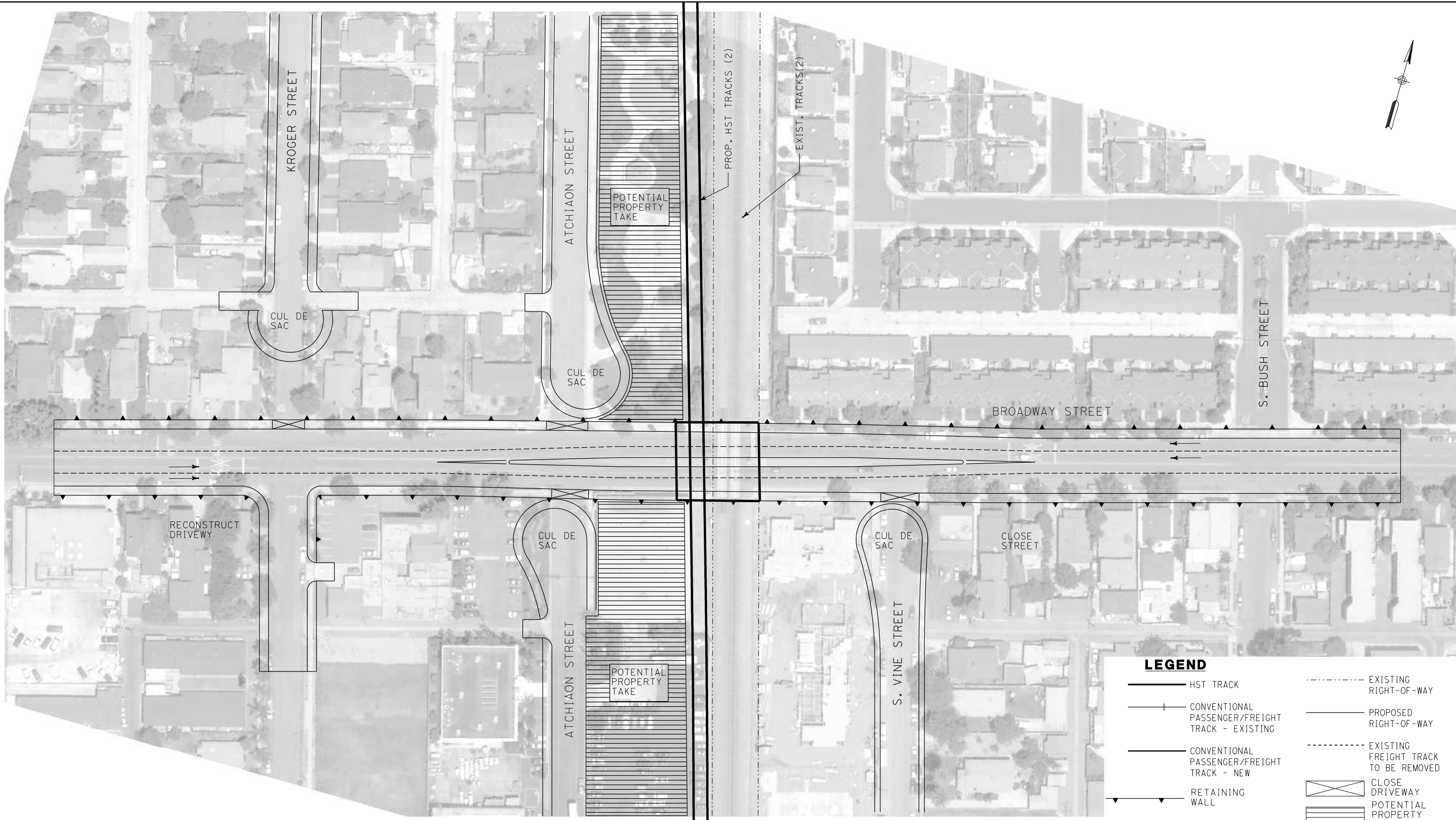
DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

BROADWAY STREET GRADE SEPARATION
PLAN

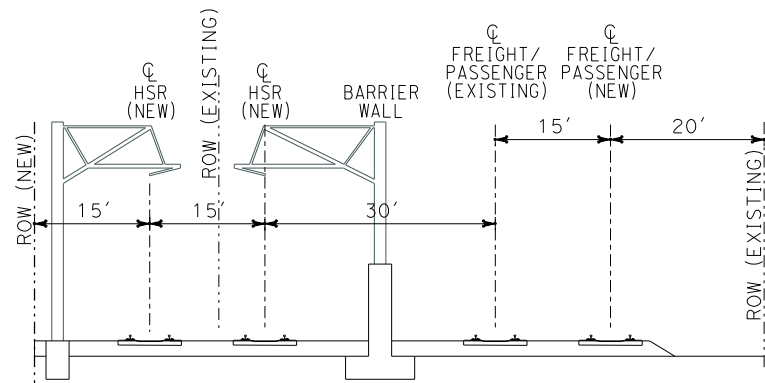
CONTRACT NO. HSR06-0005
DRAWING NO. C0412
SCALE AS SHOWN
SHEET NO. 181 OF 245



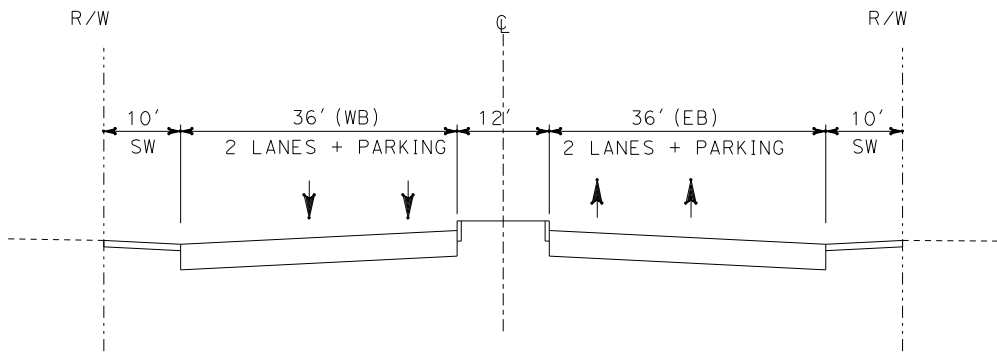
LEGEND

- HST TRACK
- CONVENTIONAL PASSENGER/FREIGHT TRACK - EXISTING
- CONVENTIONAL PASSENGER/FREIGHT TRACK - NEW
- RETAINING WALL
- EXISTING RIGHT-OF-WAY
- PROPOSED RIGHT-OF-WAY
- EXISTING FREIGHT TRACK TO BE REMOVED
- CLOSE DRIVEWAY
- POTENTIAL PROPERTY TAKE

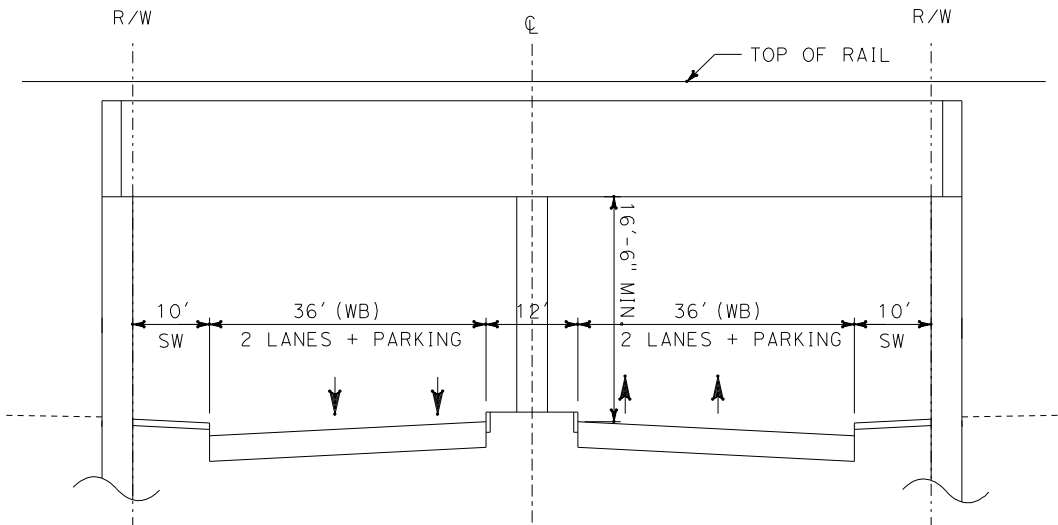
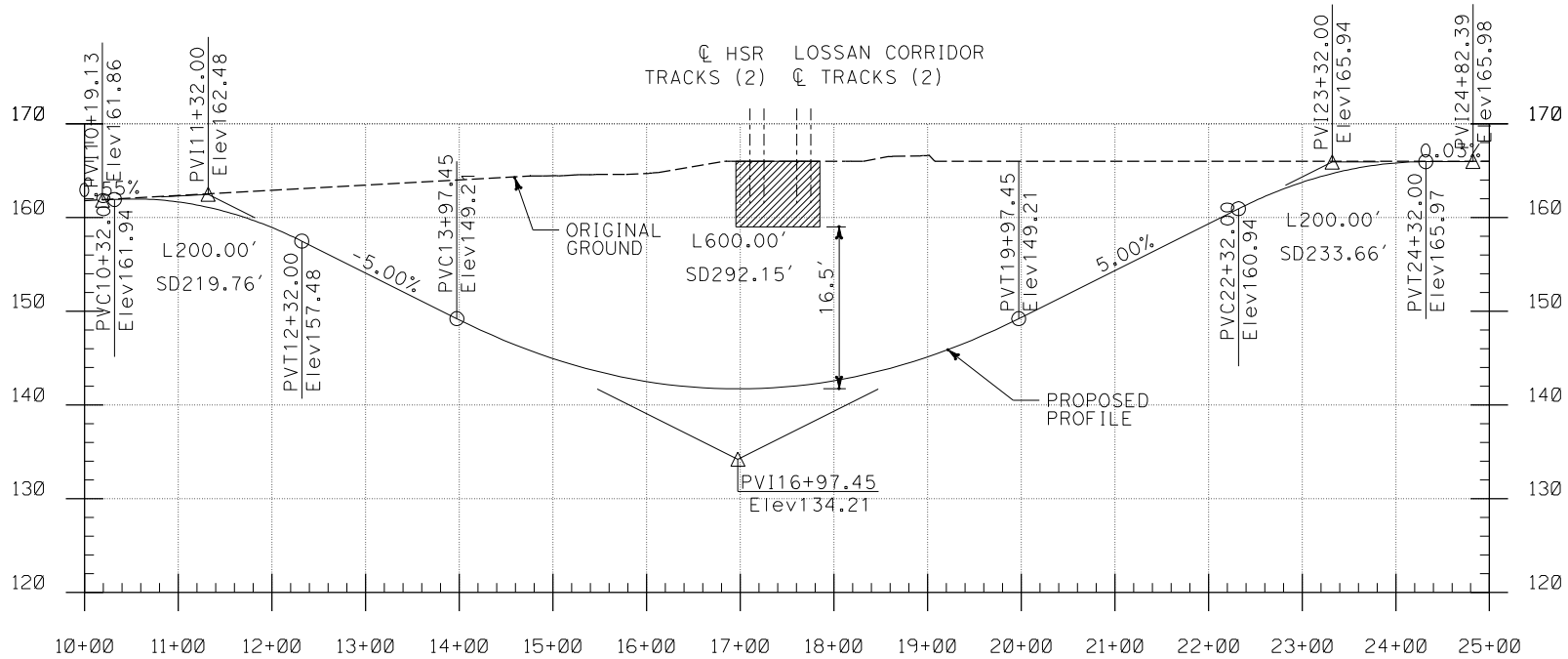
NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY



DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



**EXISTING BROADWAY - EAST AND WEST OF CROSSING
(LOOKING EAST)**



**PROPOSED BROADWAY - AT CROSSING
(LOOKING EAST)**

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

I:\projects\4013179\4013179_000\Drawings\civ\Grade Separation\OLAC-0302-Adgn
01.mxd \$\$\$\$\$\$SYTIME\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



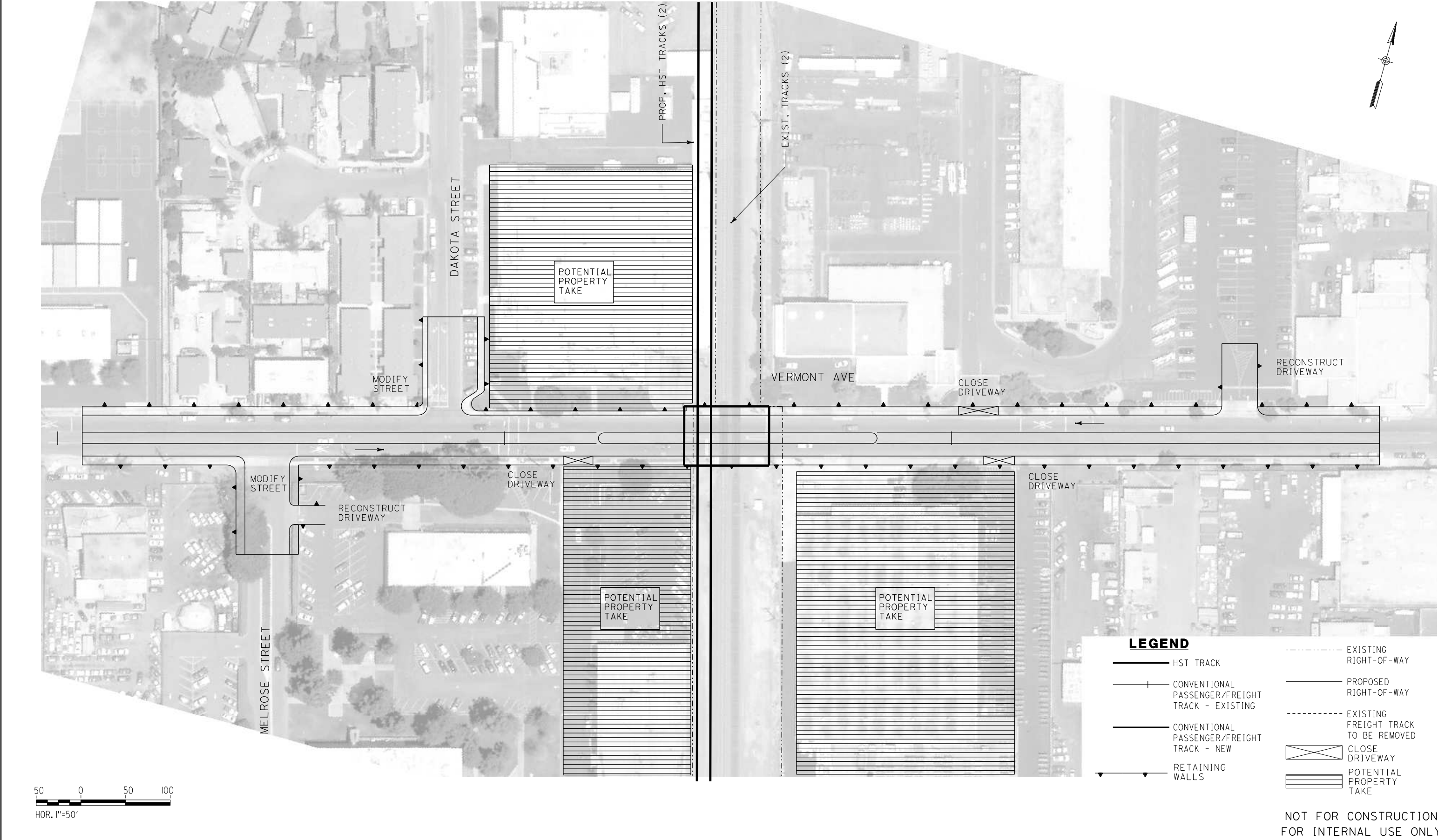
FLY CALIFORNIA
Without ever leaving the ground.

**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

BROADWAY STREET GRADE SEPARATION
PROFILE AND SECTIONS


CONTRACT NO. HSR06-0005
DRAWING NO. C0302
SCALE AS SHOWN
SHEET NO. 182 OF 245

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01.mxd



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

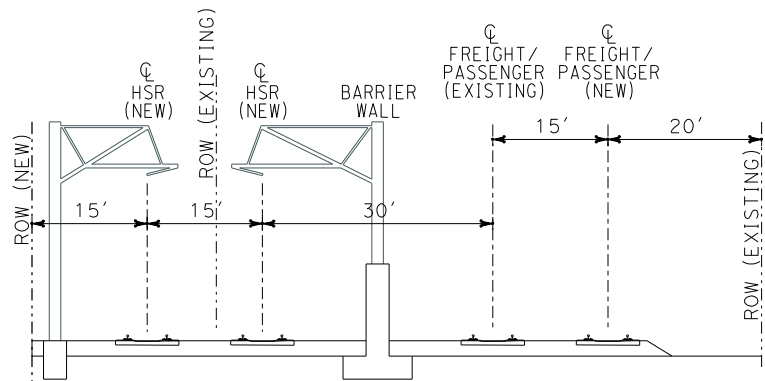


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

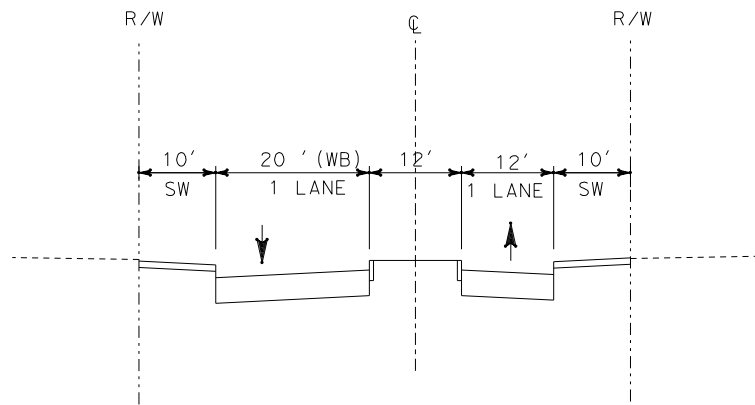
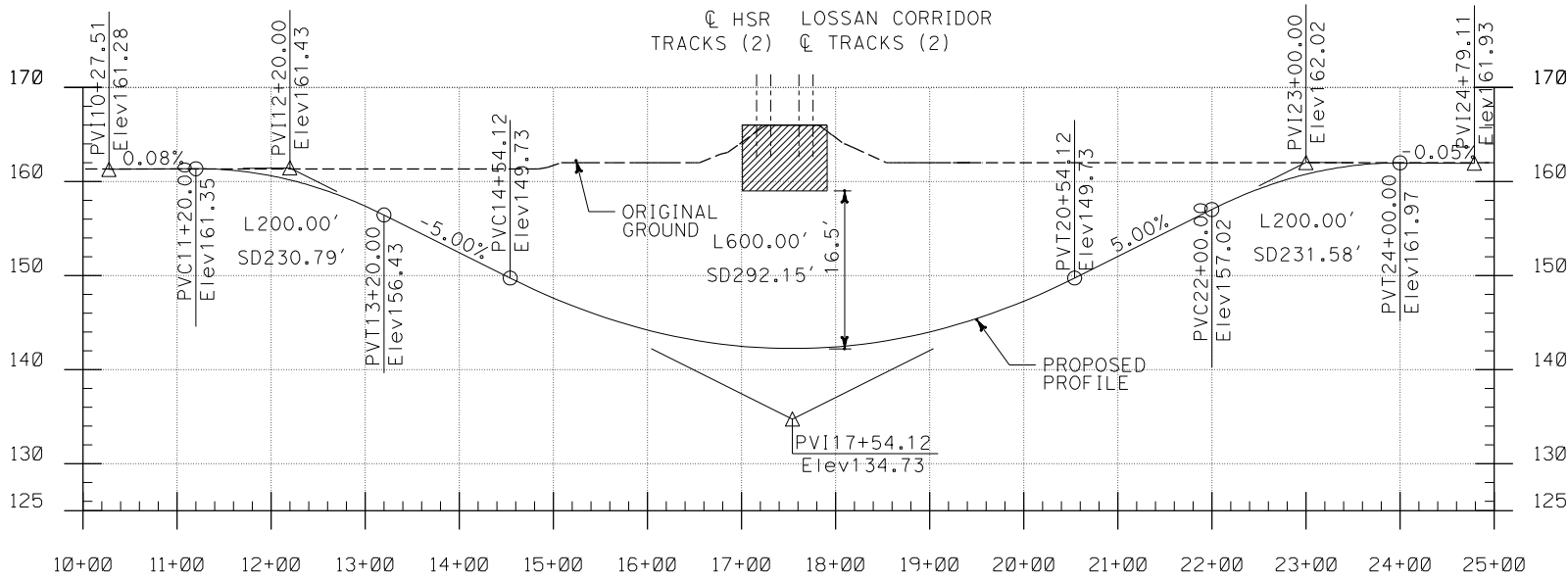
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

VERMONT AVE. GRADE SEPARATION
PLAN

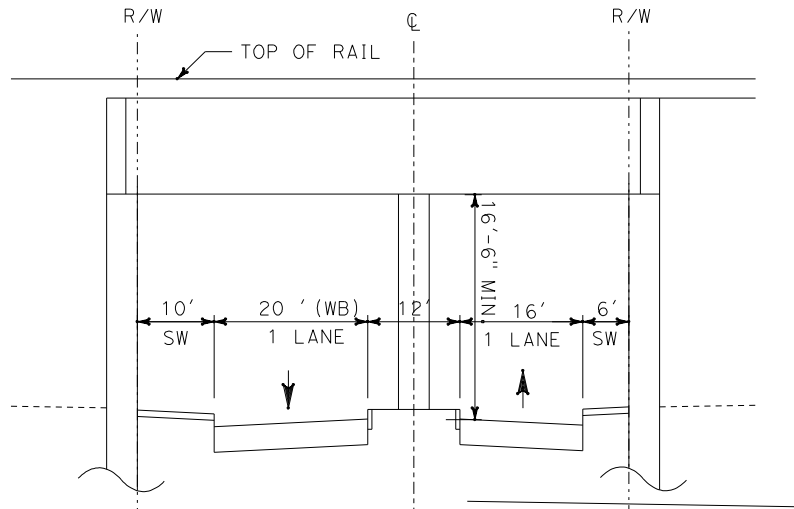
CONTRACT NO. HSR06-0005
DRAWING NO. C0413
SCALE AS SHOWN
SHEET NO. 183 OF 245



DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



EXISTING VERMONT - EAST AND WEST OF CROSSING
(LOOKING EAST)




PROPOSED VERMONT - AT CROSSING
(LOOKING EAST)

NOT FOR CONSTRUCTION
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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

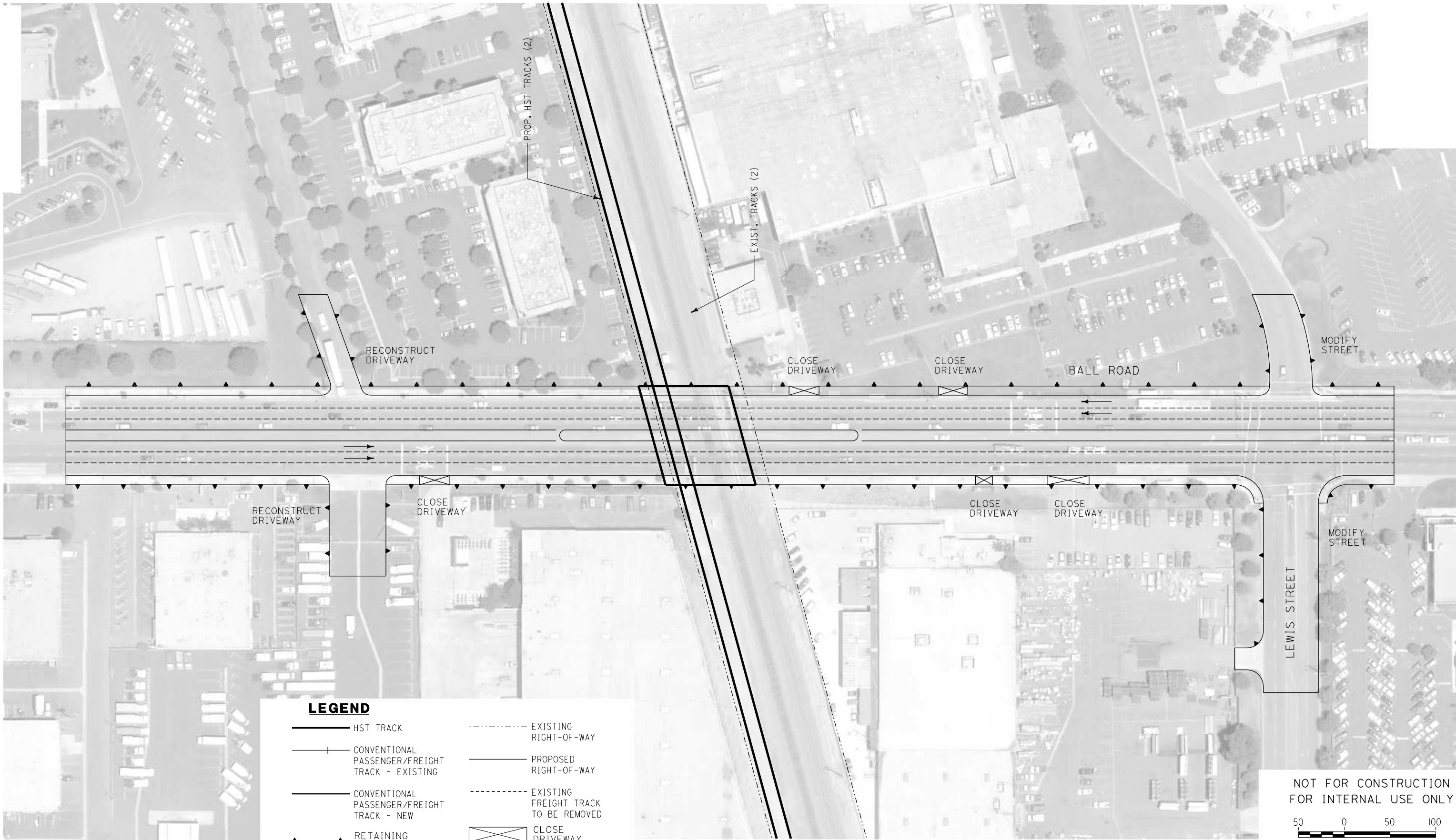
**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

VERMONT AVE. GRADE SEPARATION
PROFILE AND SECTIONS


CONTRACT NO. HSR06-0005
DRAWING NO. C0303
SCALE AS SHOWN
SHEET NO. 184 OF 245

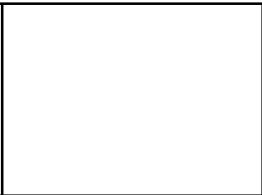
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REV	DATE	BY	SUB	APP	DESCRIPTION

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DATE 02-13-09

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ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

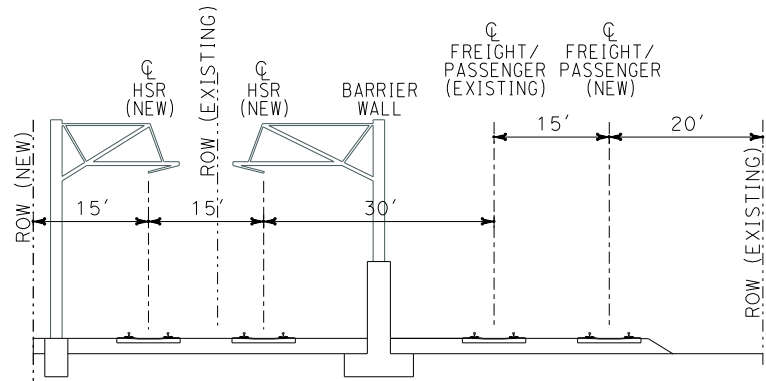


**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

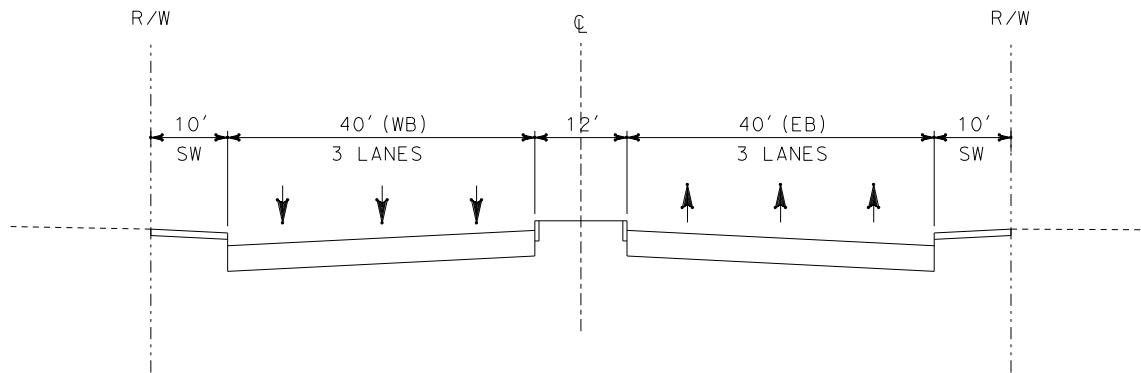
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

BALL ROAD GRADE SEPARATION
PLAN

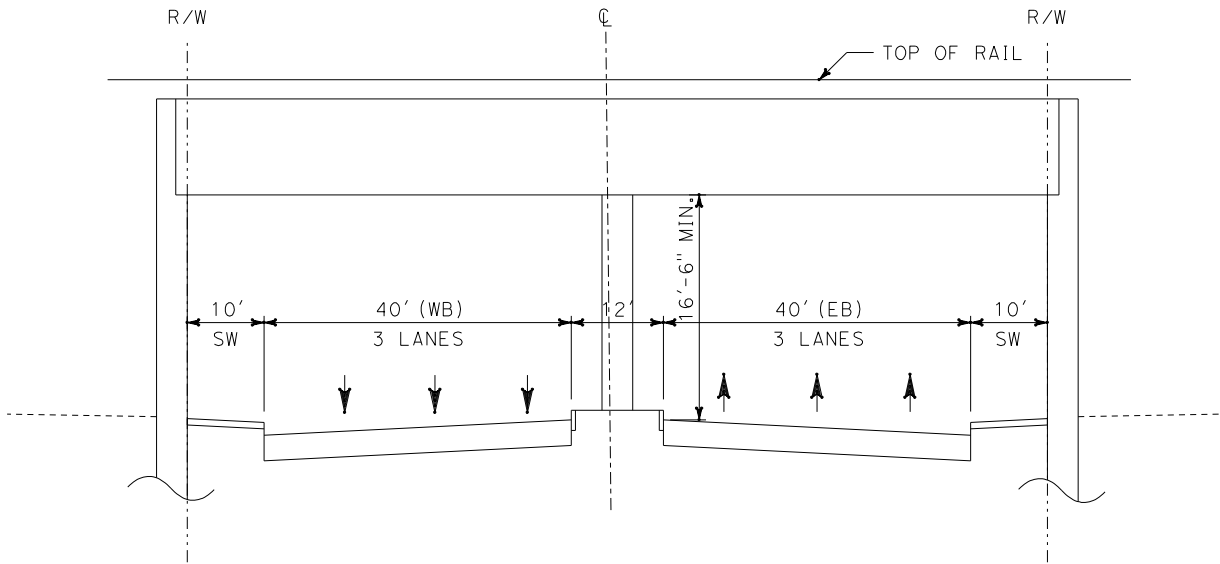
CONTRACT NO. HSR06-0005
DRAWING NO. C0414
SCALE AS SHOWN
SHEET NO. 185 OF 245



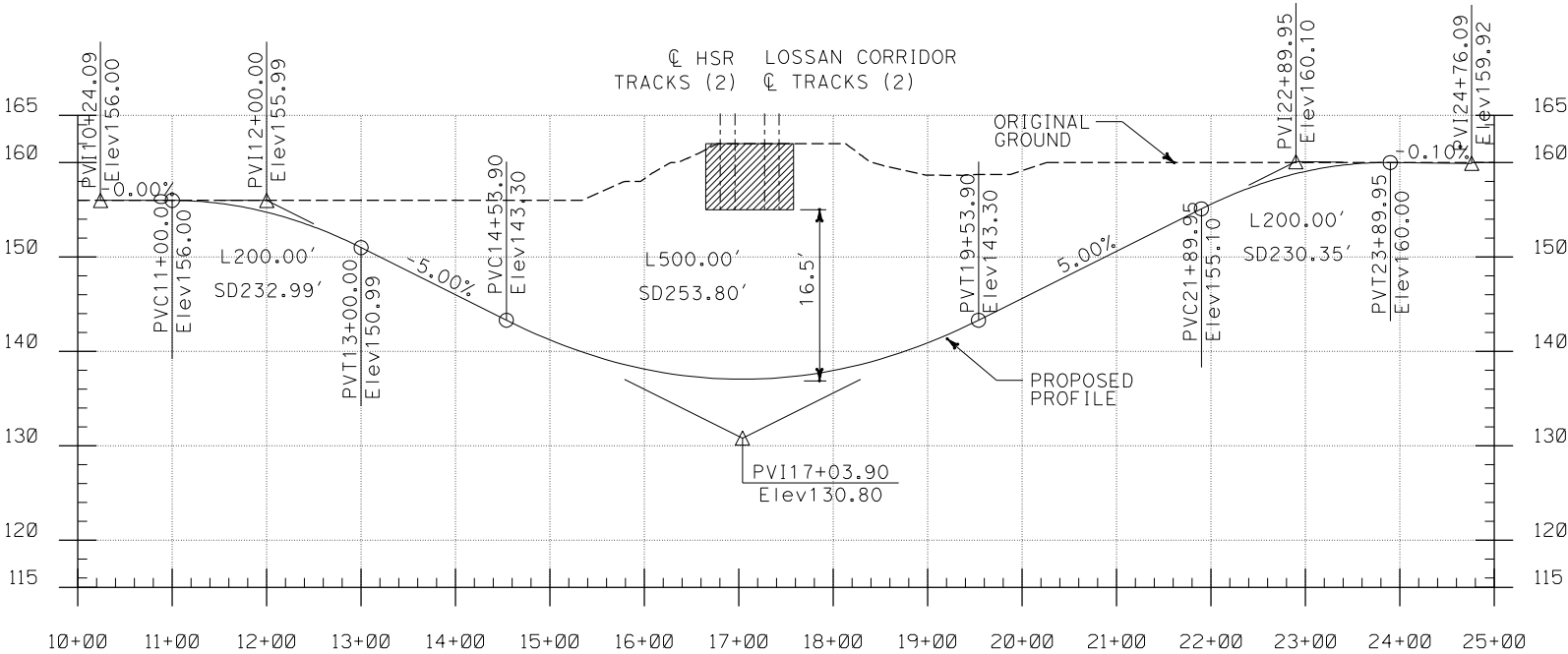
DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



**EXISTING BALL - EAST AND WEST OF CROSSING
(LOOKING EAST)**



**PROPOSED BALL - AT CROSSING
(LOOKING EAST)**



NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

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01.mxd

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

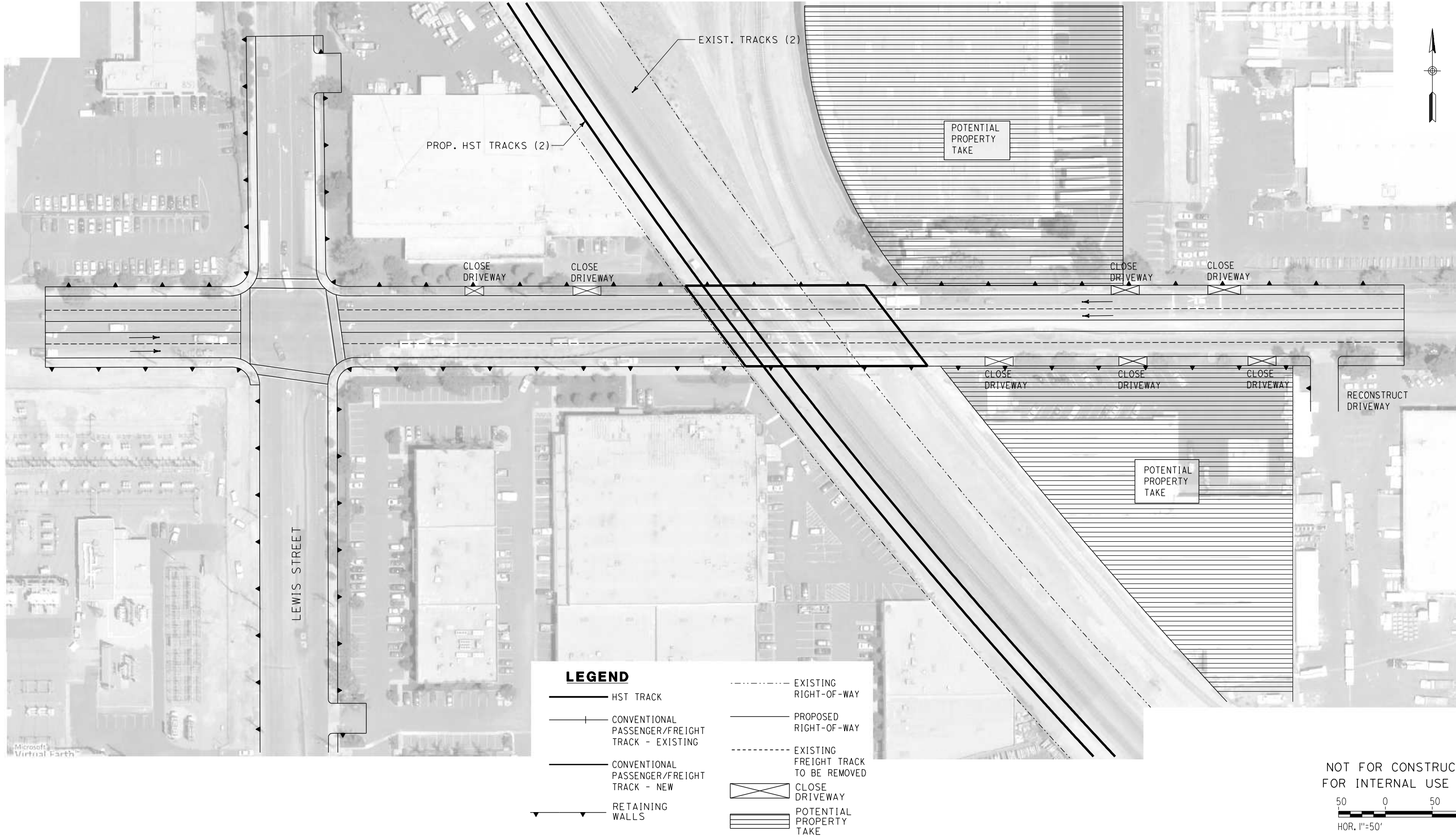


**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

BALL ROAD GRADE SEPARATION
PROILE AND SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. C0304
SCALE AS SHOWN
SHEET NO. 186 OF 245

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


NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

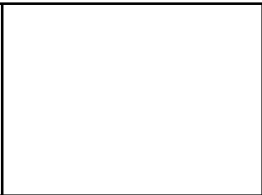
50 0 50 100
HOR. 1"=50'

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



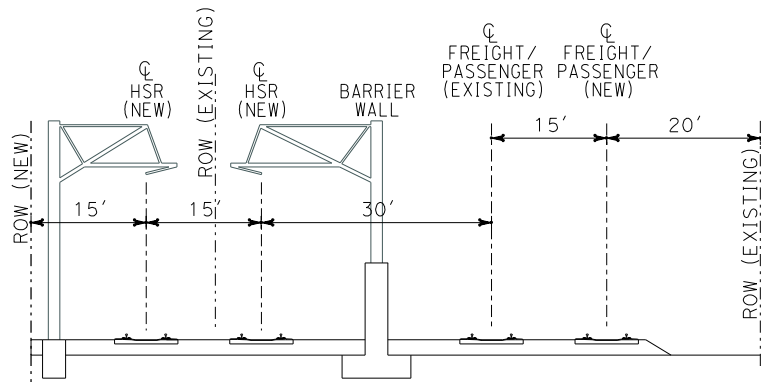


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

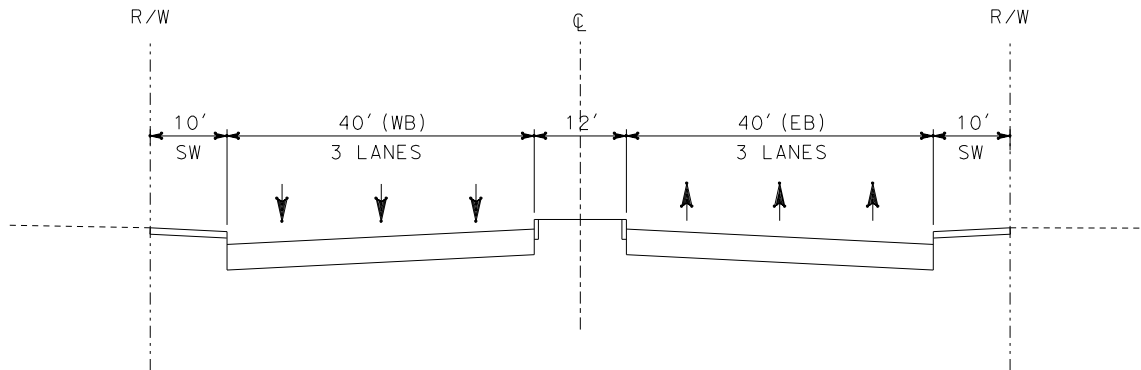
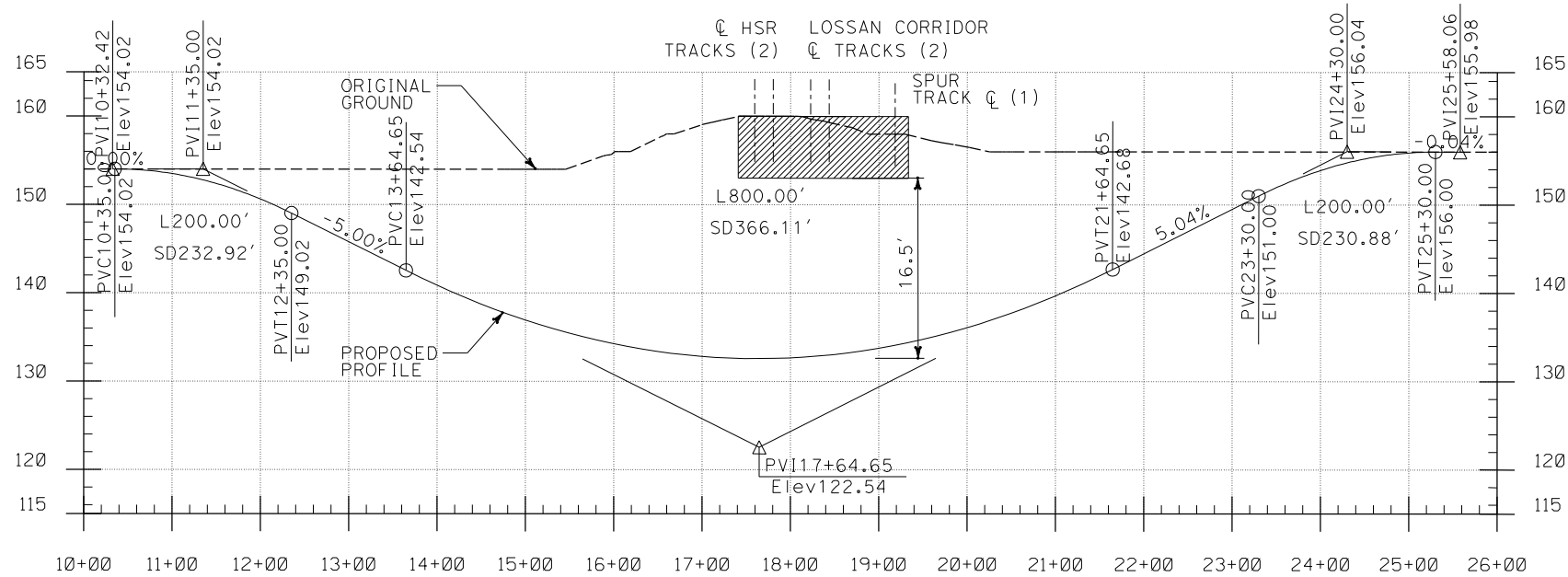
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES

CERRITOS AVE. GRADE SEPARATION
PLAN

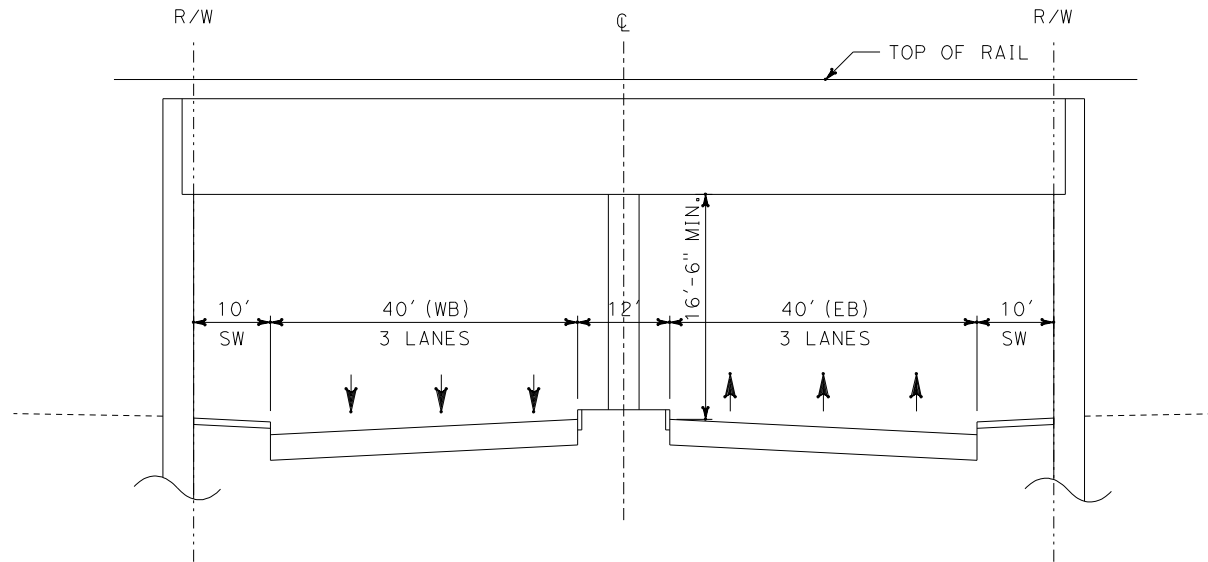
CONTRACT NO. HSR06-0005
DRAWING NO. C0415
SCALE AS SHOWN
SHEET NO. 187 OF 245



DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



EXISTING BALL - EAST AND WEST OF CROSSING
(LOOKING EAST)




PROPOSED BALL - AT CROSSING
(LOOKING EAST)

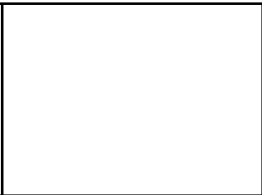
NOT FOR CONSTRUCTION
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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



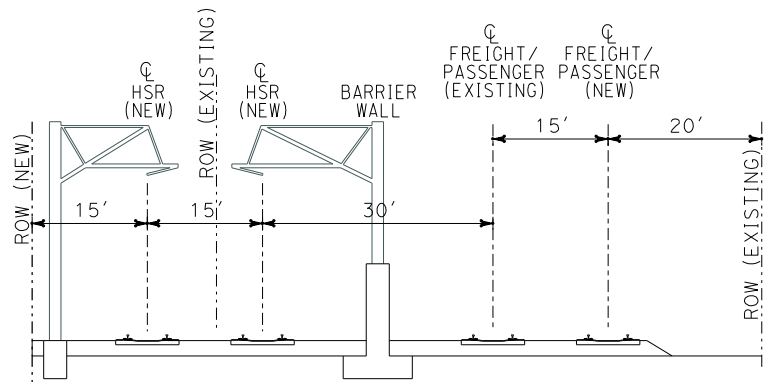
**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

**CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES**

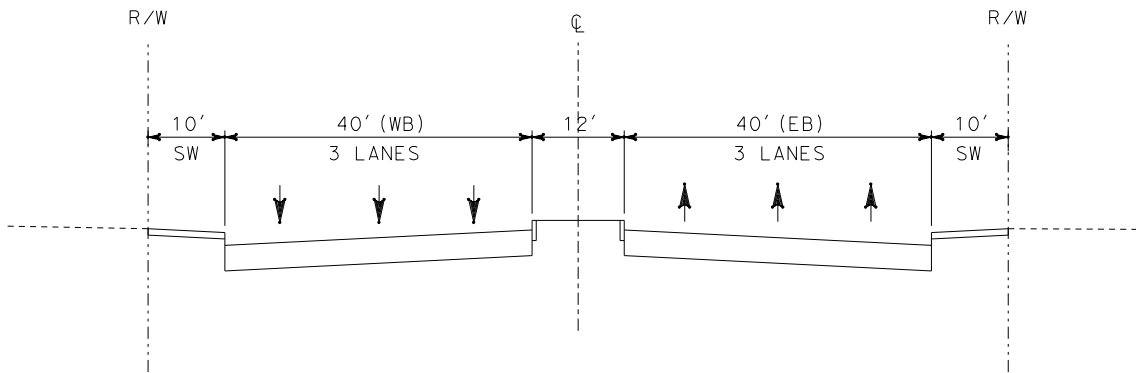
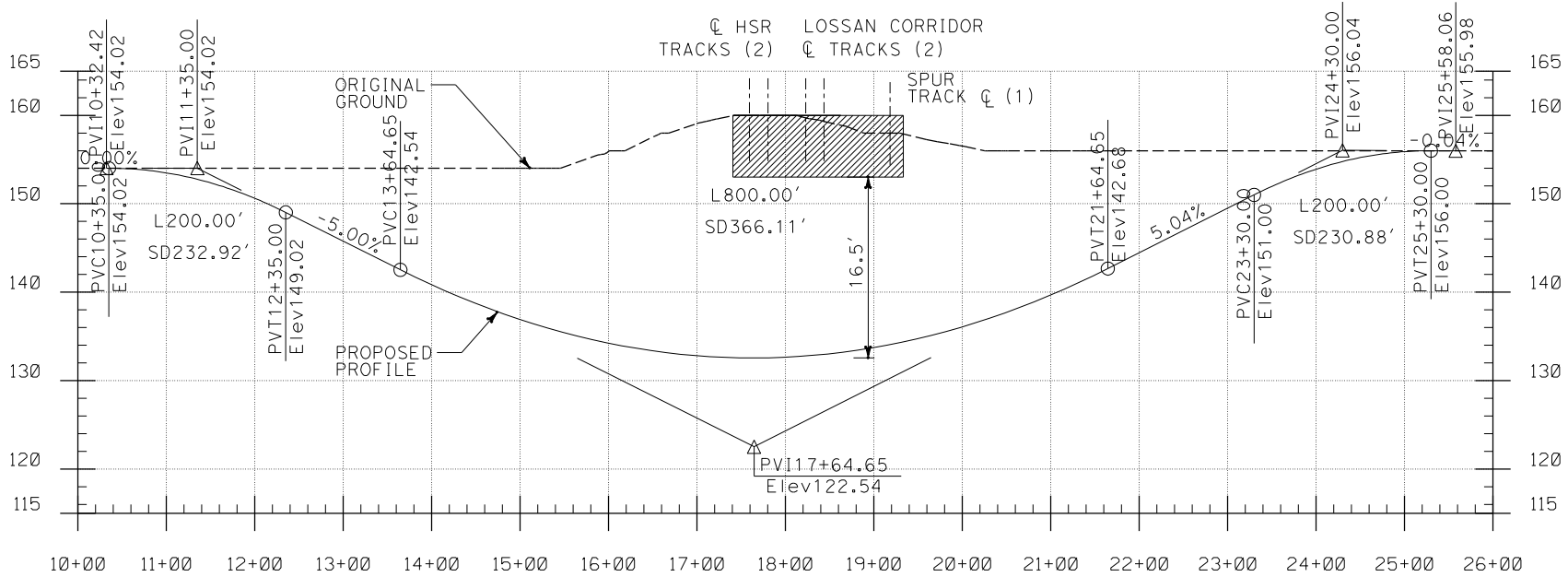
CERRITOS AVE. GRADE SEPARATION
PROFILE AND SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. C0305
SCALE AS SHOWN
SHEET NO. 188 OF 245

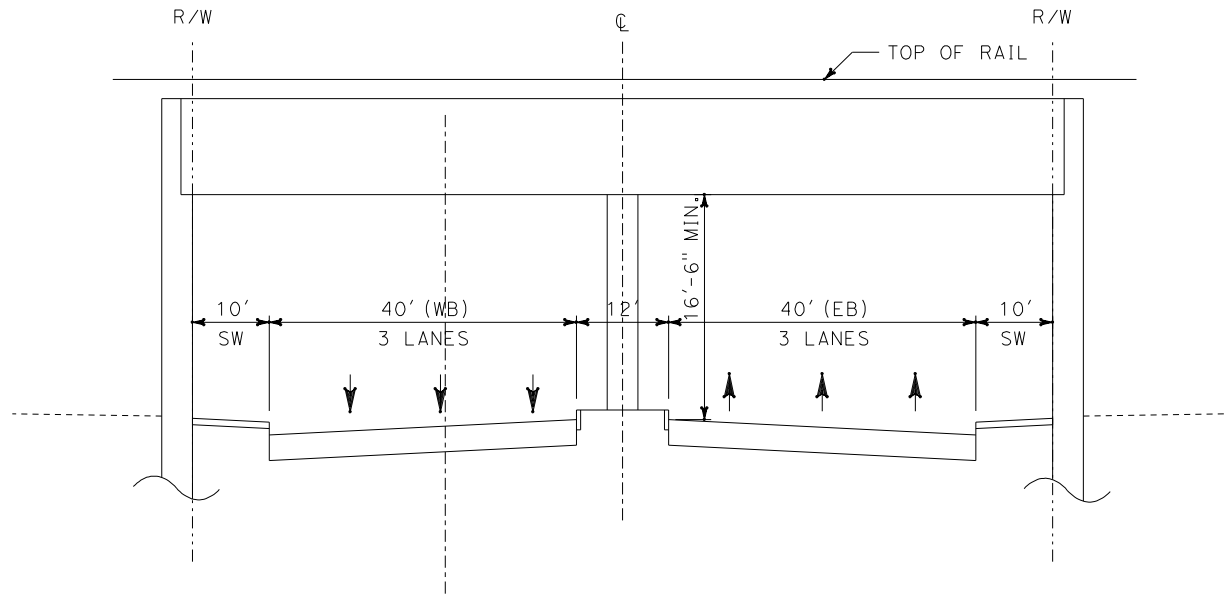
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DEDICATED HST (4+2) MIXED-GRADE ALTERNATIVE



EXISTING STATE COLLEGE - EAST AND WEST OF CROSSING
(LOOKING EAST)



PROPOSED STATE COLLEGE - AT CROSSING
(LOOKING EAST)

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY L. HOROWITZ
DRAWN BY Z. OHN
CHECKED BY M. SATISH
IN CHARGE D. THOMSON
DATE 02-13-09

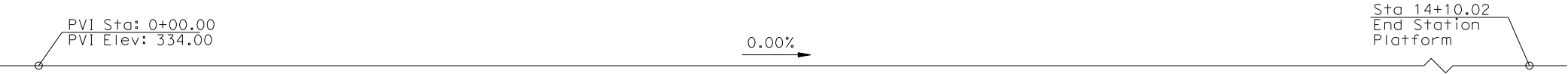


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
ALIGNMENT / OPTION

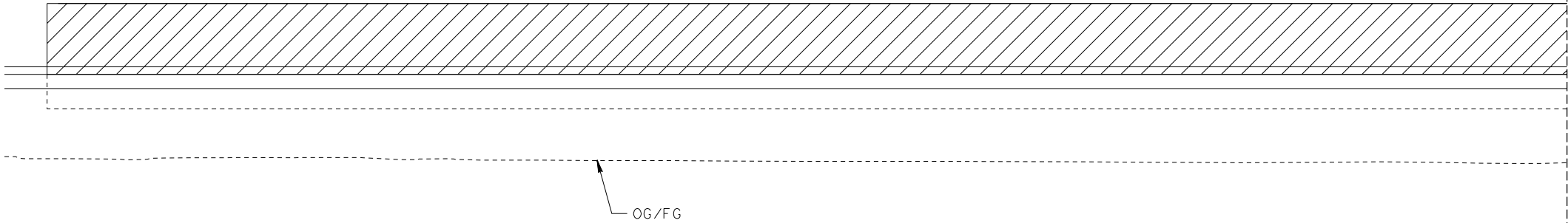
STATE COLLEGE GRADE SEPARATION
PROFILE AND SECTIONS

CONTRACT NO. HSR06-0005
DRAWING NO. C0306
SCALE AS SHOWN
SHEET NO. 190 OF 245

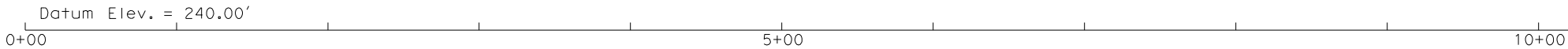
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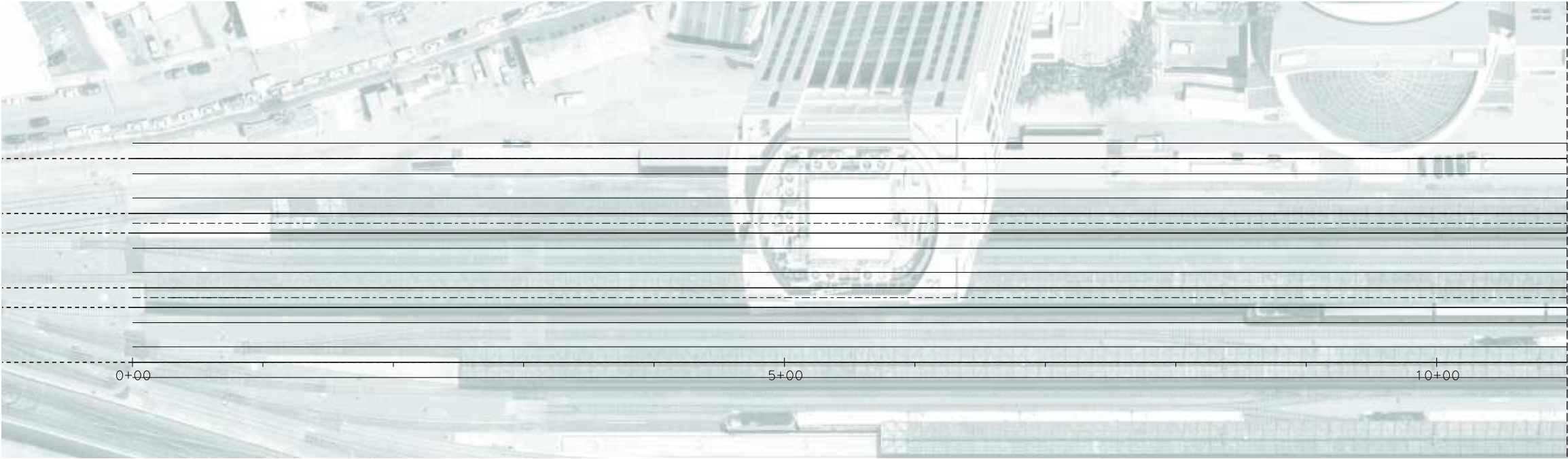
PROFILE GRADE
No Scale



Match Line Sta 11+00
Drawing S0201



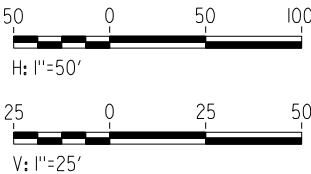
ELEVATION
Vert: 1"=25'
Horz: 1"=50'



Match Line Sta 11+00
Drawing S0201

To Anaheim
→


NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY



PLAN
1"=50'

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

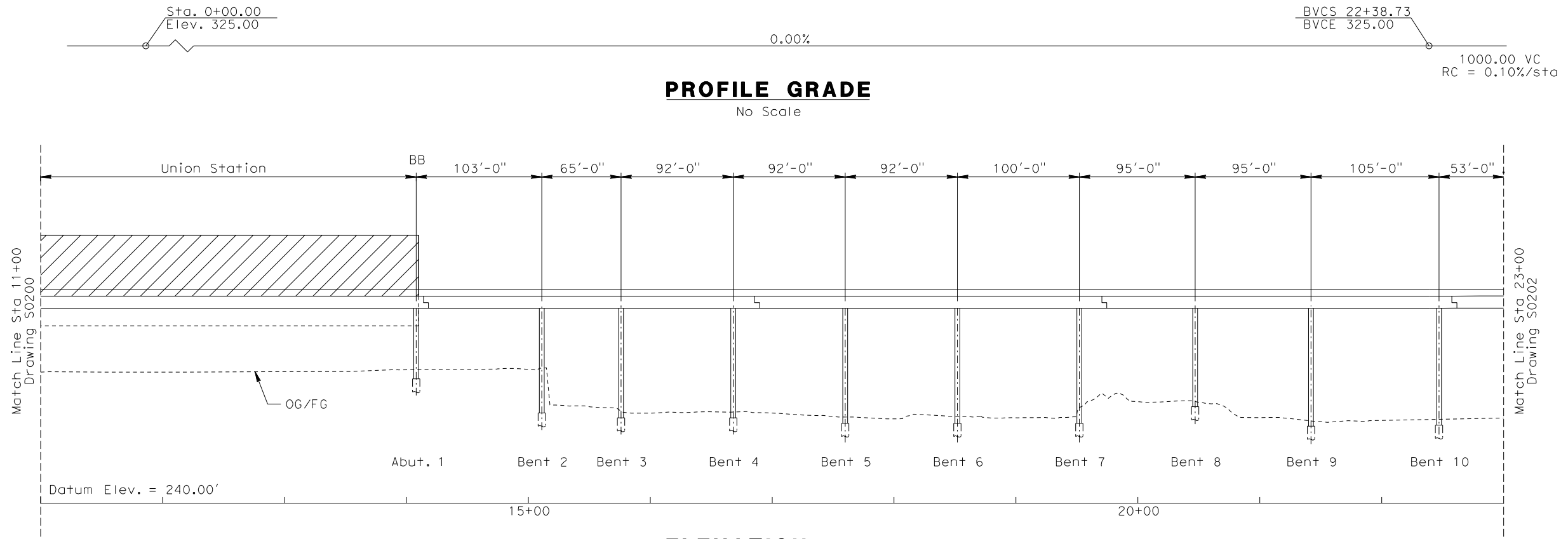




CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

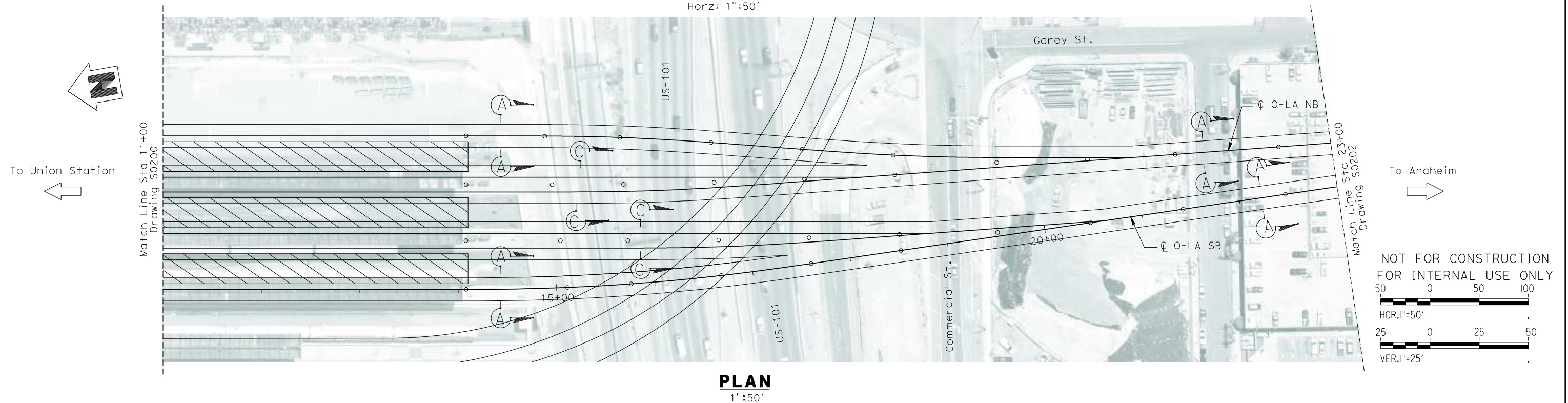
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #1

CONTRACT NO. HSR06-0005
DRAWING NO. S0200
SCALE AS SHOWN
SHEET NO. 191 OF 245



ELEVATION

Vert: 1"=25'
Horz: 1"=50'

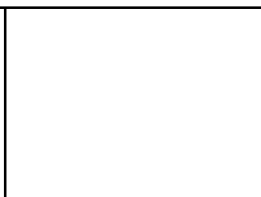


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\$\$\$\$\$SYTIME\$\$\$\$\$
Dr:query

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

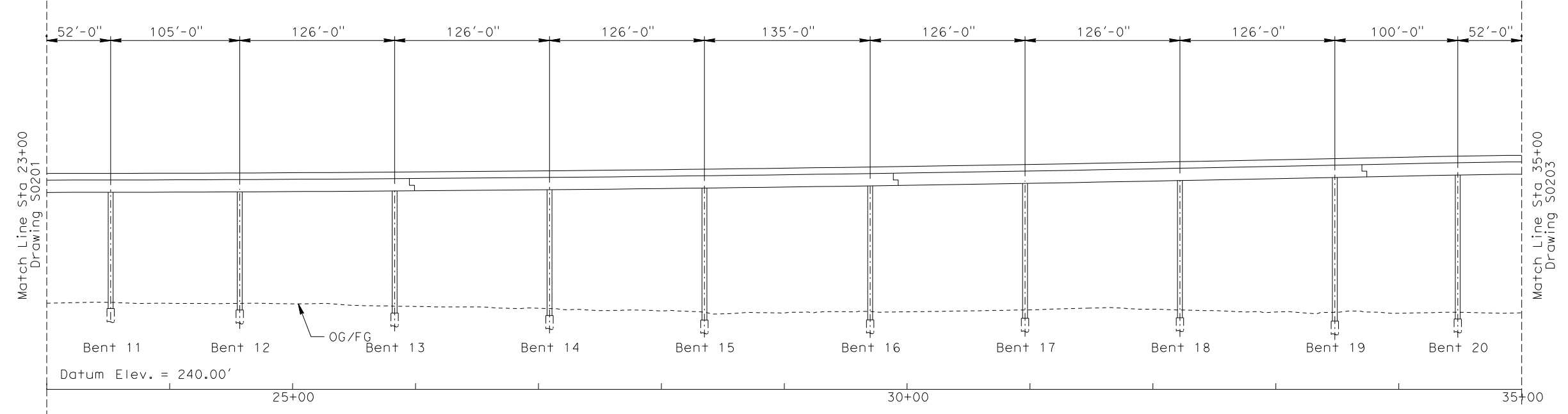
VIADUCT #1

CONTRACT NO. HSR06-0005
DRAWING NO. S0201
SCALE AS SHOWN
SHEET NO. 192 OF 245



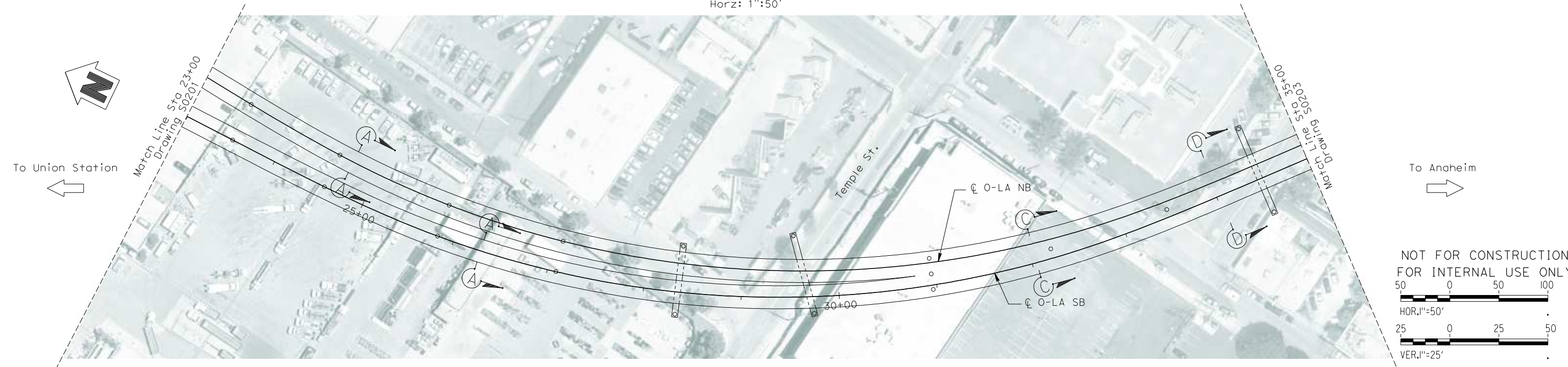
PROFILE GRADE

No Scale



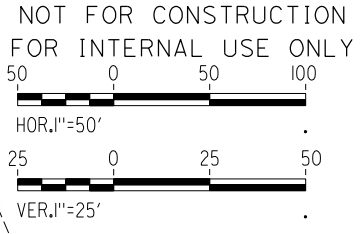
ELEVATION

Vert: 1"=25'
Horz: 1"=50'



PLAN


1"=50'



R:\Proj\4013179\4013179_000\Drawings\STR\OLA-S-0202.dgn
\$\$\$\$\$SYTIME\$\$\$\$\$
Branquerry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
B. QUERY
DRAWN BY
L. HUFF.
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



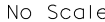


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

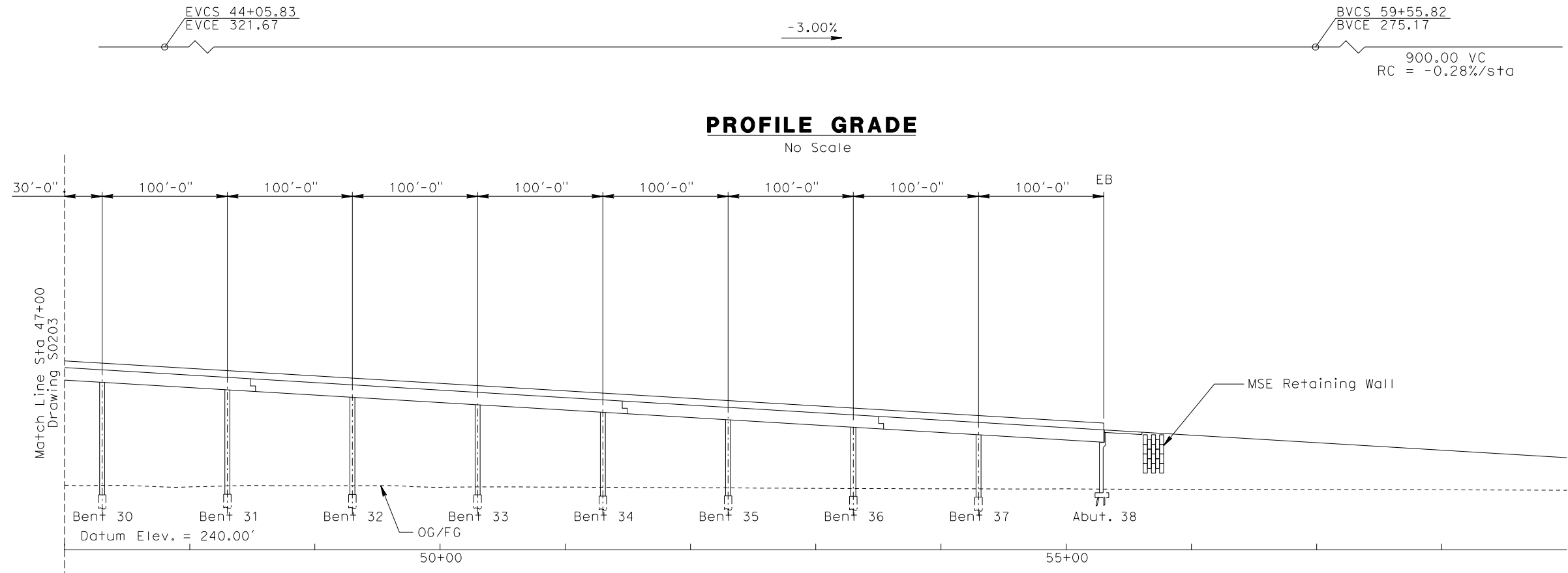
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #1

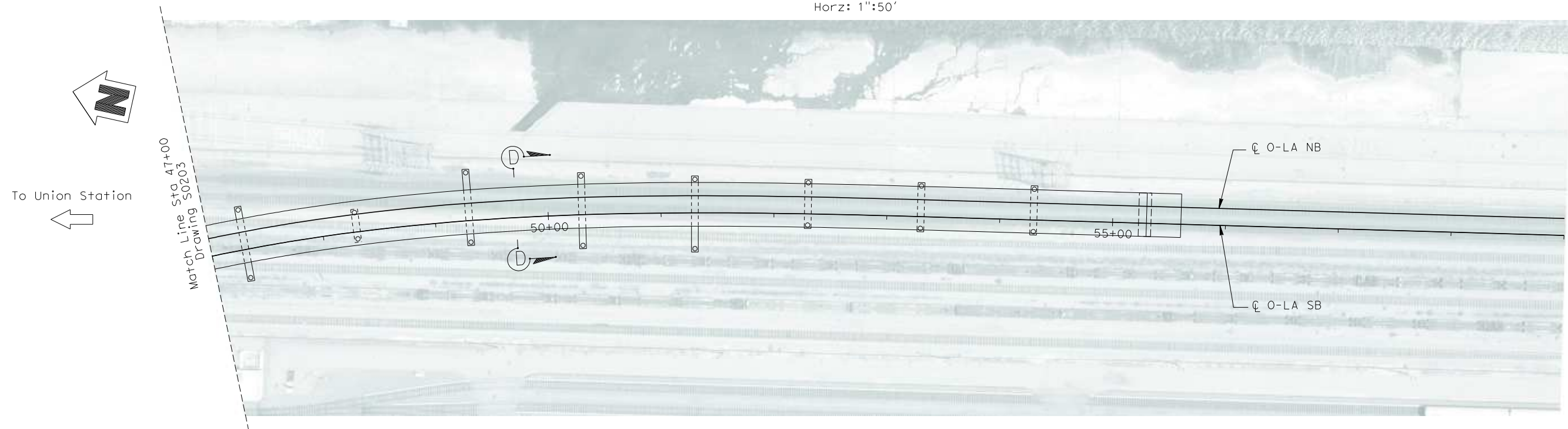
CONTRACT NO. HSR06-0005
DRAWING NO. S0202
SCALE AS SHOWN
SHEET NO. 193 OF 245



CONTRACT NO.	HSR06-0005
DRAWING NO.	S0203
SCALE	AS SHOWN
SHEET NO.	194 OF 245



ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

50 0 50 100

HOR. 1"=50'


25 0 25 50

VER. 1"=25'

R:\Proj\4013179\4013179_000\Drawings\STRA\LA-S-0204.dgn
\$\$\$\$\$SYTIME\$\$\$\$\$
Branquery

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

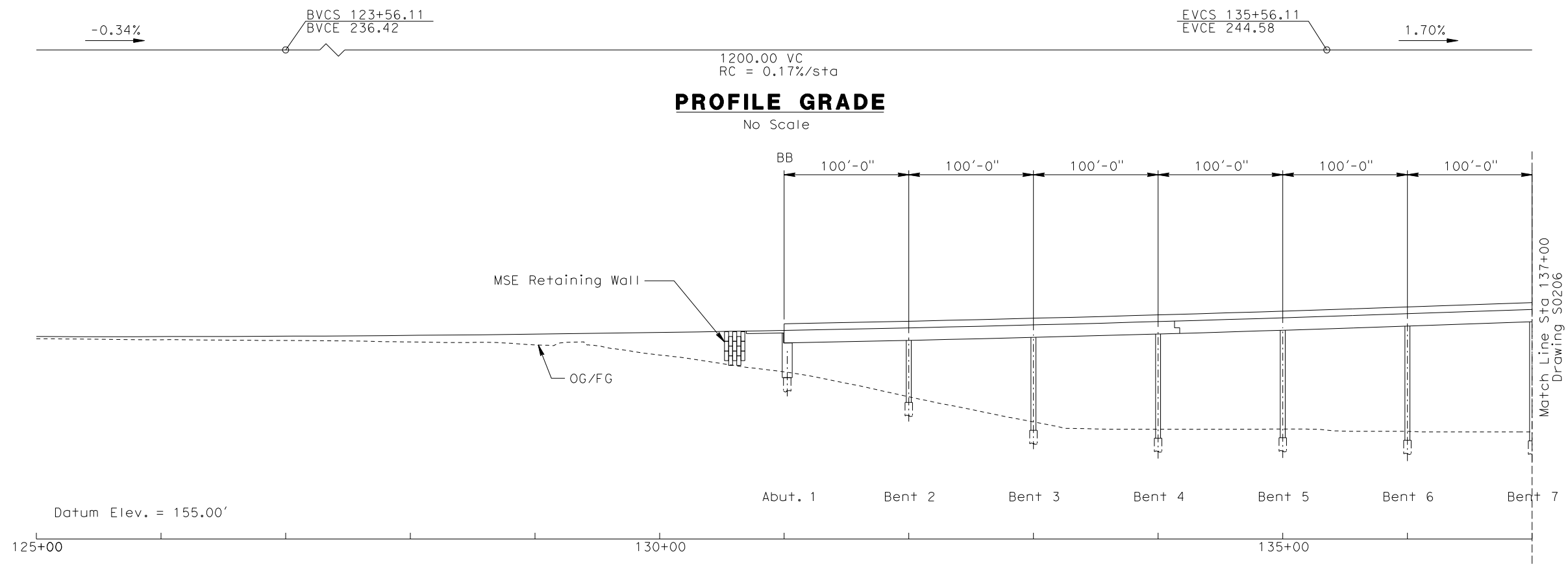




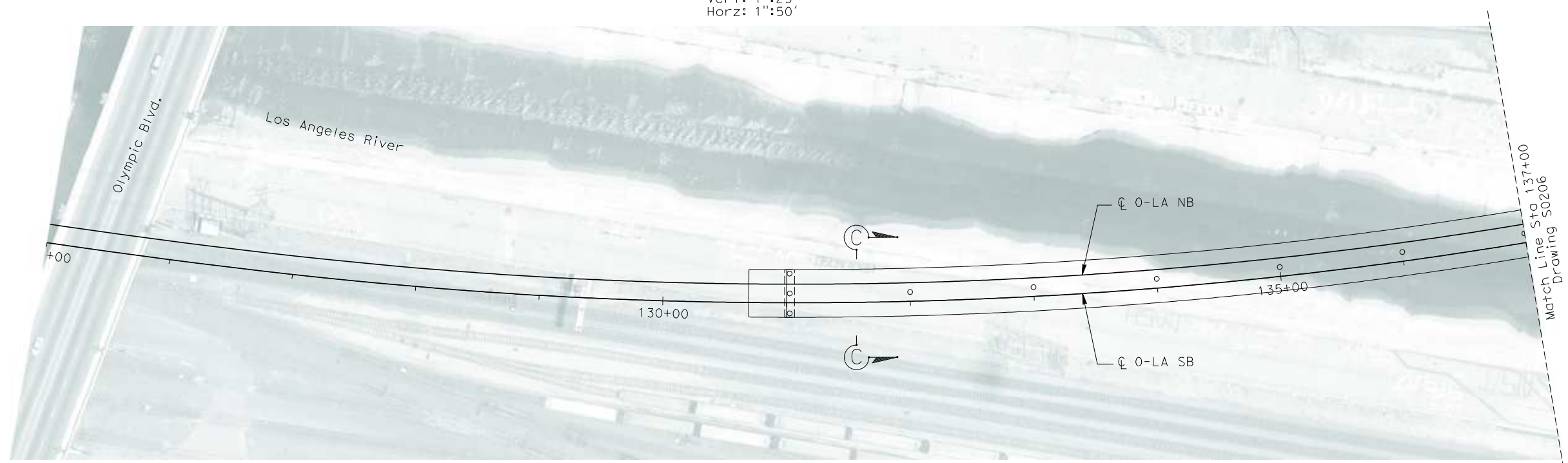
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #1

CONTRACT NO. HSR06-0005
DRAWING NO. S0204
SCALE AS SHOWN
SHEET NO. 195 OF 245



ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

50 0 50 100
HOR. 1"=50'

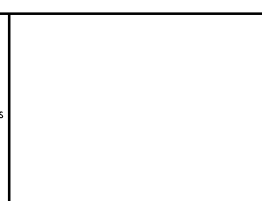
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VER. 1"=25'

R:\Proj\4013179\4013179_000\Drawings\STRA\LA-S-0205.dgn
James.Lones

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
B. QUERY
DRAWN BY
L. HUFF.
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09

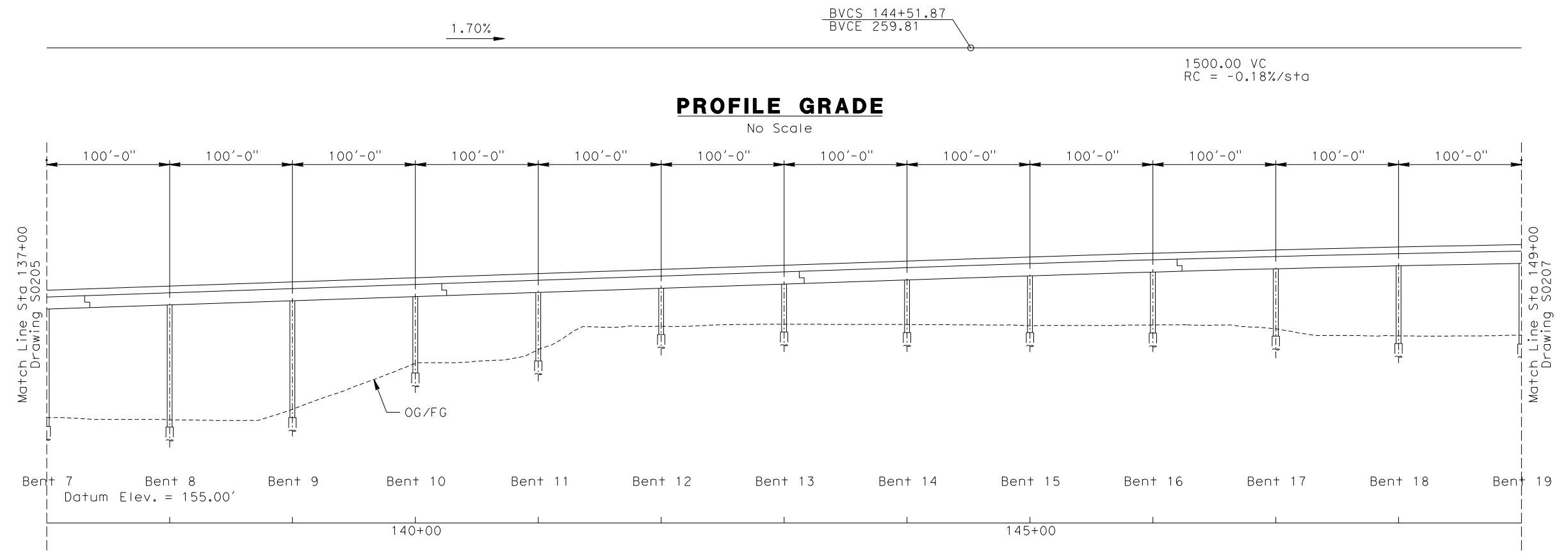
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



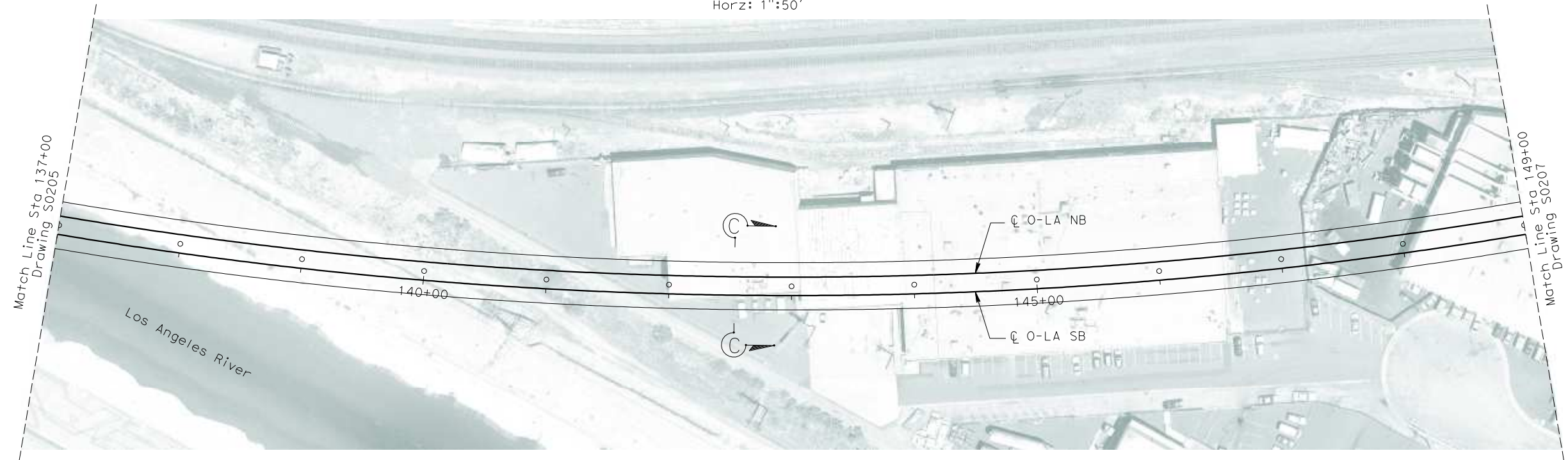
**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0205
SCALE AS SHOWN
SHEET NO. 196 OF 245



ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY

50 0 50 100
HOR. 1"=50'

25 0 25 50
VER. 1"=25'

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James.Lines

REV	DATE	BY	SUB	APP	DESCRIPTION


DESIGNED BY
B. QUERY

DRAWN BY
L. HUFF.

CHECKED BY
A. LIU

IN CHARGE
D. THOMSON

DATE
02-13-09

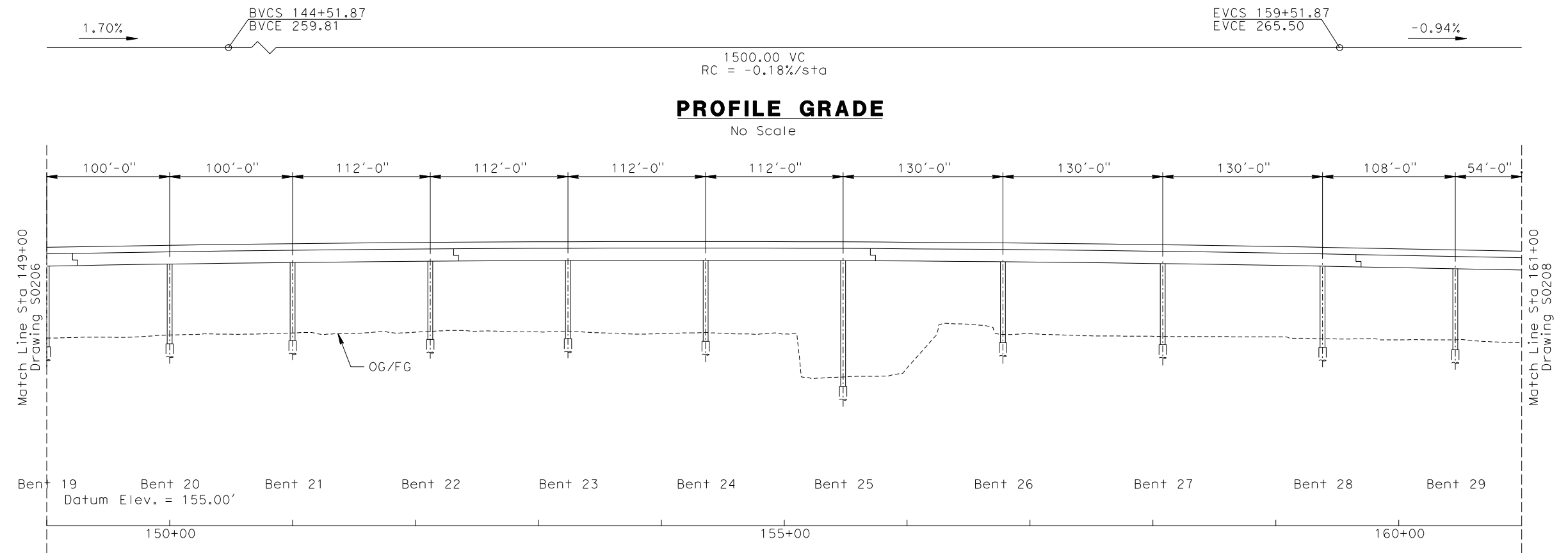
 **STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

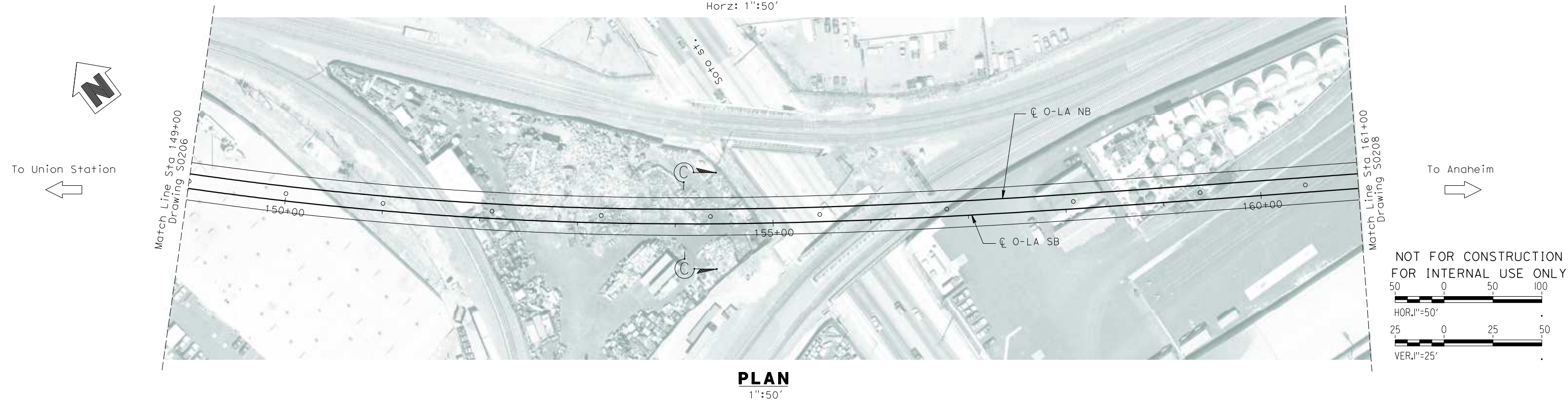
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0206
SCALE AS SHOWN
SHEET NO. 197 OF 245




ELEVATION
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Horz: 1"=50'



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James.Lines

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

 **STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

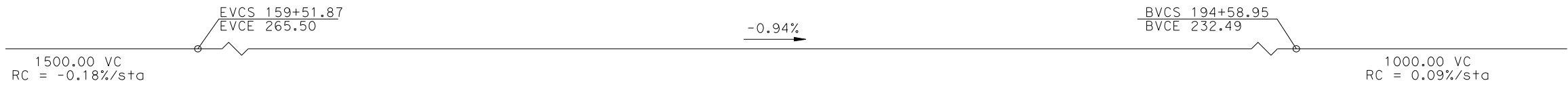


 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0207
SCALE AS SHOWN
SHEET NO. 198 OF 245

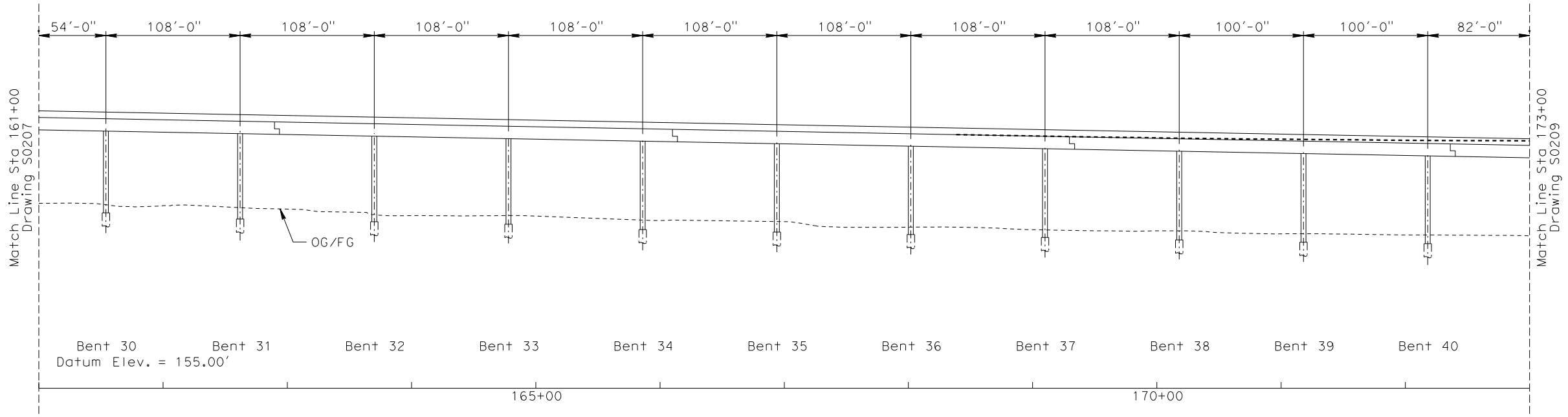
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B:\Query



NOTE : SB O-LA TRACK PROFILE
SHOWN.
NB O-LA TRACK PROFILE
NOT SHOWN FOR CLARITY.

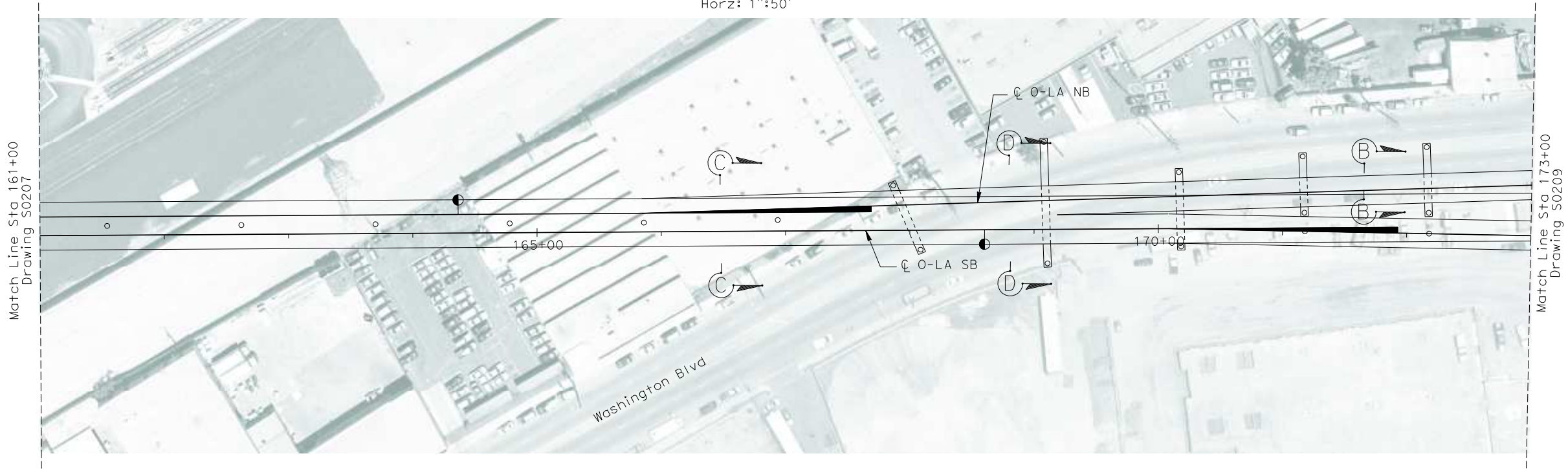
PROFILE GRADE

No Scale



ELEVATION

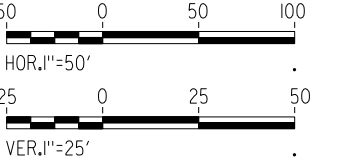
Vert: 1"=25'
Horz: 1"=50'



PLAN

1"=50'

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FOR INTERNAL USE ONLY



REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
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CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED RAIL AUTHORITY

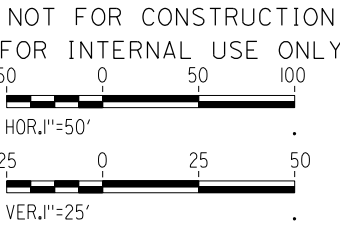
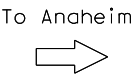
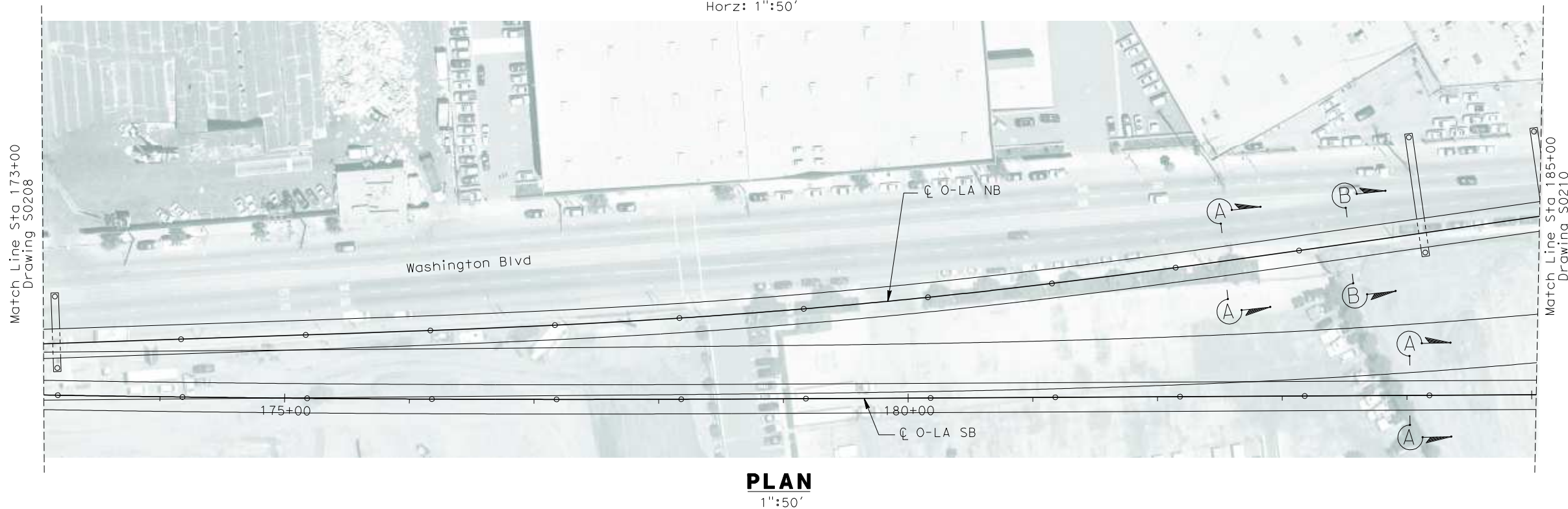
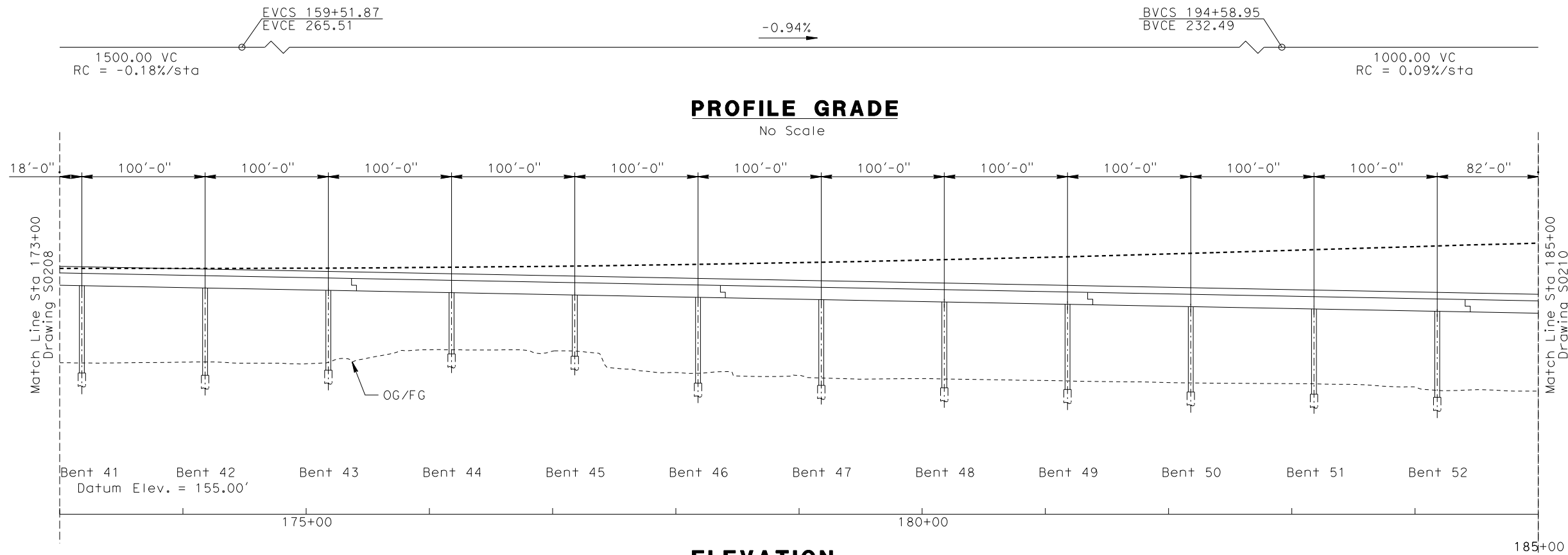
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0208
SCALE AS SHOWN
SHEET NO. 199 OF 245


NOTE : SB O-LA TRACK PROFILE SHOWN.
NB O-LA TRACK PROFILE NOT SHOWN FOR CLARITY.



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\$\$\$\$\$SYTIME\$\$\$\$\$
Branquerry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

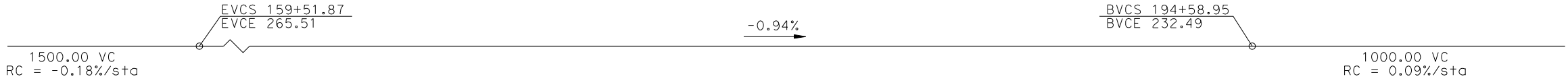
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0209
SCALE AS SHOWN
SHEET NO. 200 OF 245

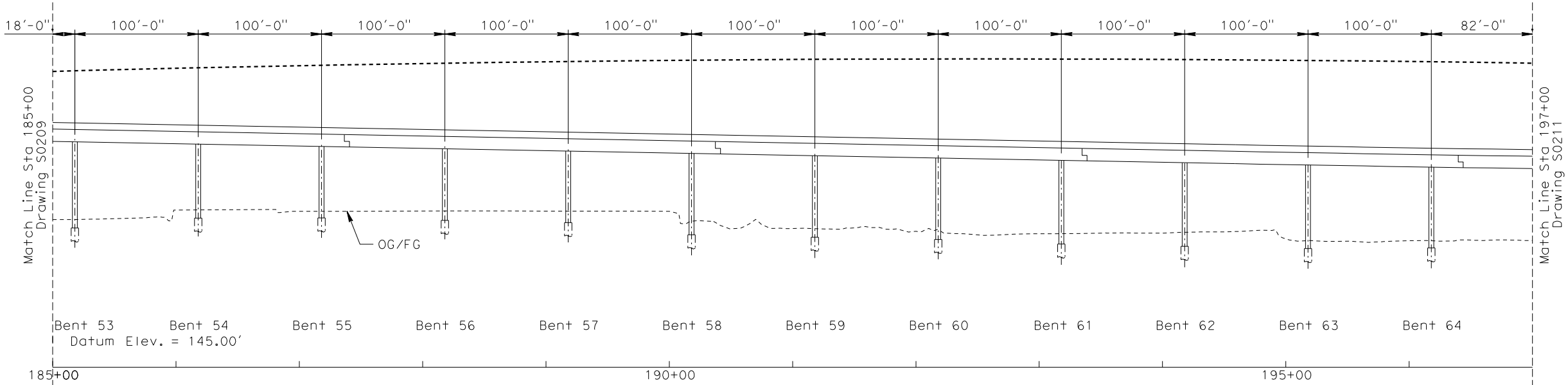
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\$\$\$\$\$SYTIME\$\$\$\$\$
Branquery

NOTE : SB O-LA TRACK PROFILE
SHOWN.
NB O-LA TRACK PROFILE
NOT SHOWN FOR CLARITY.



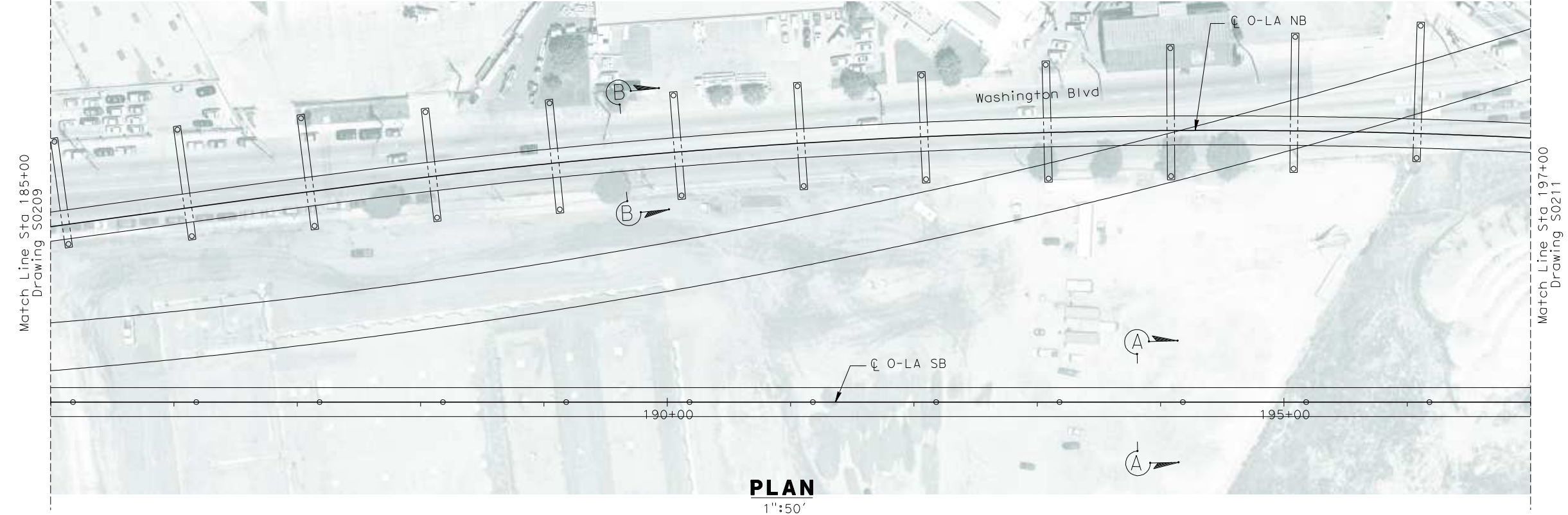
PROFILE GRADE

No Scale



ELEVATION

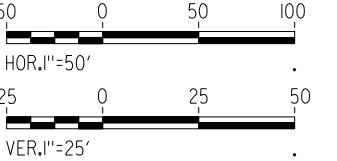
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Horz: 1"=50'



PLAN

1"=50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY



REV	DATE	BY	SUB	APP	DESCRIPTION

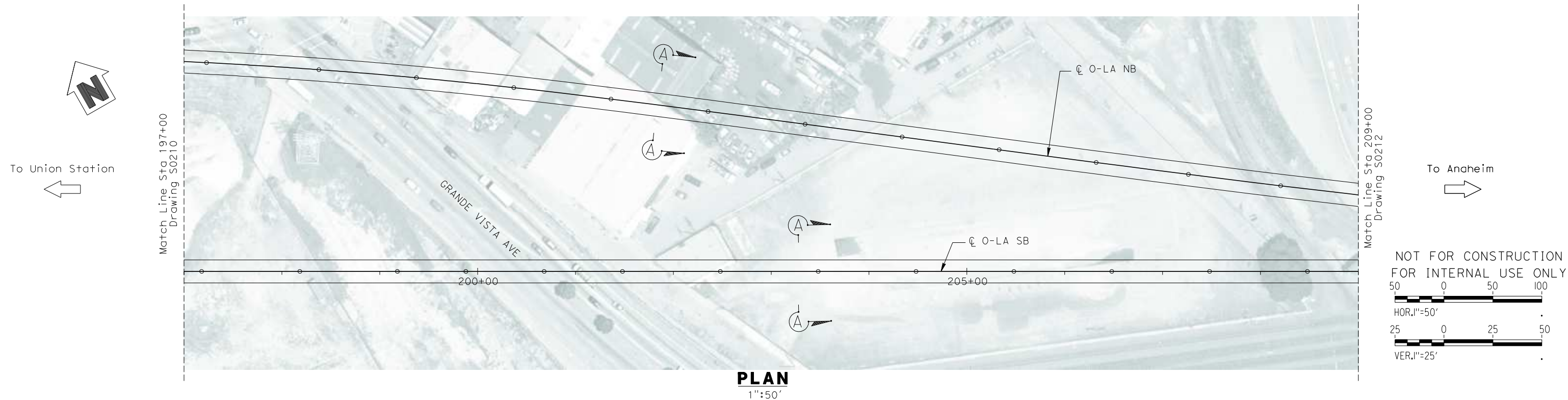
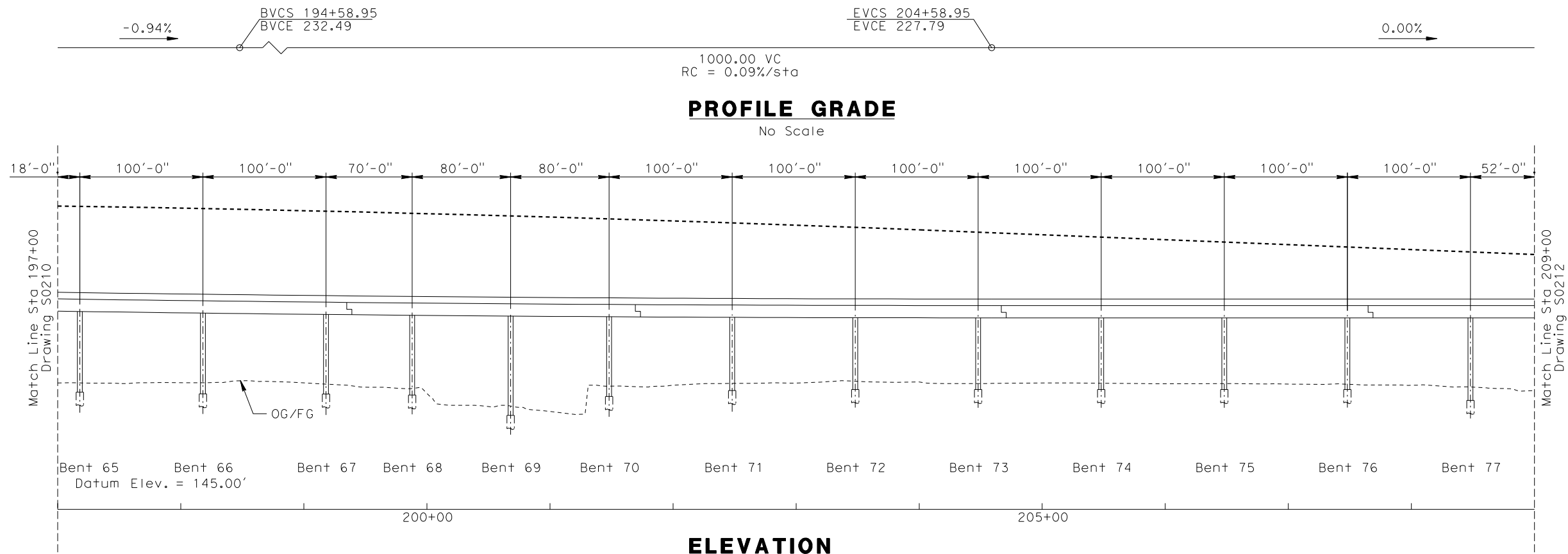
DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2


CONTRACT NO. HSR06-0005
DRAWING NO. S0210
SCALE AS SHOWN
SHEET NO. 201 OF 245



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\$\$\$\$\$SYTIME\$\$\$\$\$
Branquerry

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

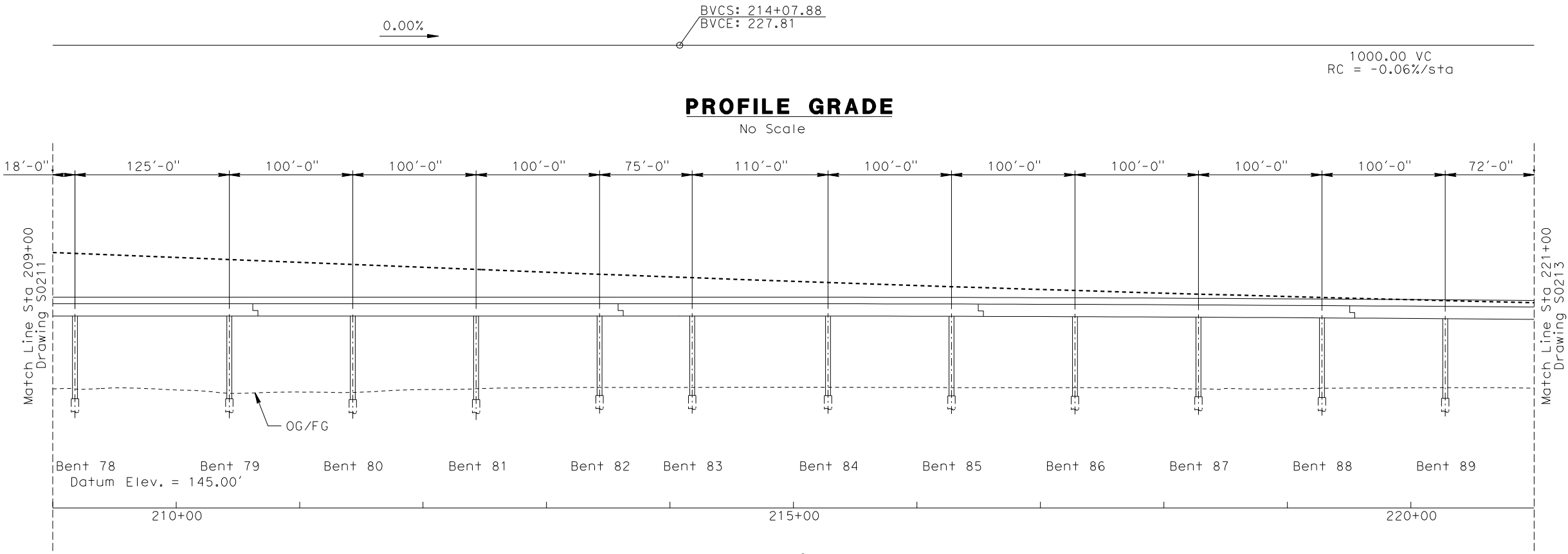
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0211
SCALE AS SHOWN
SHEET NO. 202 OF 245

NOTE : SB O-LA TRACK PROFILE SHOWN.
NB O-LA TRACK PROFILE NOT SHOWN FOR CLARITY.



ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY


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HOR. 1"=50'

25 0 25 50
VER. 1"=25'

R:\Proj\4013179\4013179_000\Drawings\STR\OLA-S-0212.dgn
\$\$\$\$\$SYTIME\$\$\$\$\$
Branquery

REV	DATE	BY	SUB	APP	DESCRIPTION

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DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



**FLY CALIFORNIA**
Without ever leaving the ground.

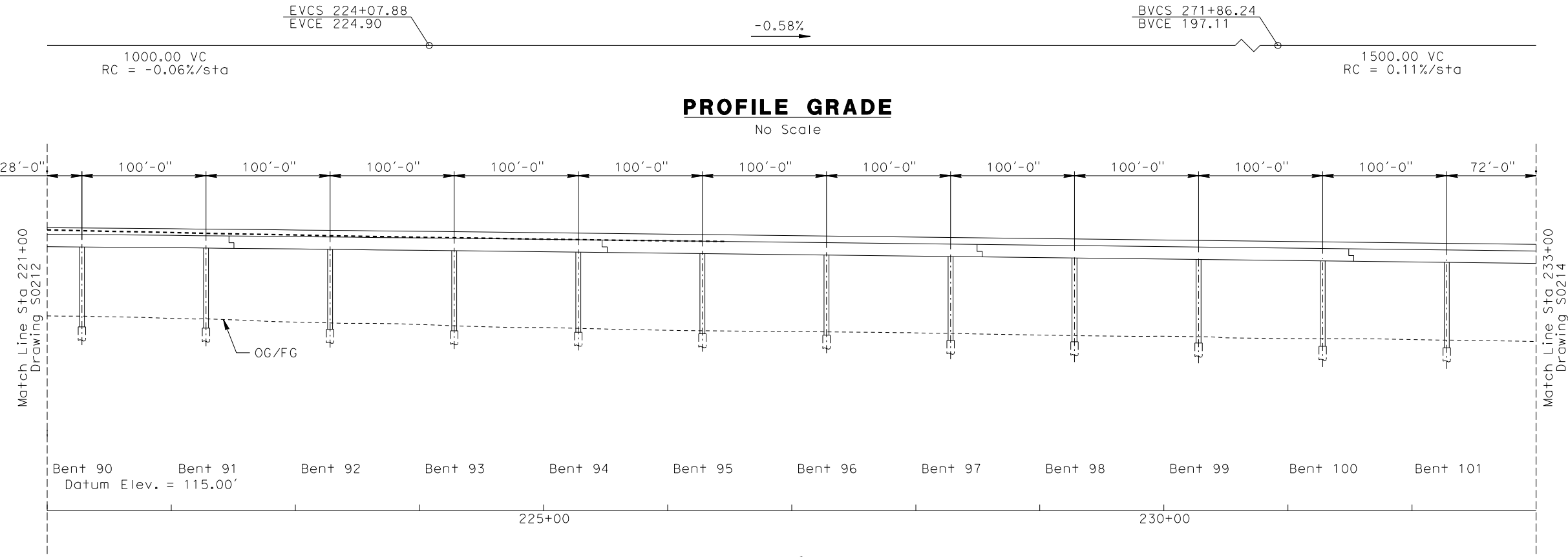
CALIFORNIA HIGH SPEED RAIL AUTHORITY

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

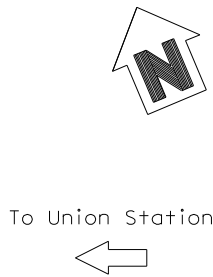
CONTRACT NO. HSR06-0005
DRAWING NO. S0212
SCALE AS SHOWN
SHEET NO. 203 OF 245

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James.Lines
\$\$\$\$\$SYTIME\$\$\$\$\$

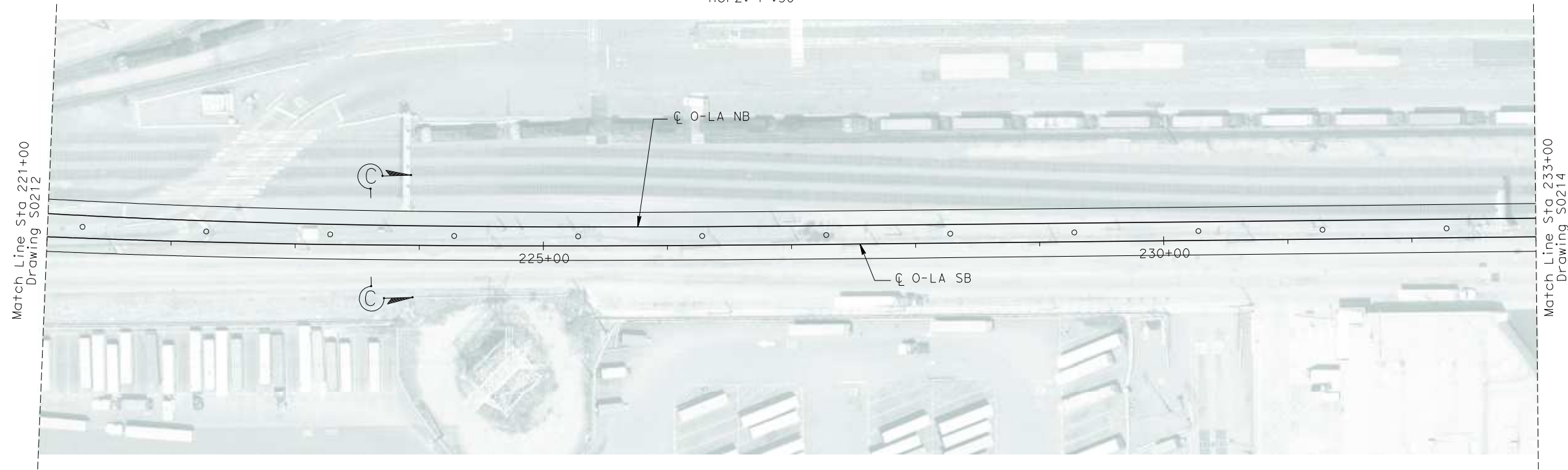
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SHOWN.
NB O-LA TRACK PROFILE
NOT SHOWN FOR CLARITY.



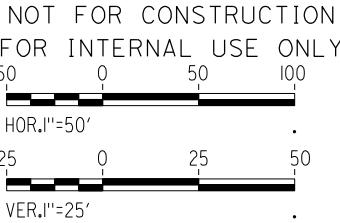
ELEVATION
Vert: 1":25'
Horz: 1":50'



To Union Station



To Anaheim



PLAN
1":50'

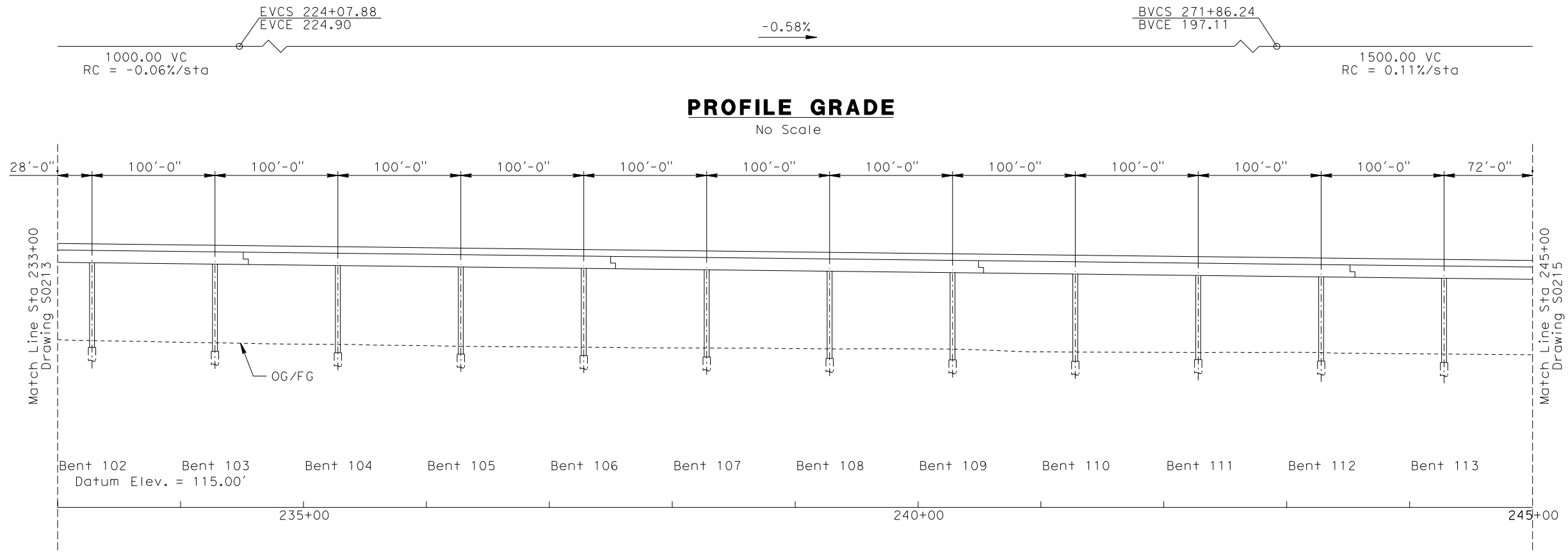
REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



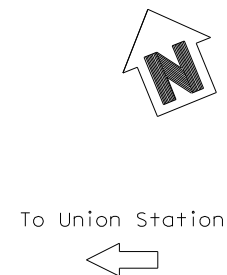
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0213
SCALE AS SHOWN
SHEET NO. 204 OF 245

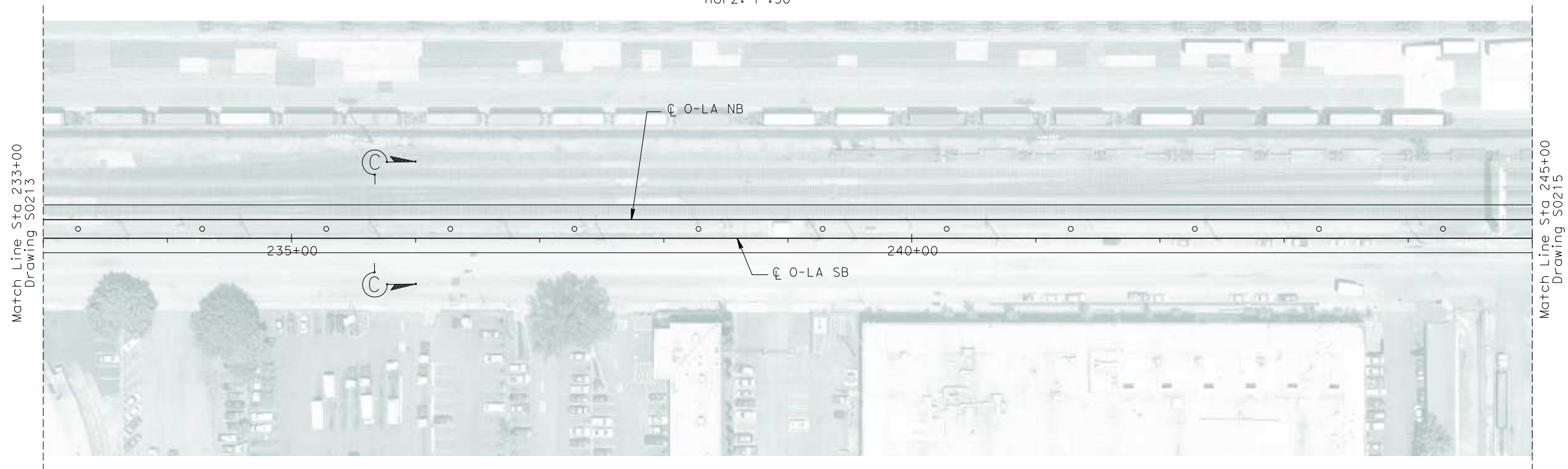


ELEVATION

Vert: 1":25'
Horz: 1":50'



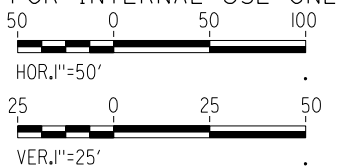
To Union Station



To Anaheim



NOT FOR CONSTRUCTION
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PLAN

1":50'

R:\Proj\4013179\4013179_000\Drawings\STRA\LA-S-0214.dgn
\$\$\$\$\$SYTIME\$\$\$\$\$
Branquerry

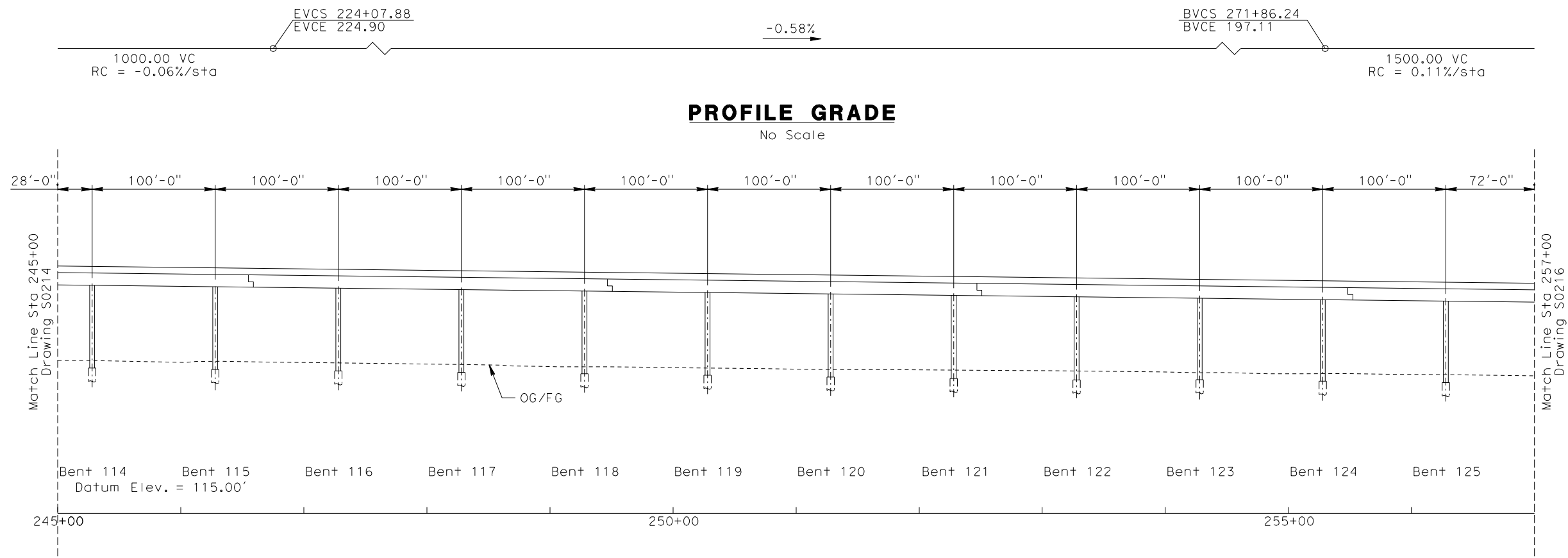
REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

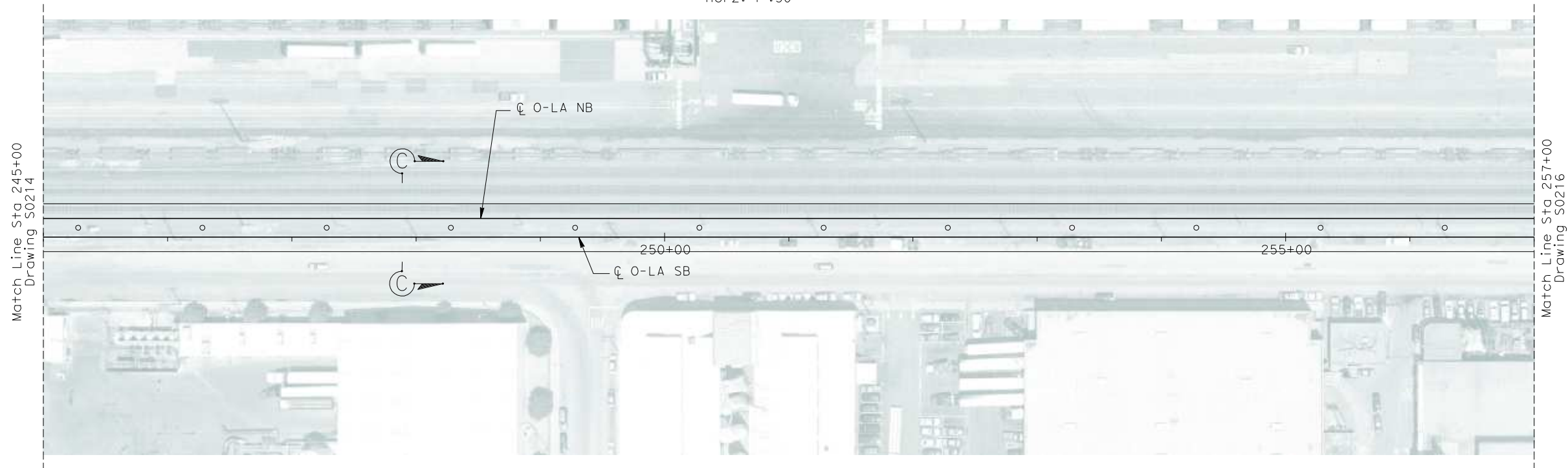
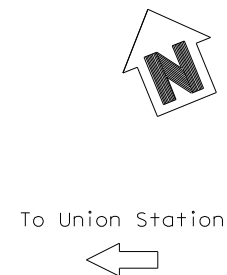
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



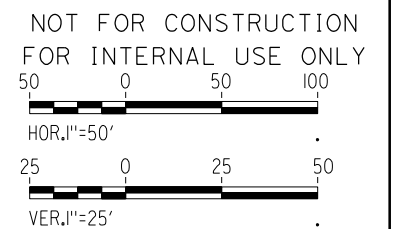
CALIFORNIA HIGH SPEED TRAIN PROJECT		CONTRACT NO. HSR06-0005
ORANGE COUNTY TO LOS ANGELES		DRAWING NO. S0214
BASELINE ALIGNMENT		SCALE AS SHOWN
VIADUCT #2		SHEET NO. 205 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'



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James.Lines

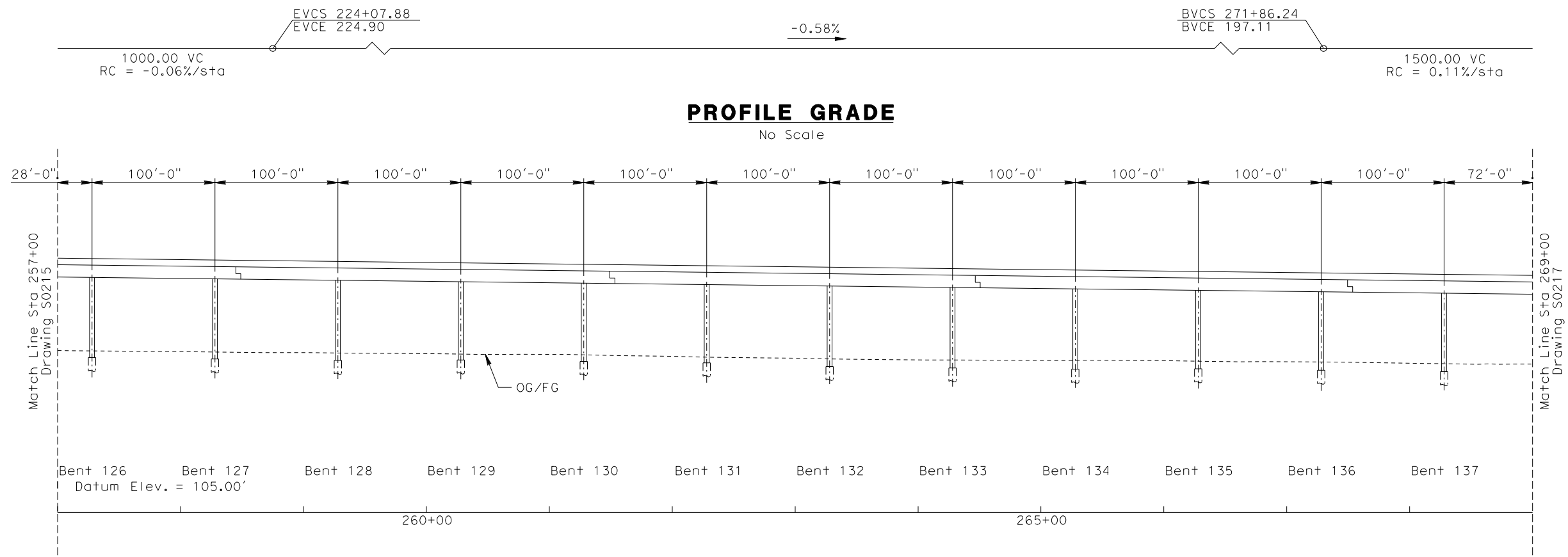
REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

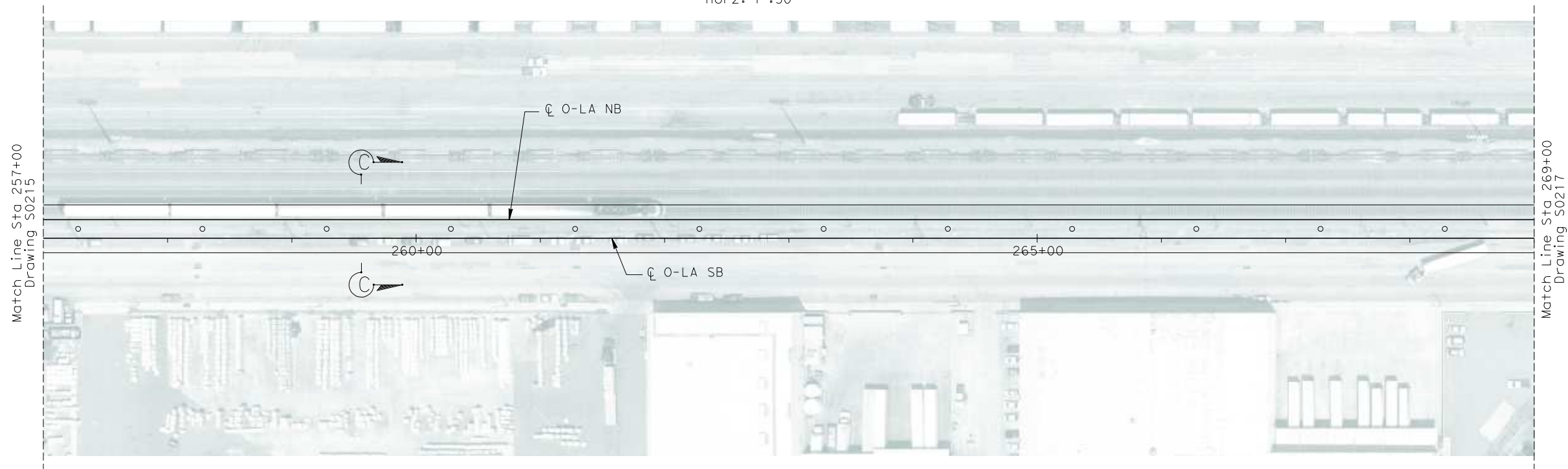
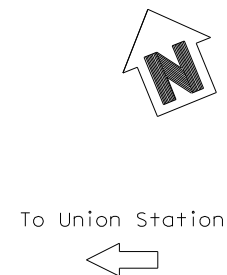
 **STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

 **CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

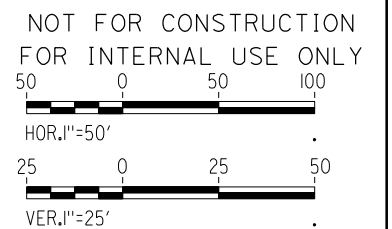
CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES BASELINE ALIGNMENT VIADUCT #2	CONTRACT NO. HSR06-0005
	DRAWING NO. S0215
	SCALE AS SHOWN
	SHEET NO. 206 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'




PLAN
1":50'



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

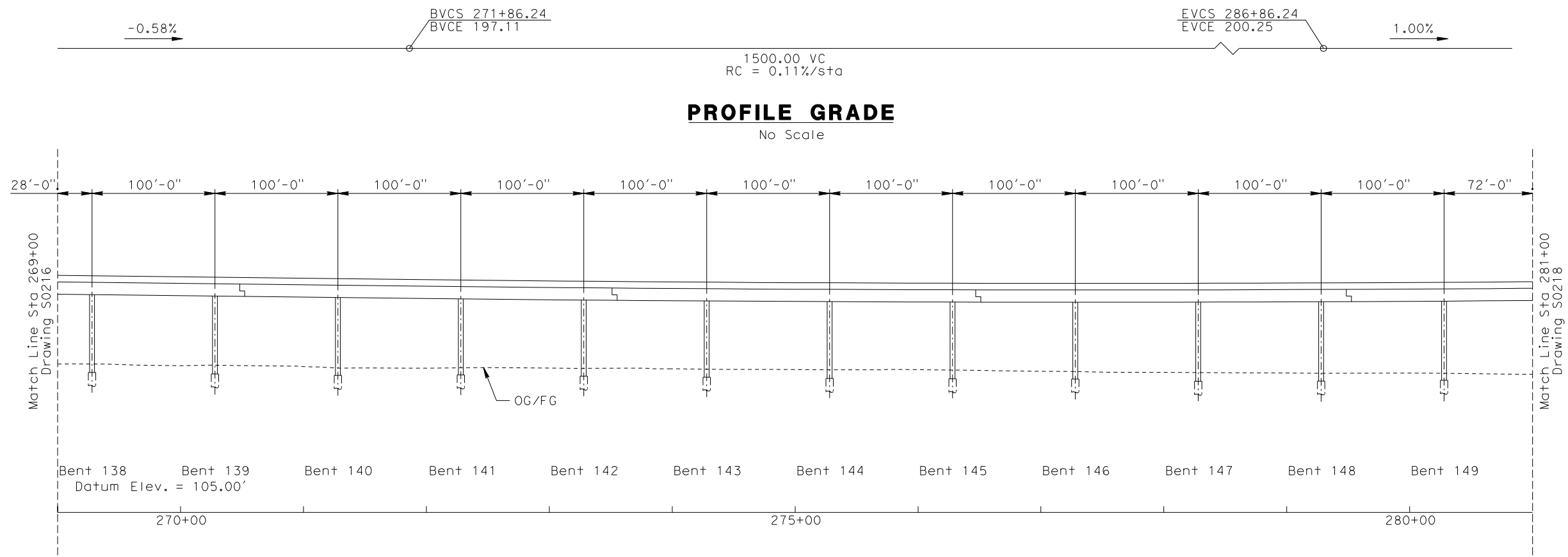
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

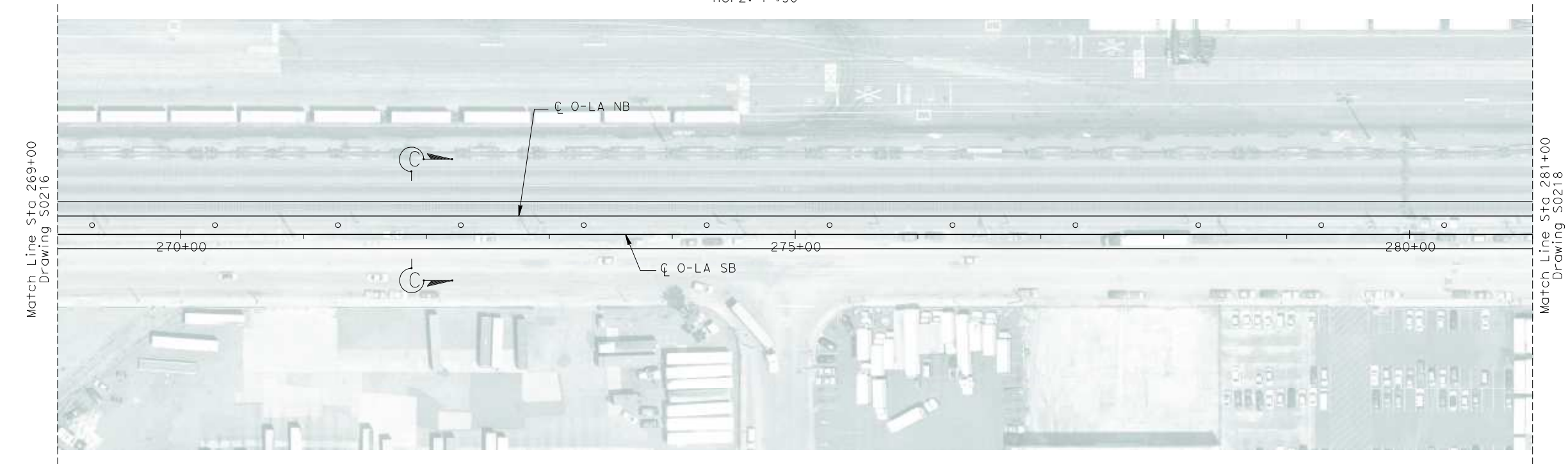
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0216
SCALE AS SHOWN
SHEET NO. 207 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'

NOT FOR CONSTRUCTION
FOR INTERNAL USE ONLY


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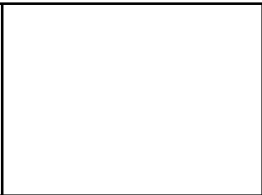
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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY B. QUERY
DRAWN BY L. HUFF.
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

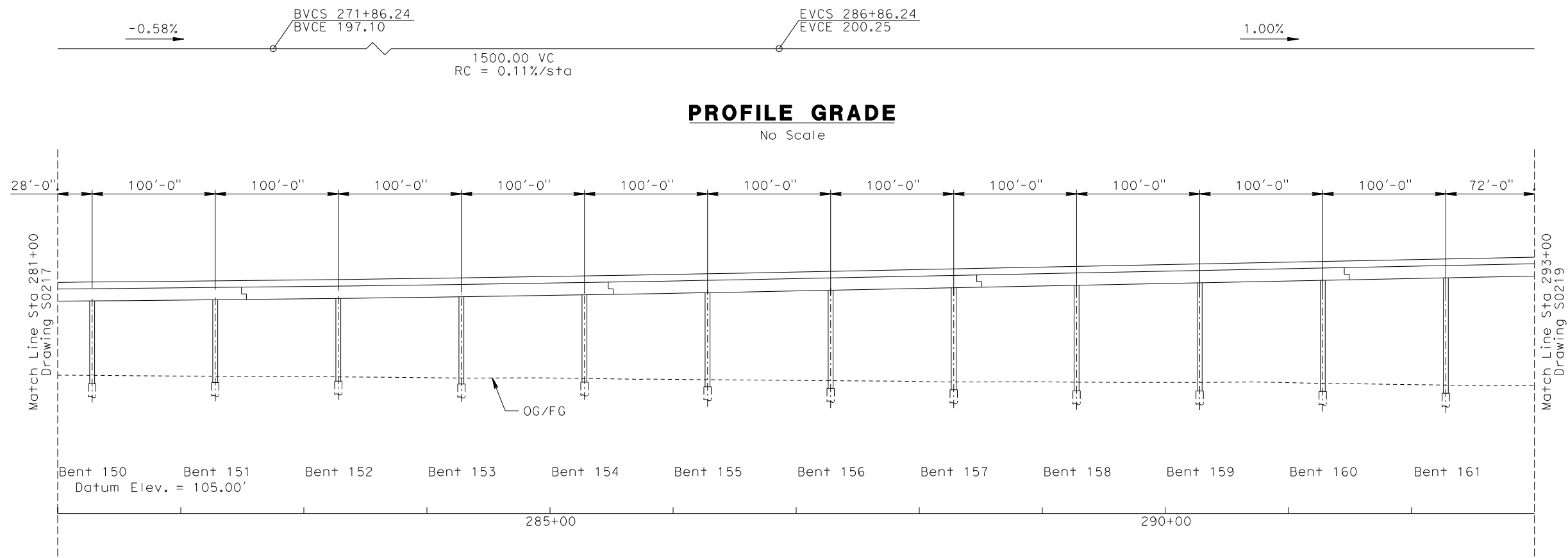


**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
Without ever leaving the ground.

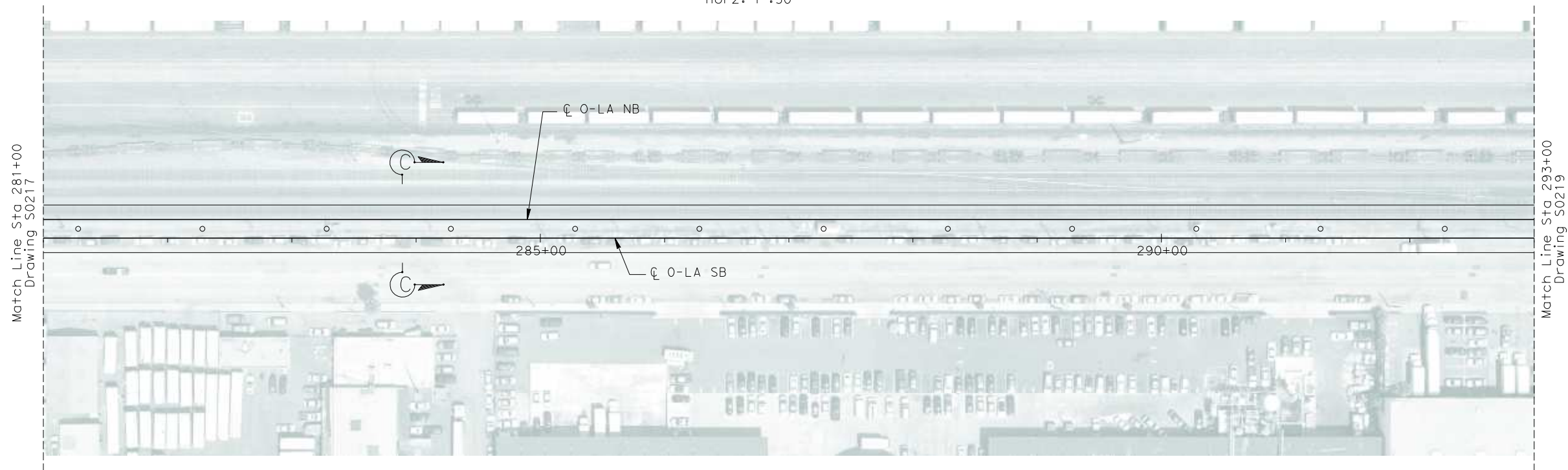
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

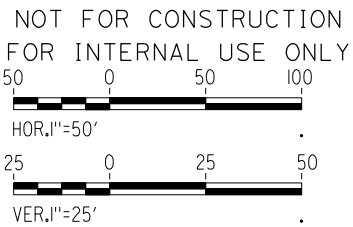
CONTRACT NO. HSR06-0005
DRAWING NO. S0217
SCALE AS SHOWN
SHEET NO. 208 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'




PLAN
1":50'

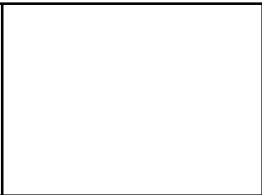


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IN CHARGE D. THOMSON
DATE 02-13-09

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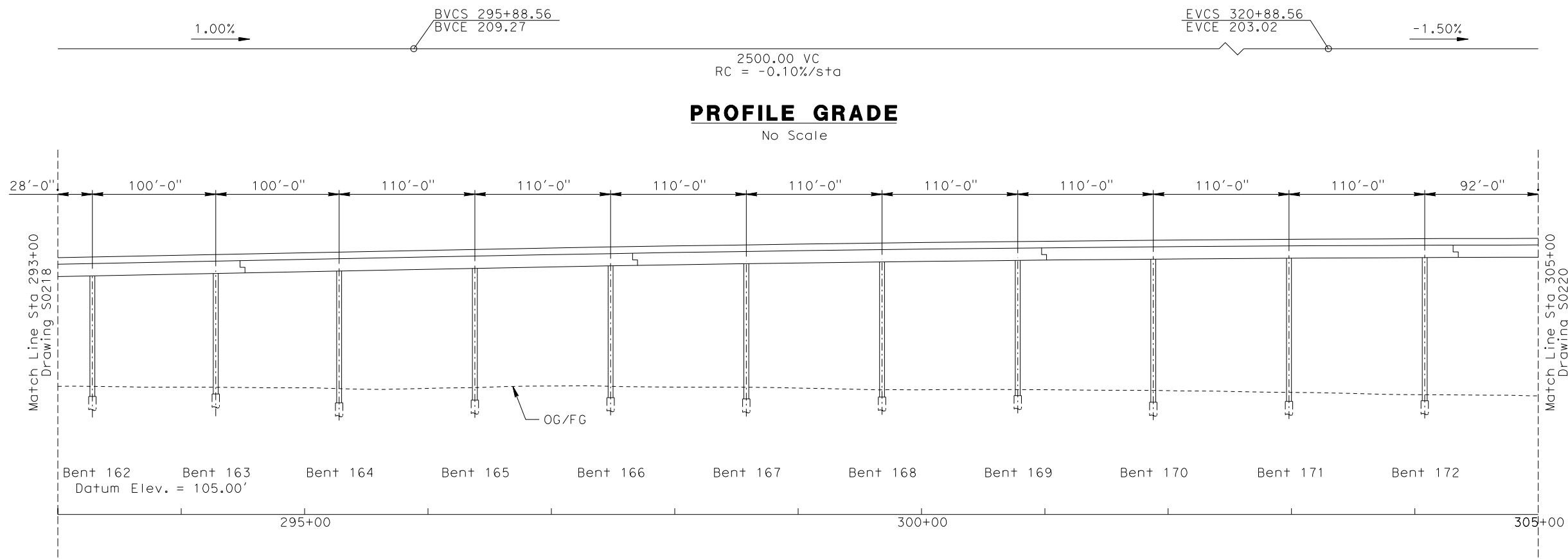


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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

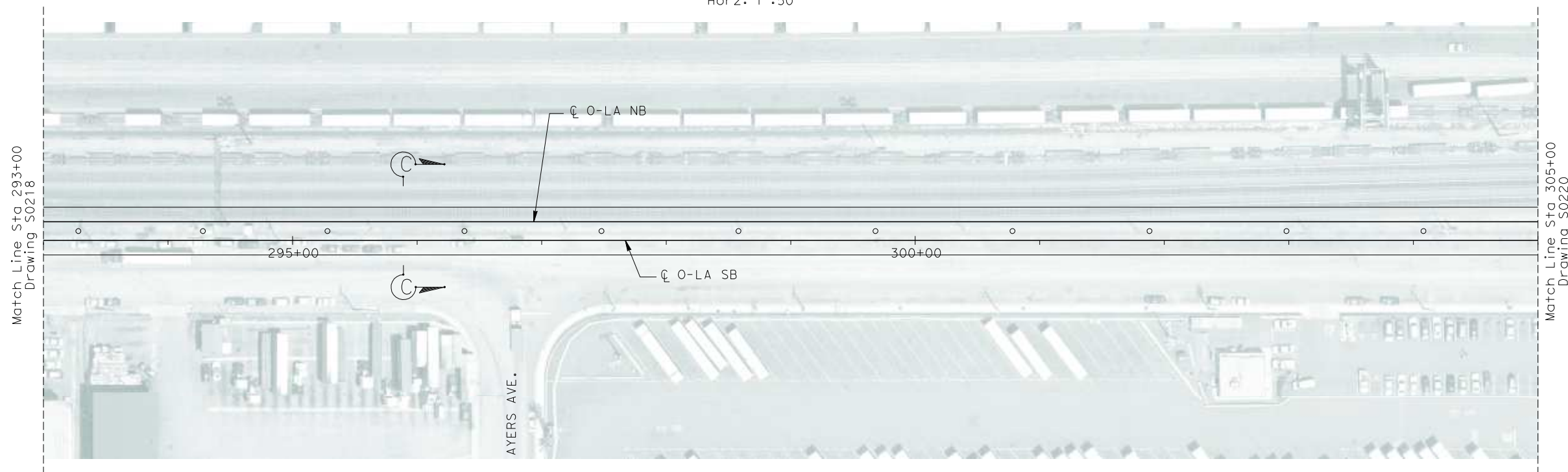
VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0218
SCALE AS SHOWN
SHEET NO. 209 OF 245



ELEVATION

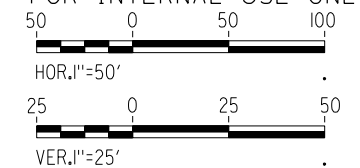
Vert: 1"=25'
Horz: 1"=50'



PLAN

1"=50'

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\$\$\$\$\$SYTIME\$\$\$\$\$
Branquerry

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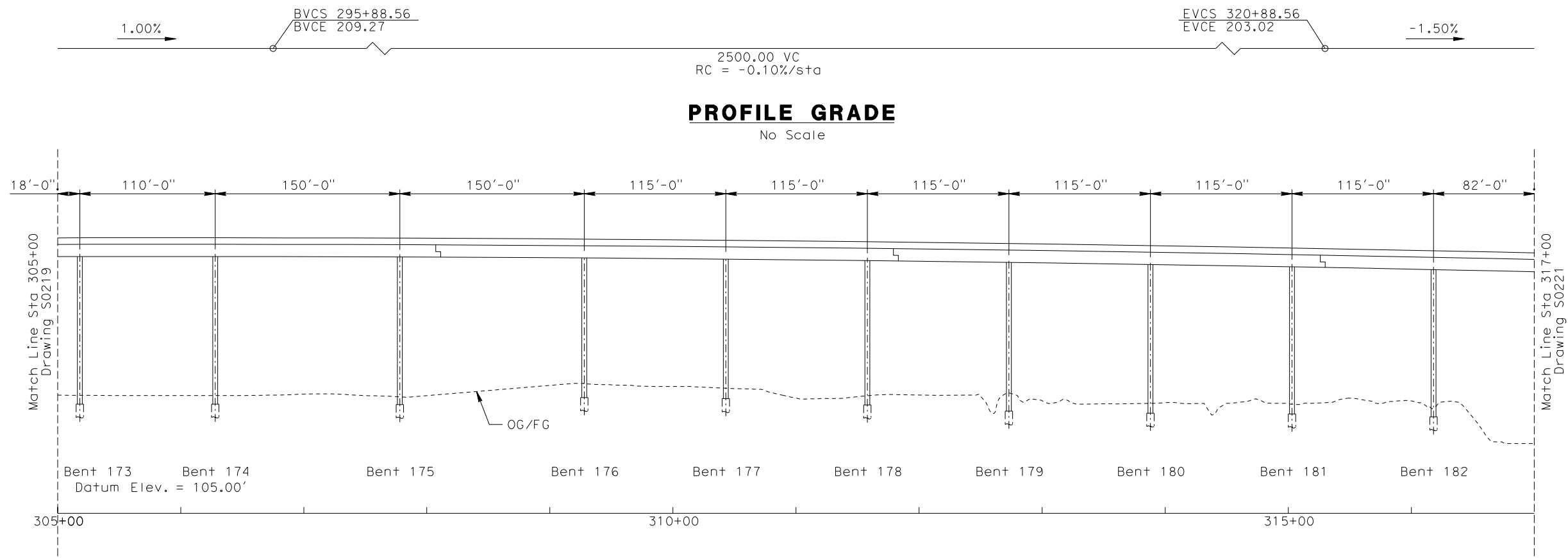


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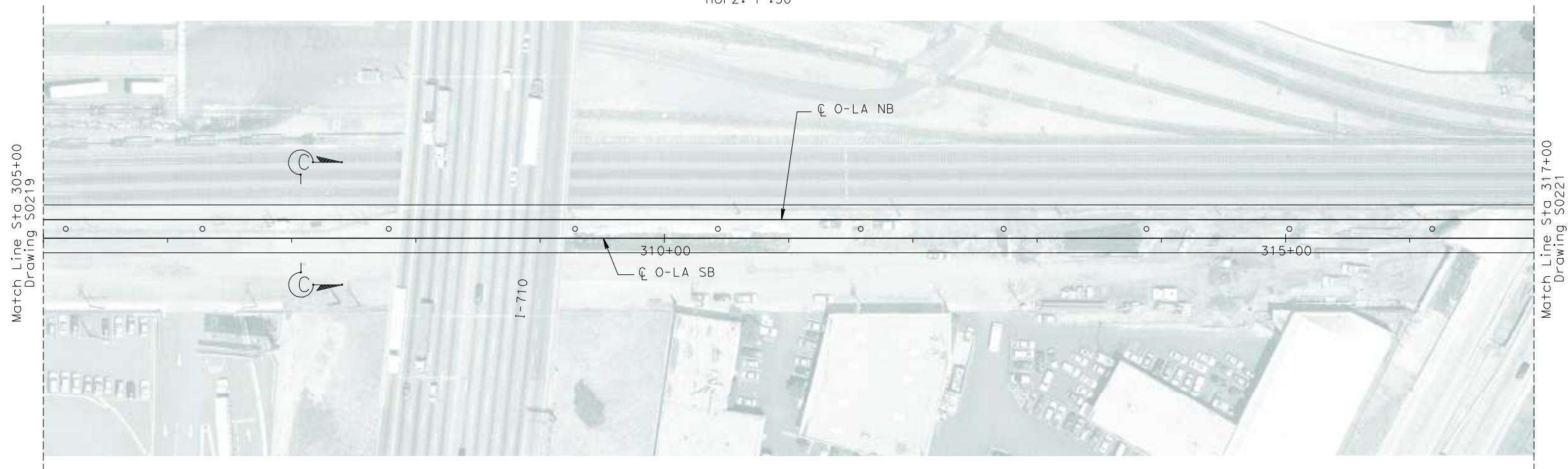
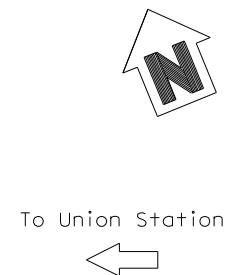
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #2

CONTRACT NO. HSR06-0005
DRAWING NO. S0219
SCALE AS SHOWN
SHEET NO. 210 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'

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50 0 50 100
HOR. 1"=50'

25 0 25 50
VER. 1"=25'

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Branquery

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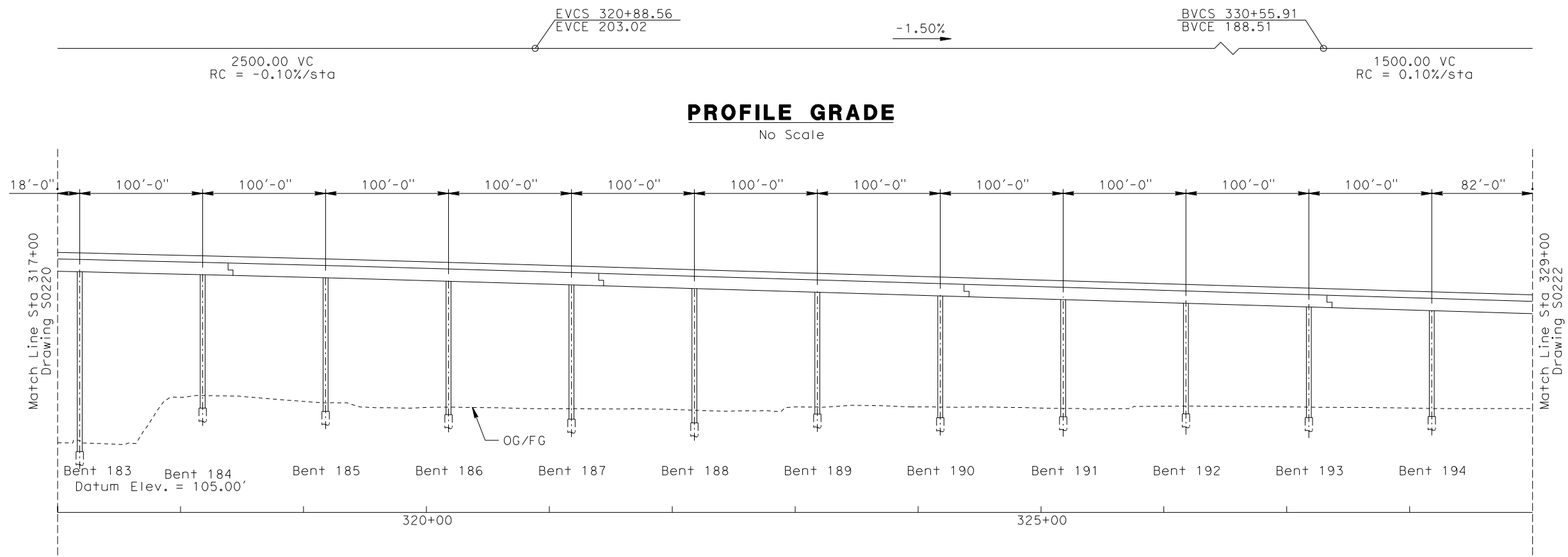
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LOS ANGELES, CA 90017-2556

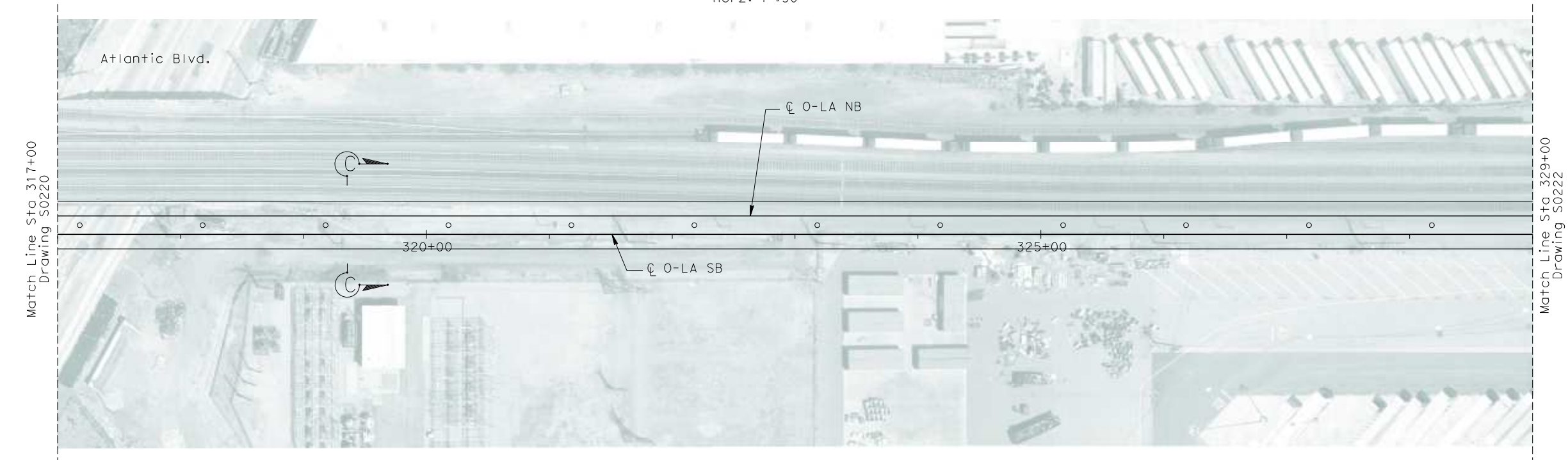


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

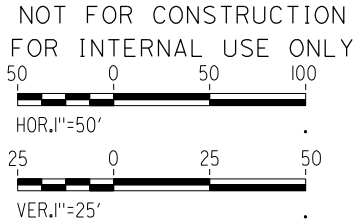
CONTRACT NO. HSR06-0005
DRAWING NO. S0220
SCALE AS SHOWN
SHEET NO. 211 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'




PLAN
1":50'



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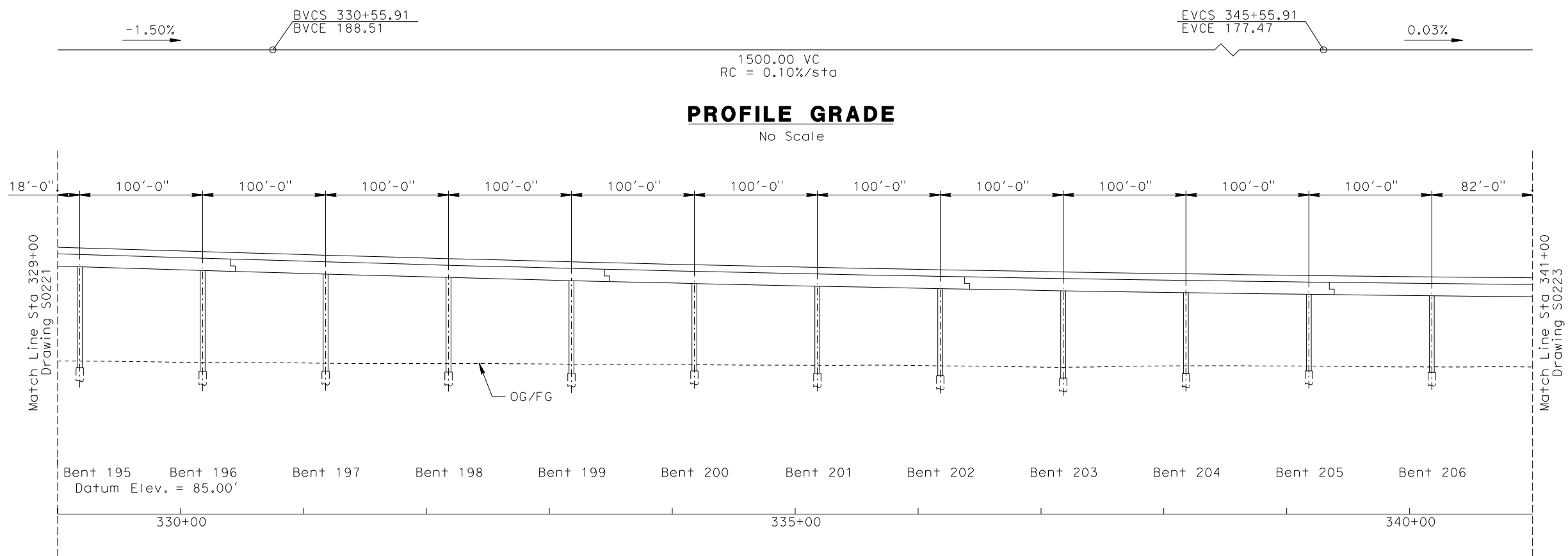
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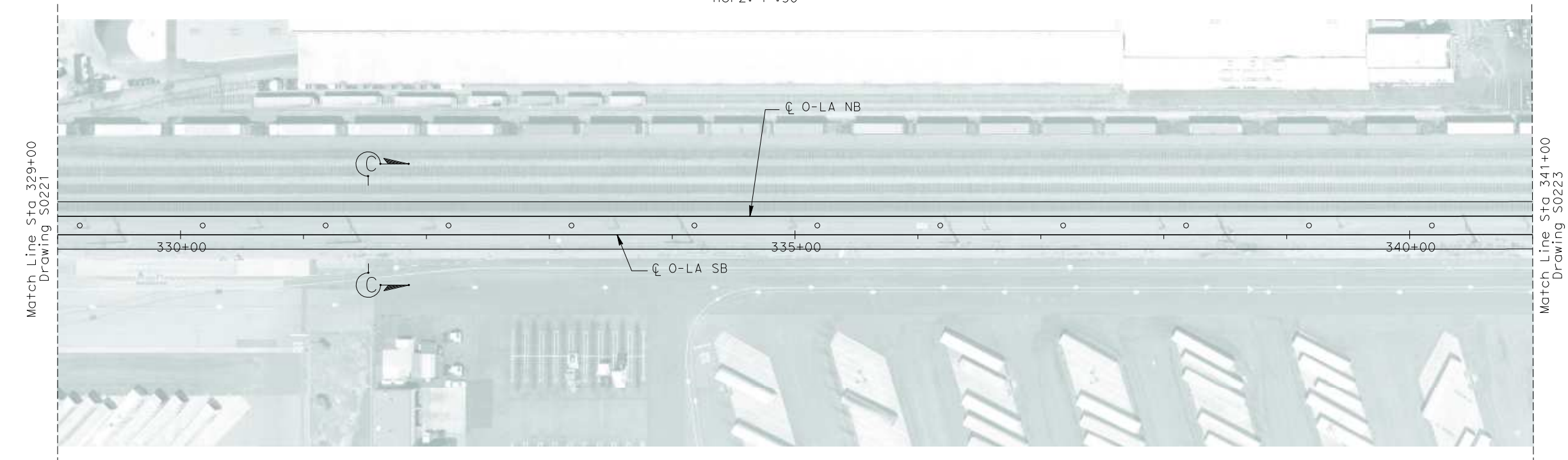
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

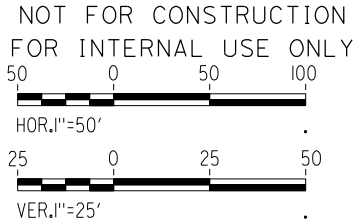
CONTRACT NO. HSR06-0005
DRAWING NO. S0221
SCALE AS SHOWN
SHEET NO. 212 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'




PLAN
1":50'



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REV	DATE	BY	SUB	APP	DESCRIPTION

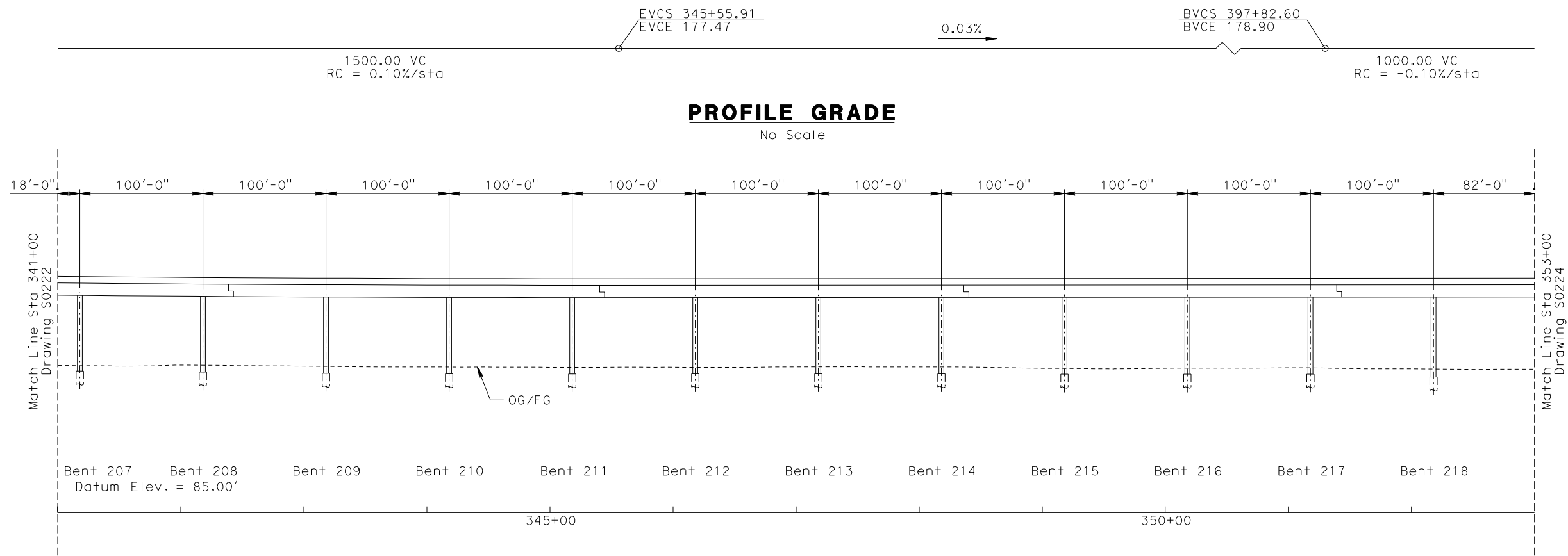
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B. QUERY
DRAWN BY
L. HUFF.
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09

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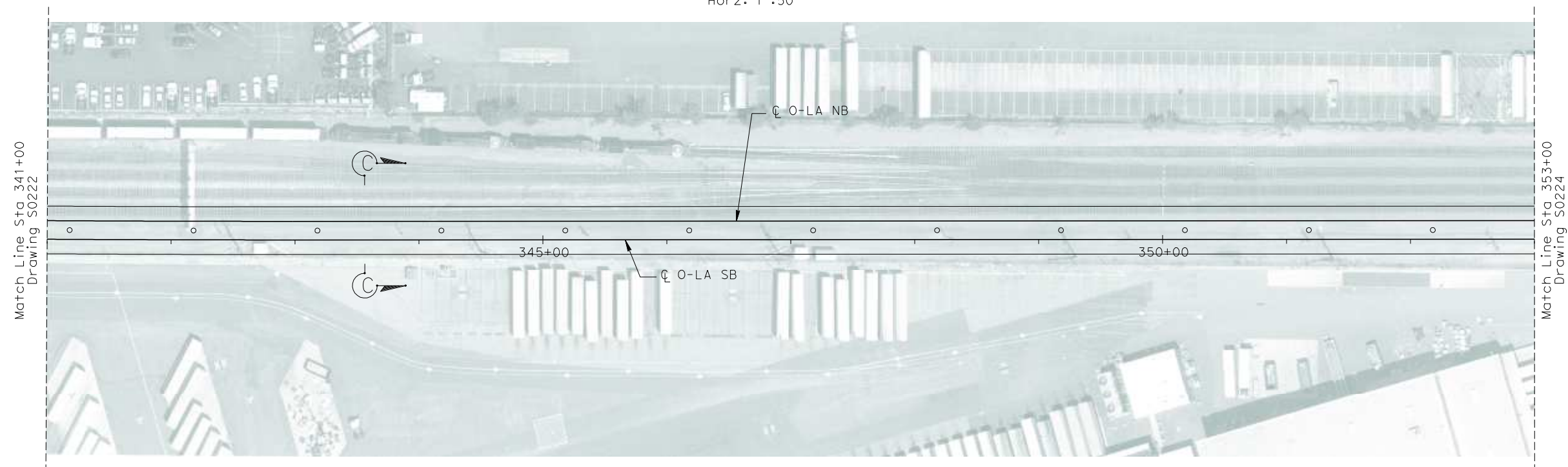
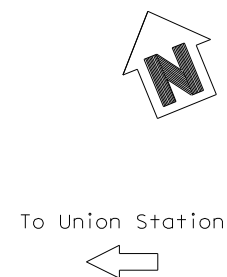


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

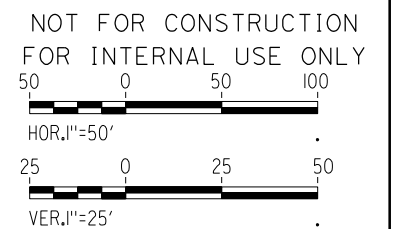
CONTRACT NO. HSR06-0005
DRAWING NO. S0222
SCALE AS SHOWN
SHEET NO. 213 OF 245



ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'



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REV	DATE	BY	SUB	APP	DESCRIPTION

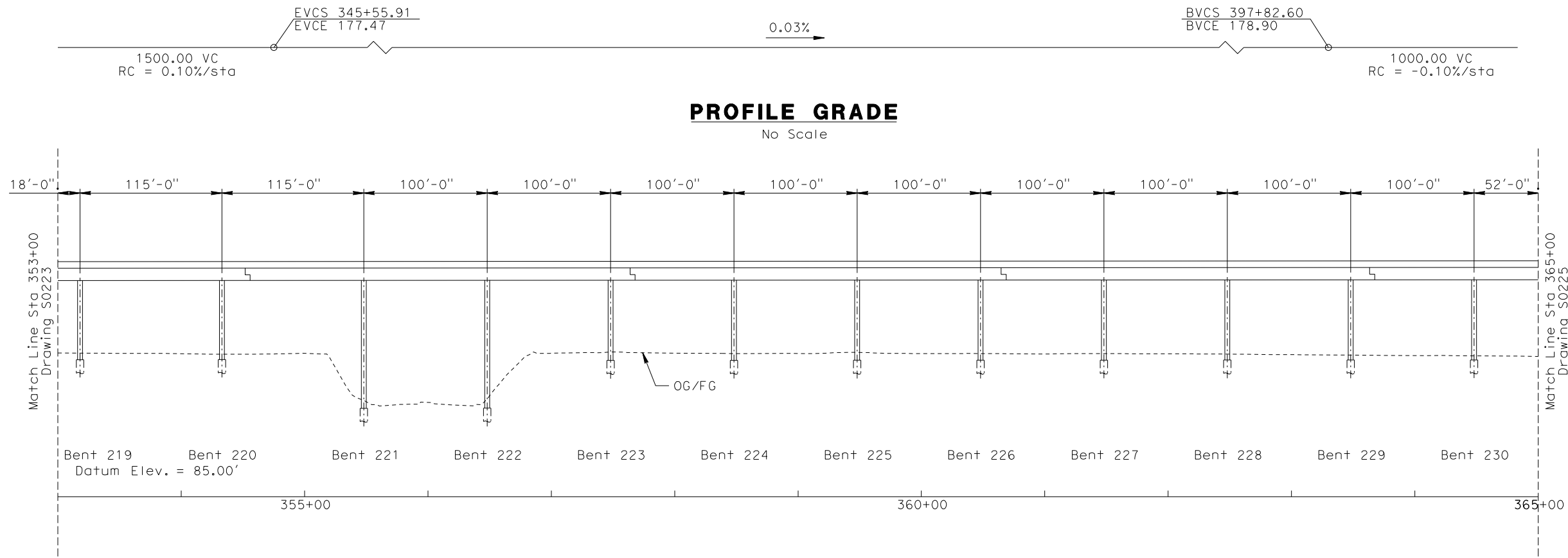
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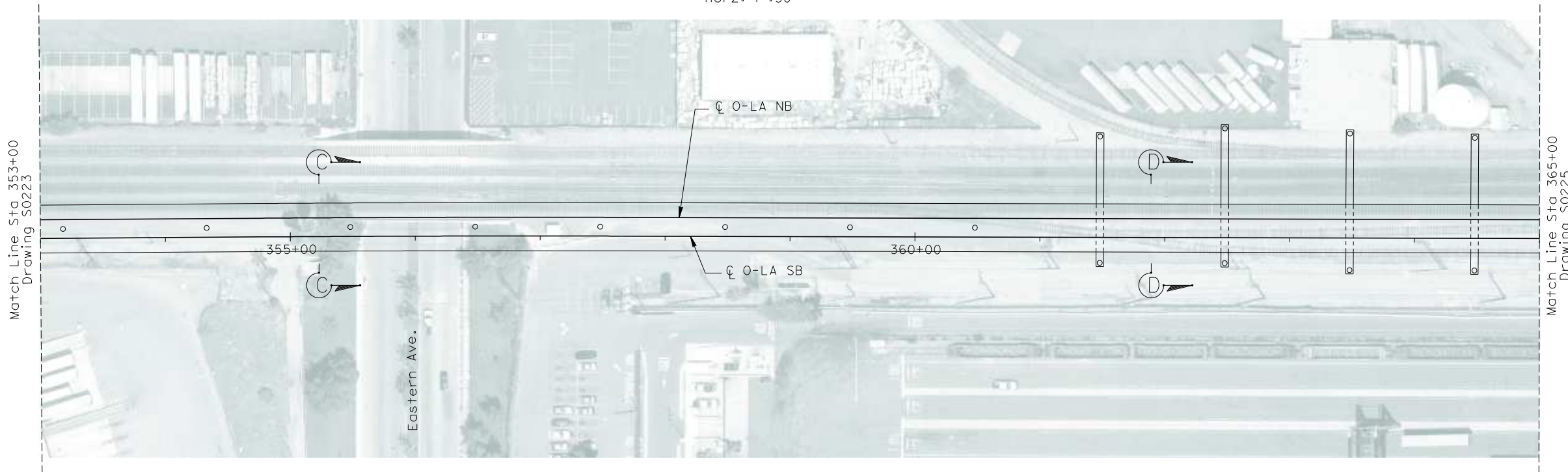
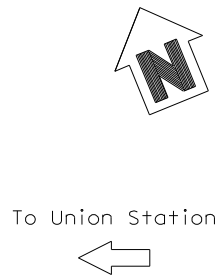


CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES BASELINE ALIGNMENT VIADUCT #2		CONTRACT NO. HSR06-0005
		DRAWING NO. S0223
		SCALE AS SHOWN
		SHEET NO. 214 OF 245

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ELEVATION
Vert: 1":25'
Horz: 1":50'



PLAN
1":50'

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50 0 50 100


HOR. 1"=50'

25 0 25 50

VER. 1"=25'

REV	DATE	BY	SUB	APP	DESCRIPTION

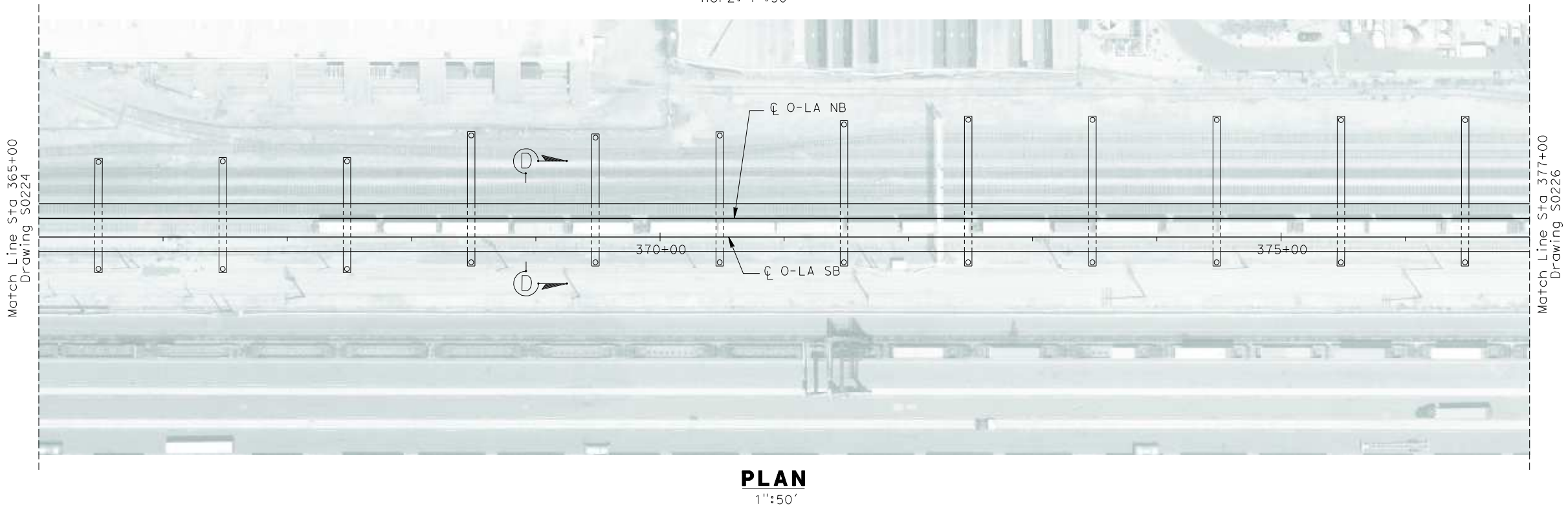
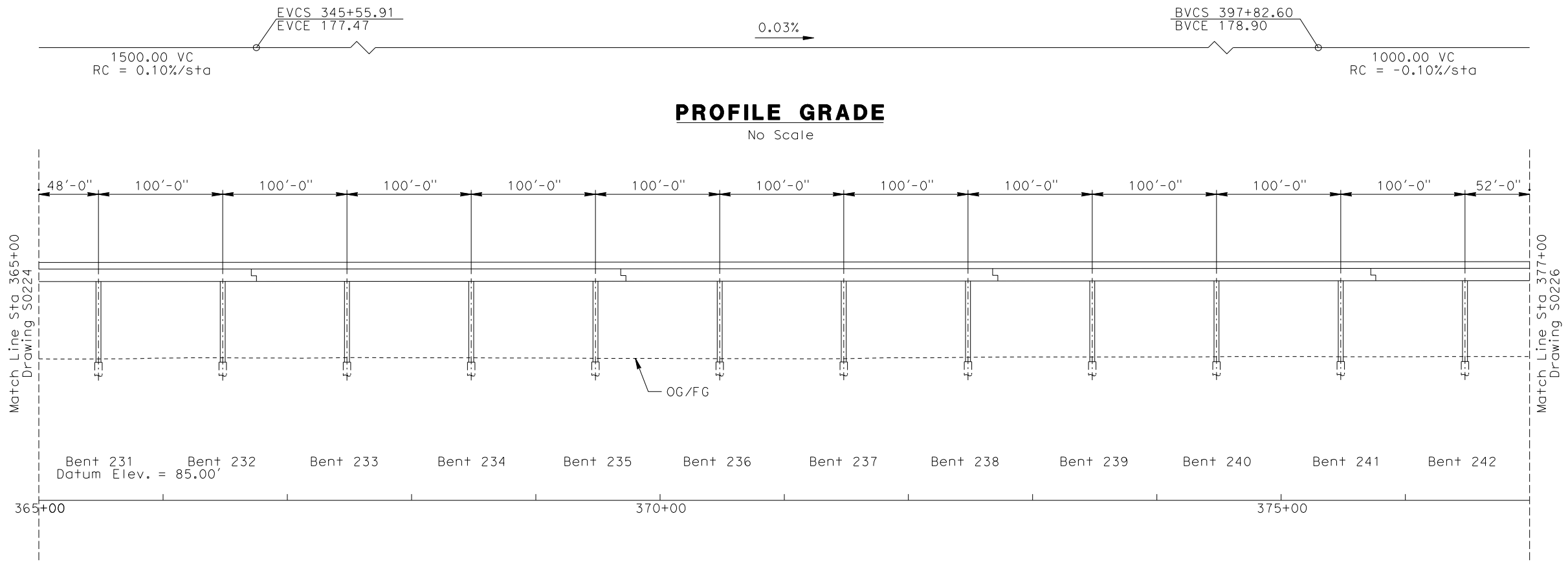
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		DRAWING NO. S0224
		SCALE AS SHOWN
		SHEET NO. 215 OF 245

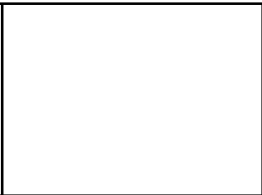
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LOS ANGELES, CA 90017-2556

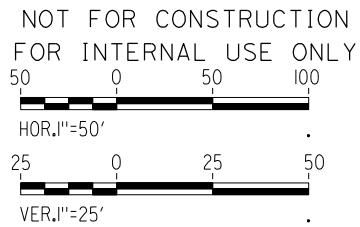
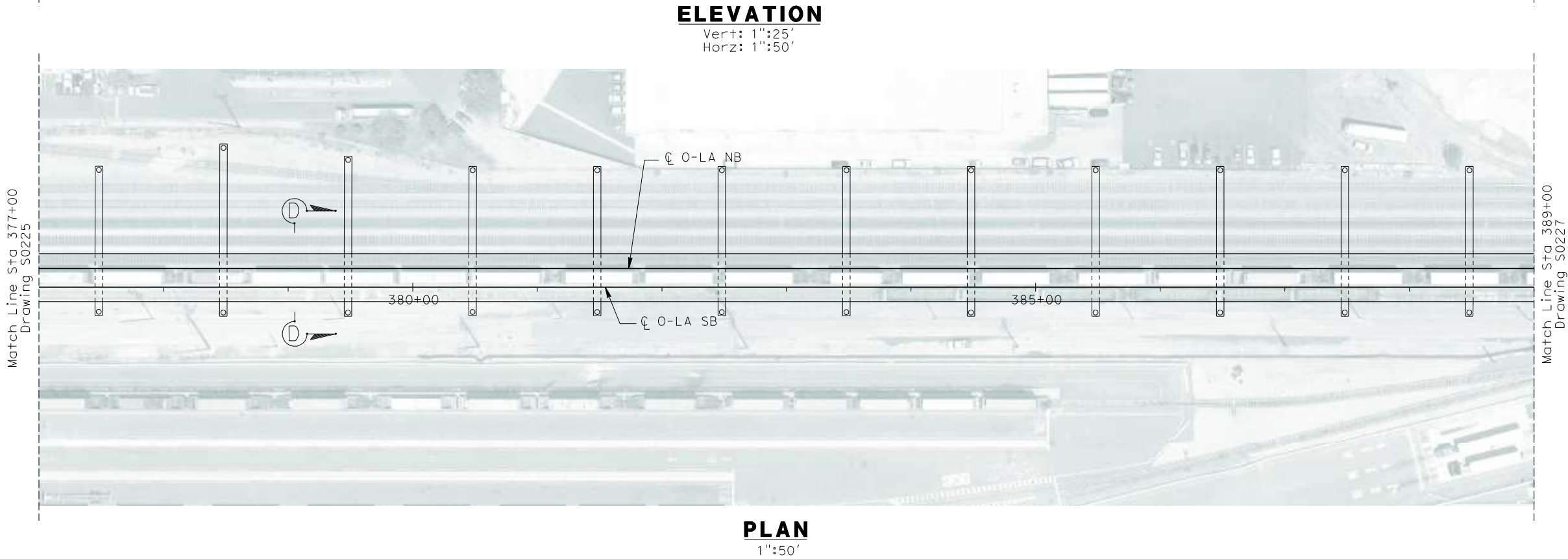
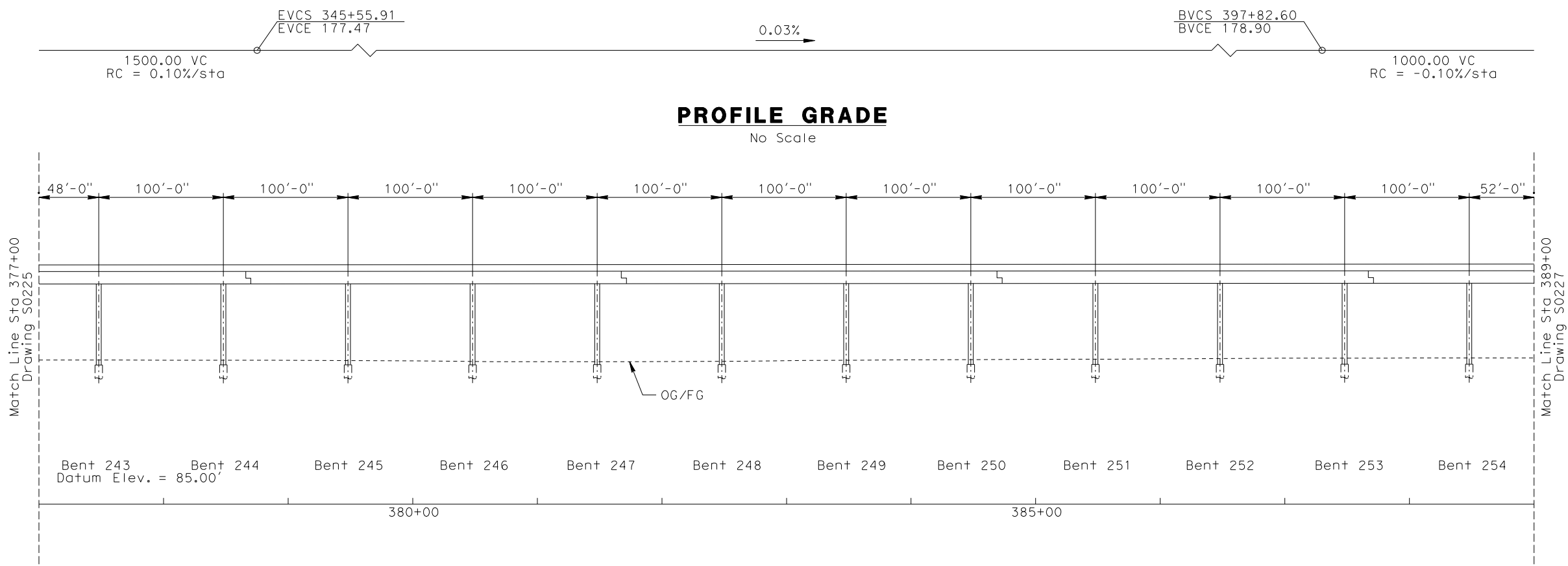
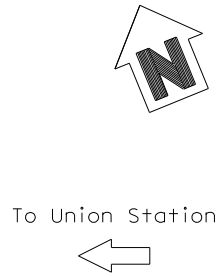


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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2


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DRAWING NO. S0225
SCALE AS SHOWN
SHEET NO. 216 OF 245

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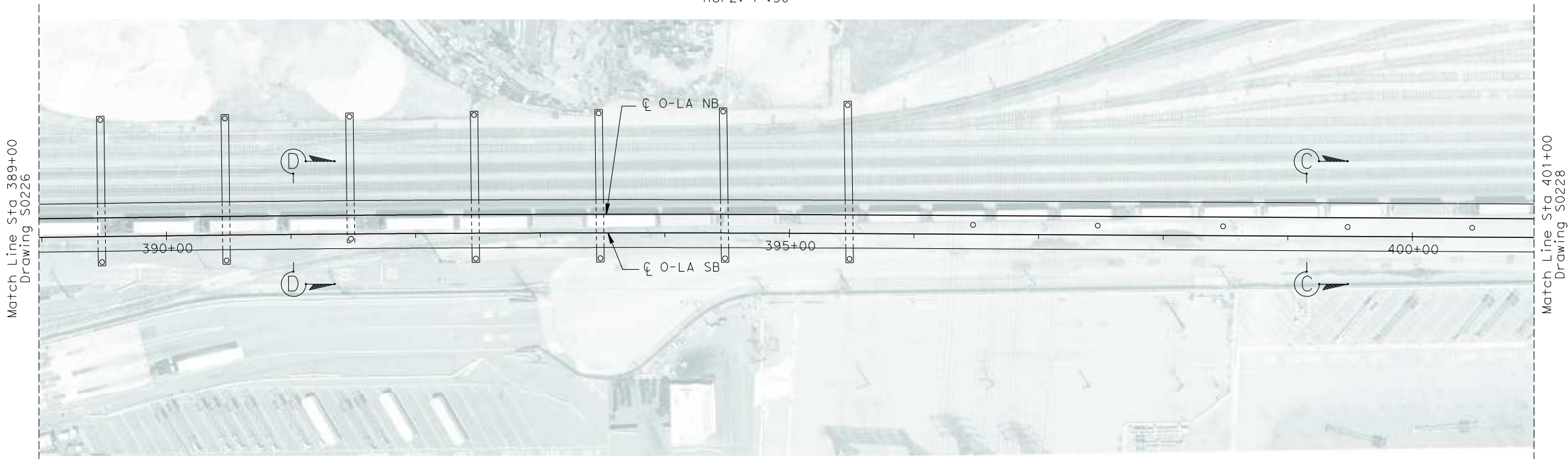
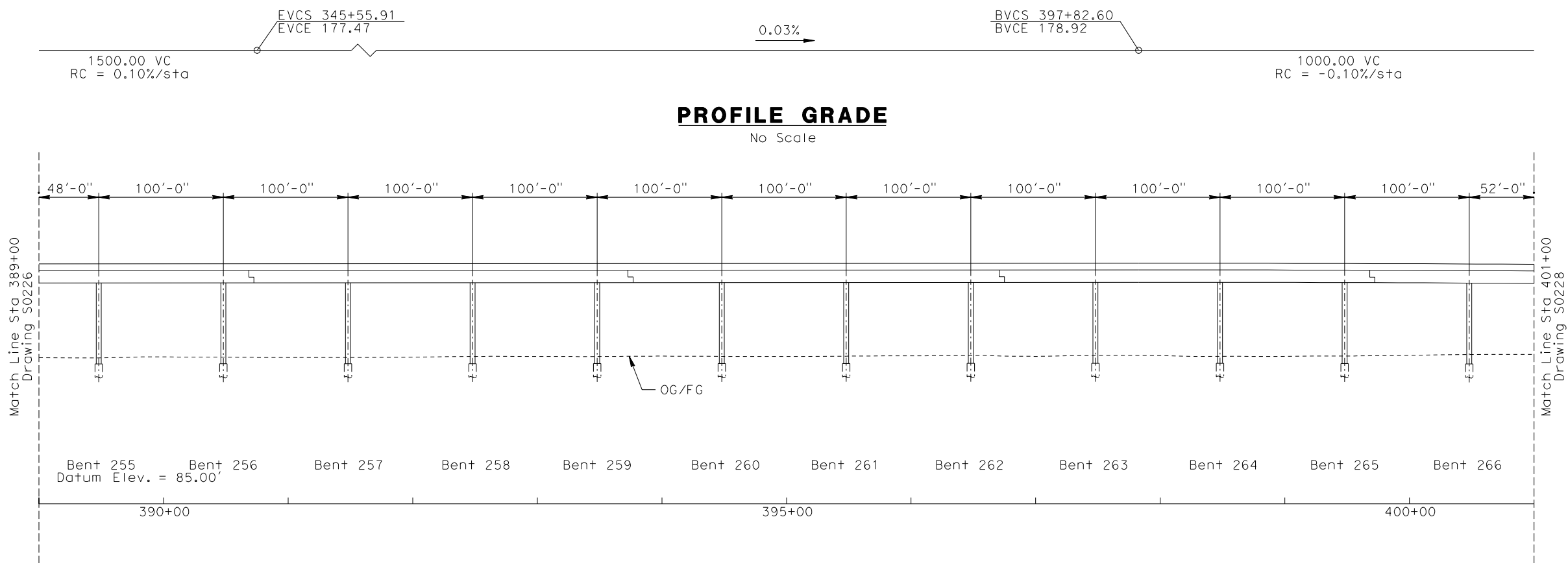
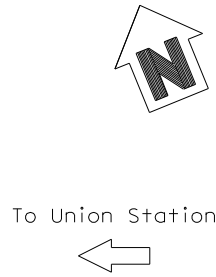
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LOS ANGELES, CA 90017-2556



CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
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VIADUCT #2

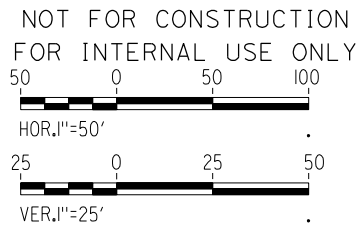
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SCALE AS SHOWN
SHEET NO. 217 OF 245

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ELEVATION
Vert: 1":25'
Horz: 1":50'

PLAN
1":50'



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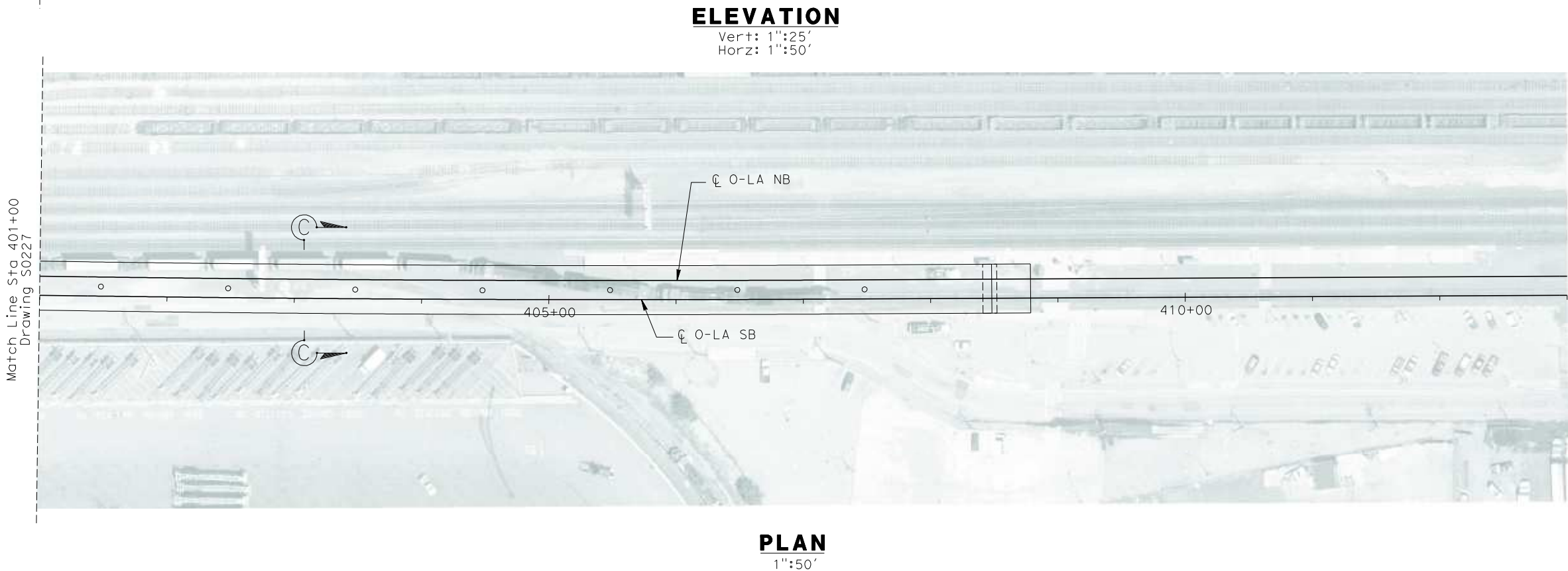
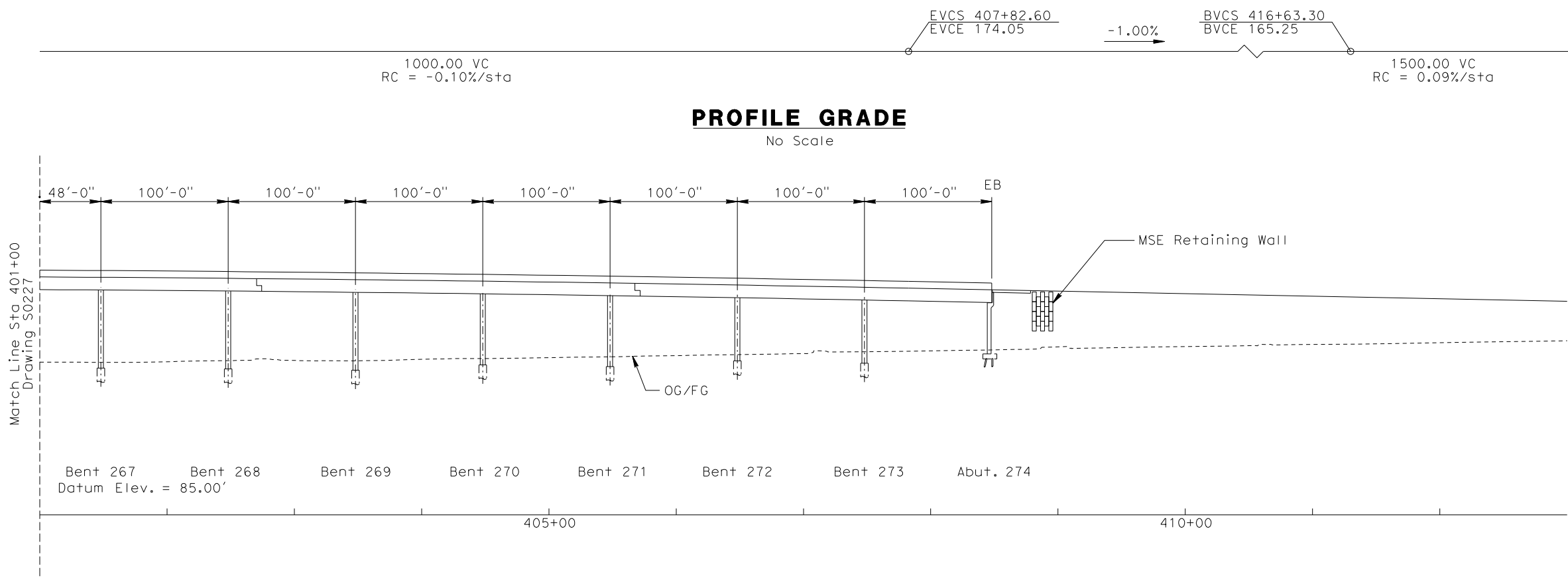
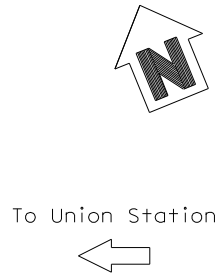


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CALIFORNIA HIGH SPEED TRAIN PROJECT ORANGE COUNTY TO LOS ANGELES BASELINE ALIGNMENT VIADUCT #2		CONTRACT NO. HSR06-0005
		DRAWING NO. S0227
		SCALE AS SHOWN
		SHEET NO. 218 OF 245

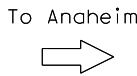
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
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HOR. 1"=50'

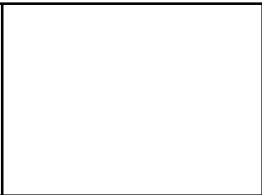
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VER. 1"=25'



REV	DATE	BY	SUB	APP	DESCRIPTION

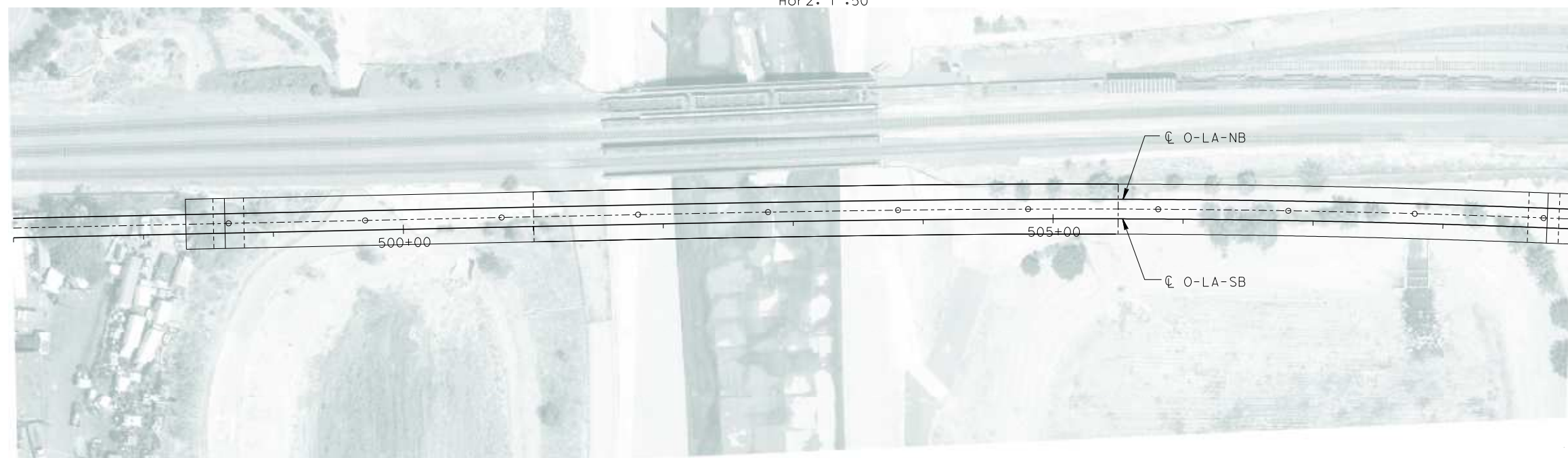
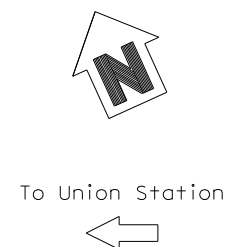
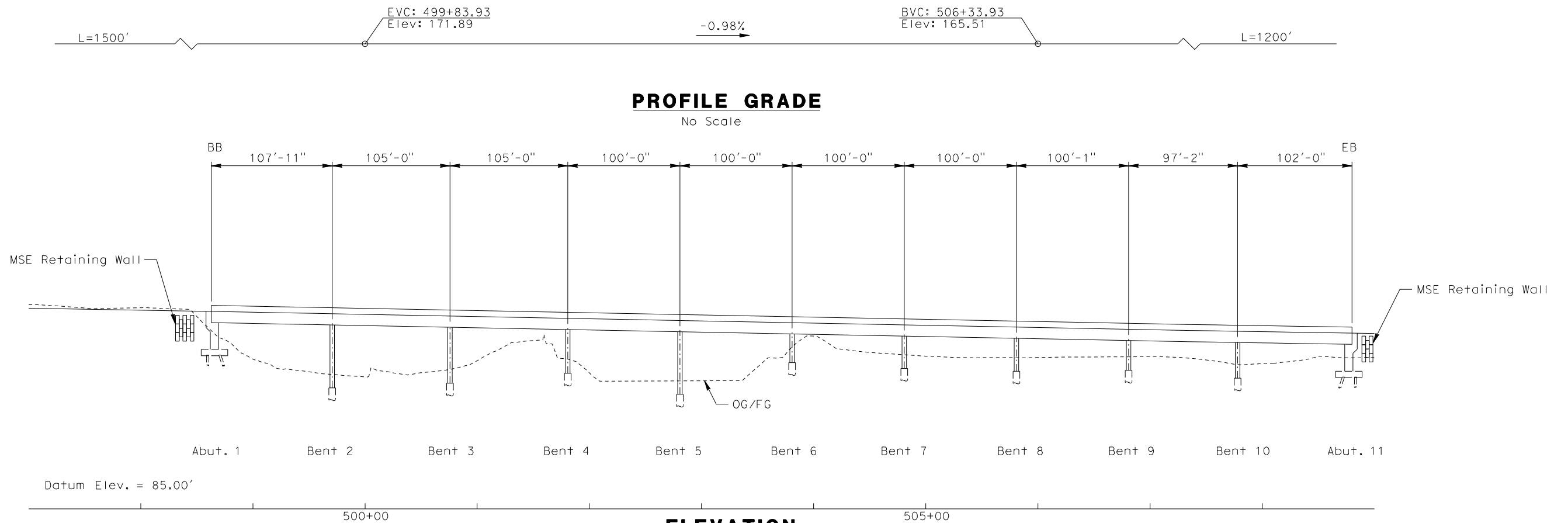
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #2

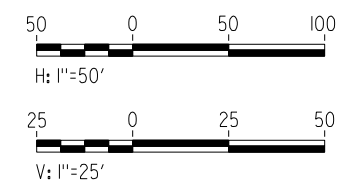
CONTRACT NO. HSR06-0005
DRAWING NO. S0228
SCALE AS SHOWN
SHEET NO. 219 OF 245



PLAN

1"=50'

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IN CHARGE D. THOMSON
DATE 02-13-09

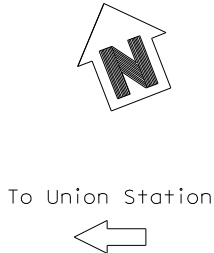
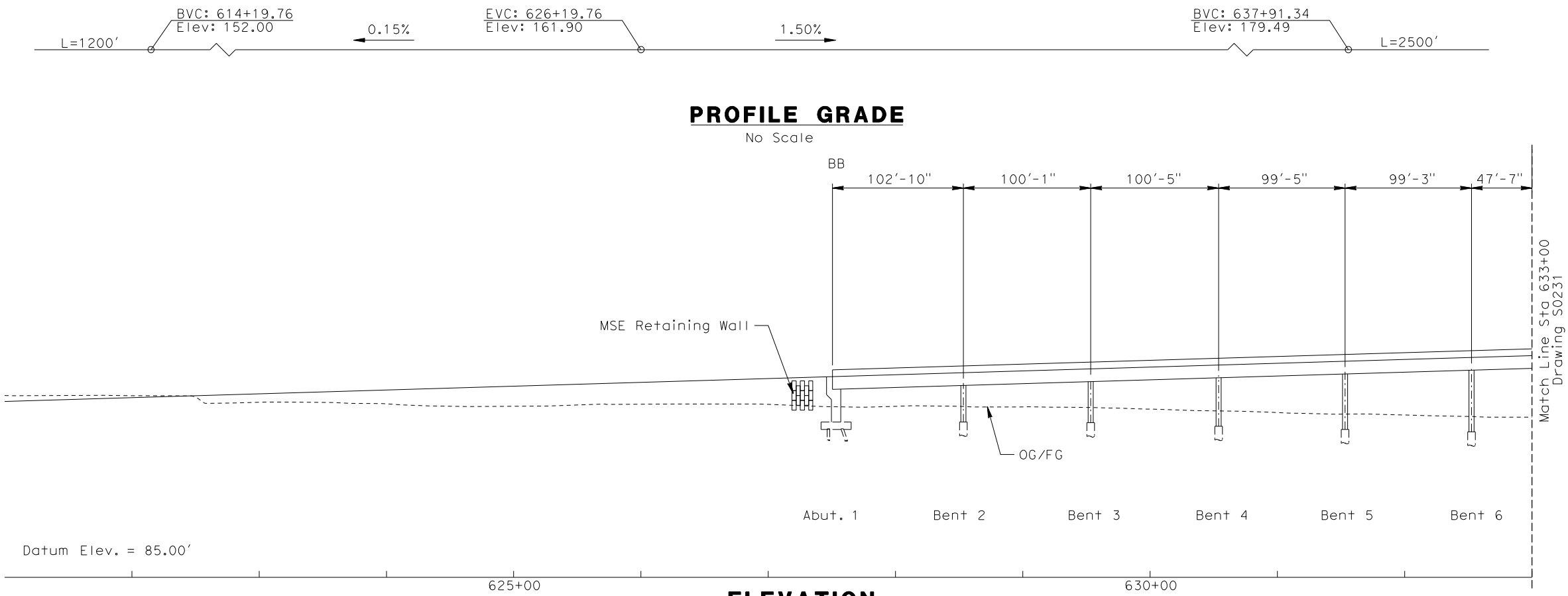


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #3

CONTRACT NO. HSR06-0005
DRAWING NO. S0229
SCALE AS SHOWN
SHEET NO. 220 OF 245


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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

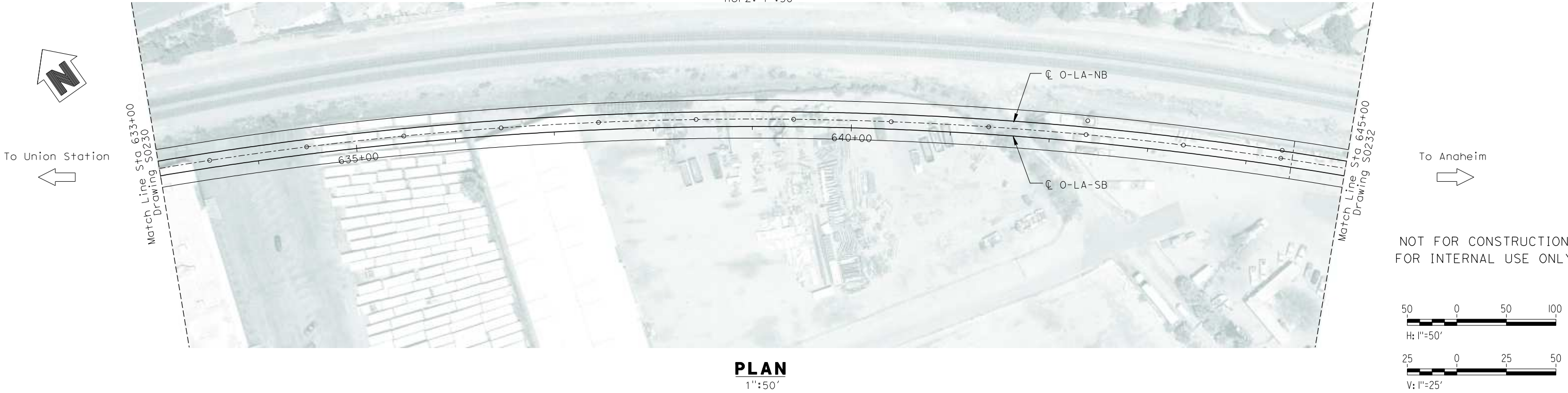
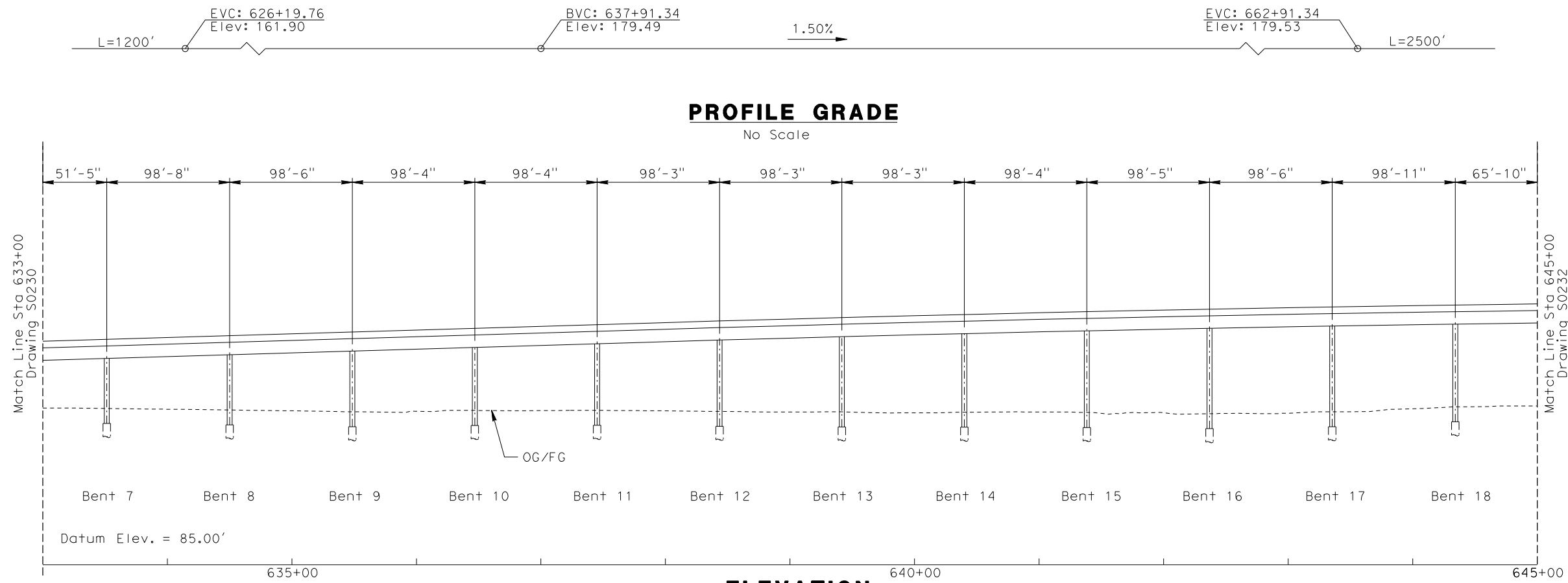




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FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #4

CONTRACT NO. HSR06-0005
DRAWING NO. S0230
SCALE AS SHOWN
SHEET NO. 221 OF 245




PLAN
1"=50'

I:\projects\4013179\4013179_000\Drawings\STP\OLA-S-0231-Adgn
dequzmmf \$\$\$\$\$\$SYTIME\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
W. H-H CHU
DRAWN BY
M.F. DE GUZMAN
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

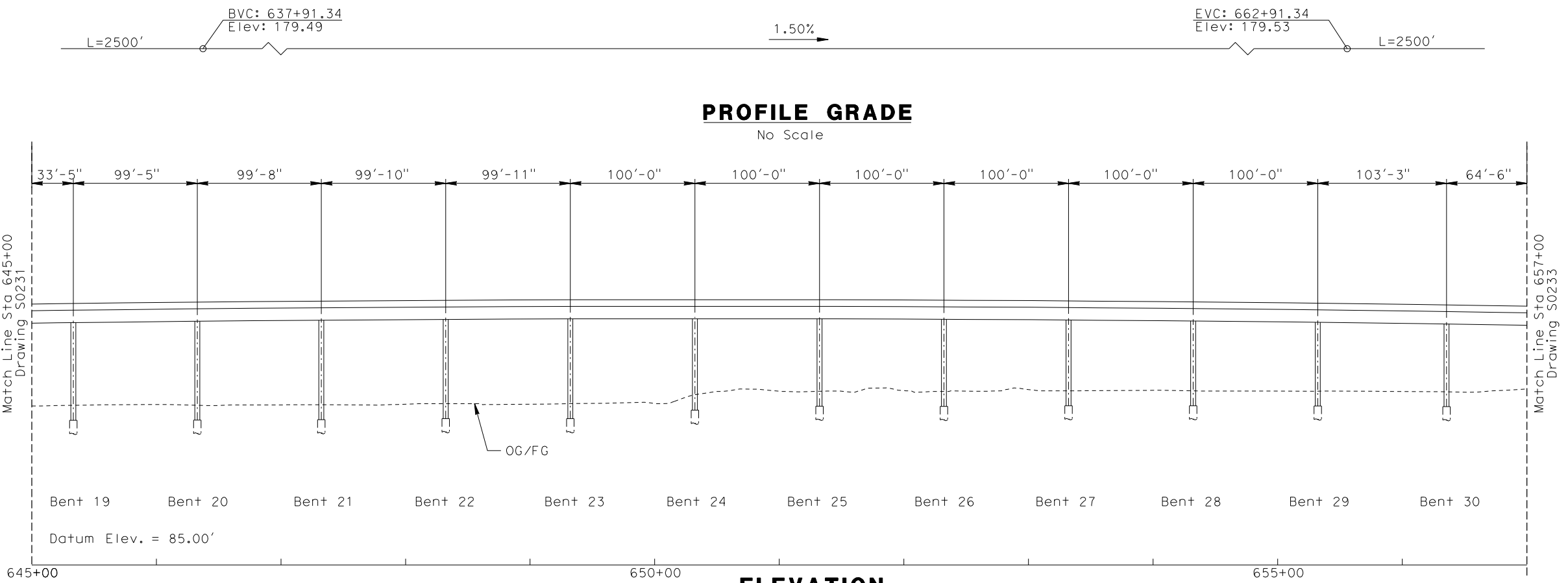


CALIFORNIA HIGH SPEED RAIL AUTHORITY

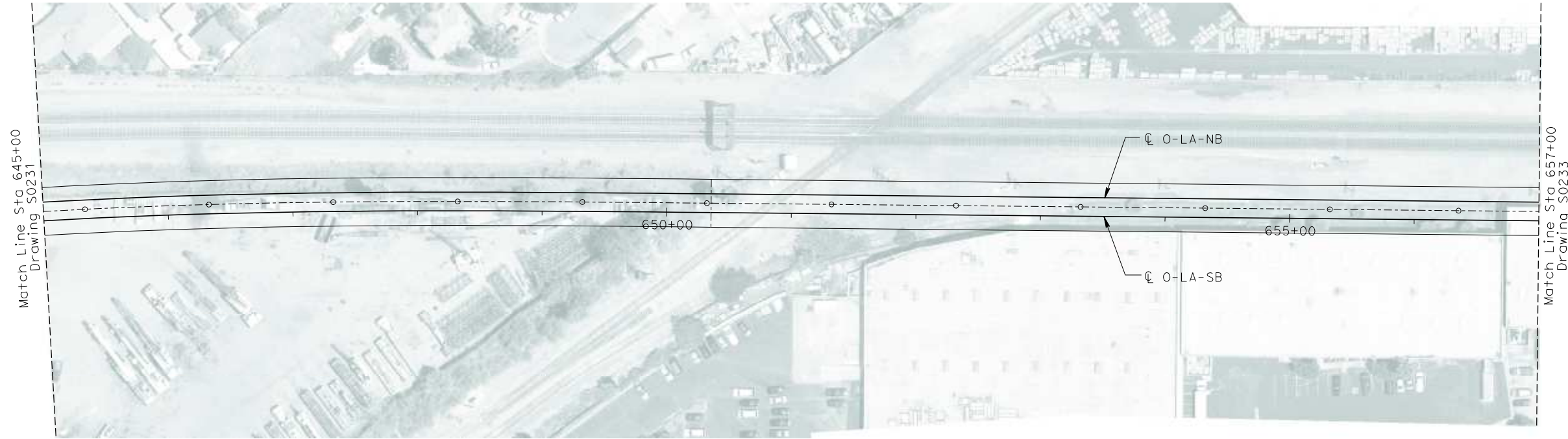
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #4

CONTRACT NO. HSR06-0005
DRAWING NO. S0231
SCALE AS SHOWN
SHEET NO. 222 OF 245

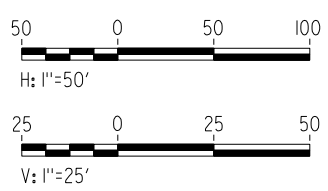


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'


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dequzmmf \$\$\$\$\$\$SYTIME\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556





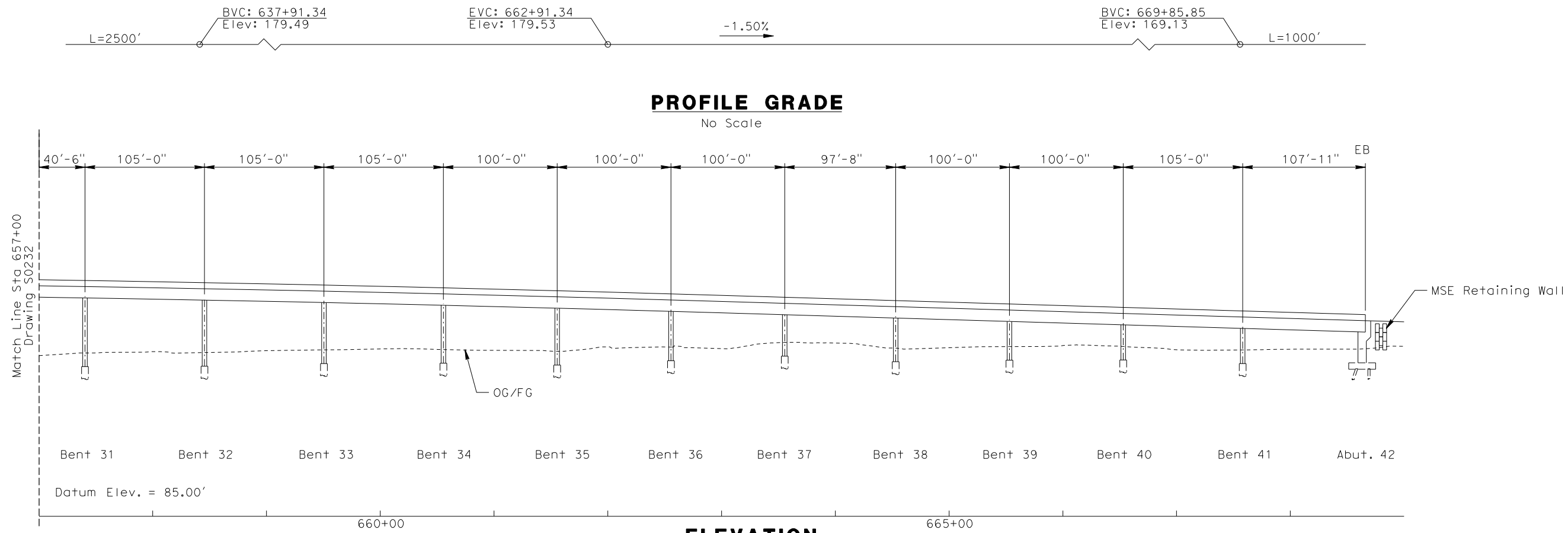
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

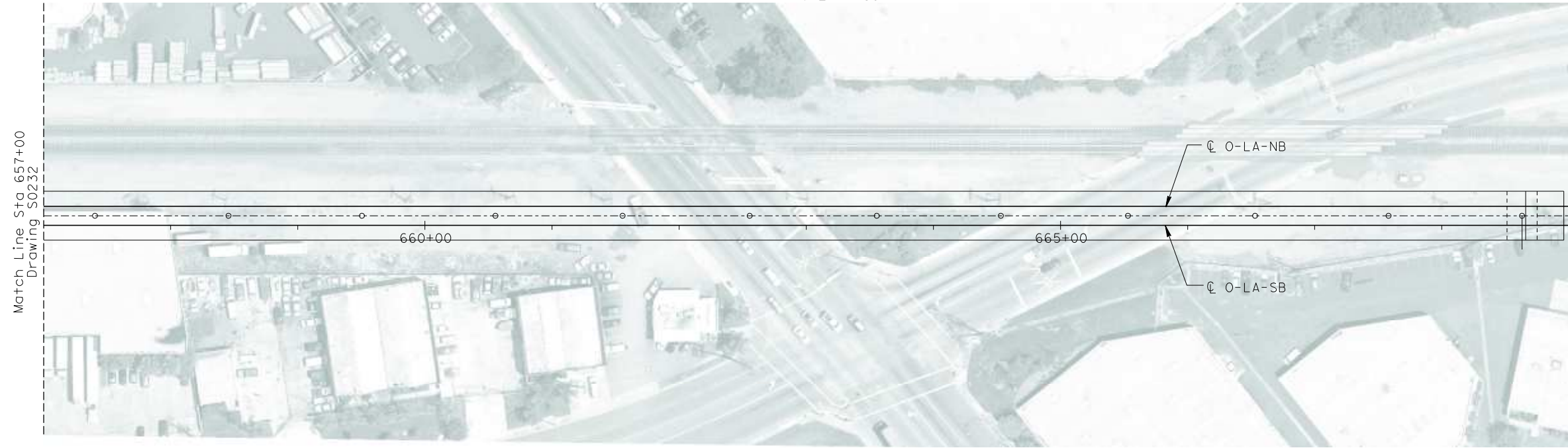
VIADUCT #4

CONTRACT NO. HSR06-0005
DRAWING NO. S0232
SCALE AS SHOWN
SHEET NO. 223 OF 245

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dequzmmf \$\$\$\$\$\$SYTIME\$\$\$\$\$



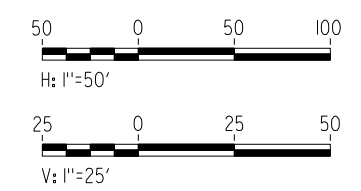
To Union Station



To Anaheim



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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

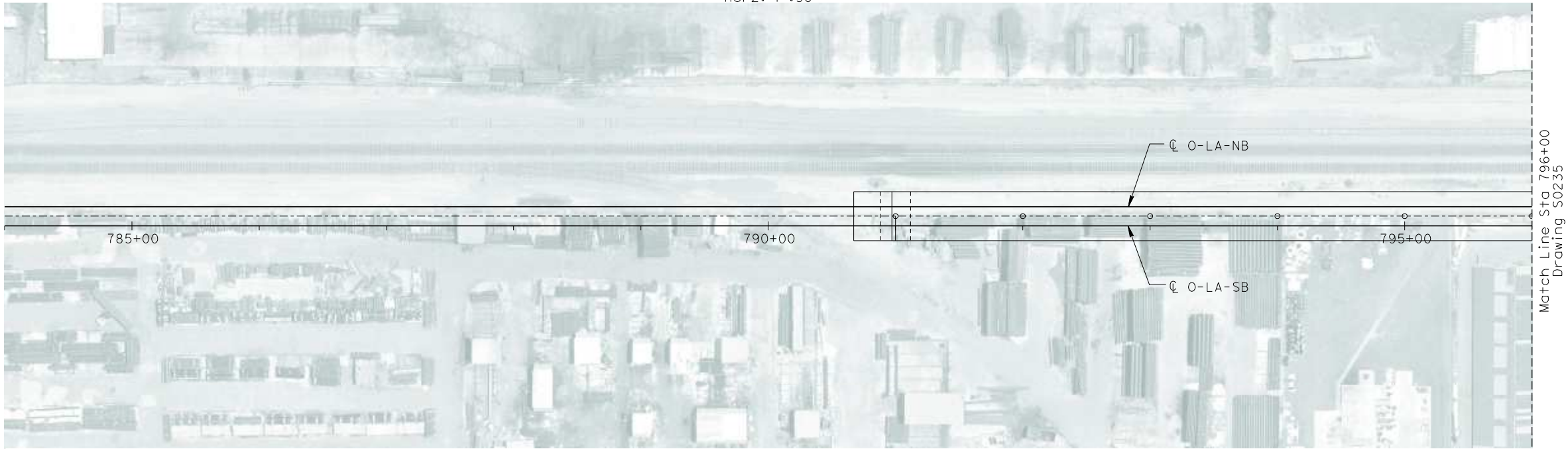
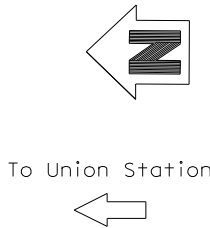
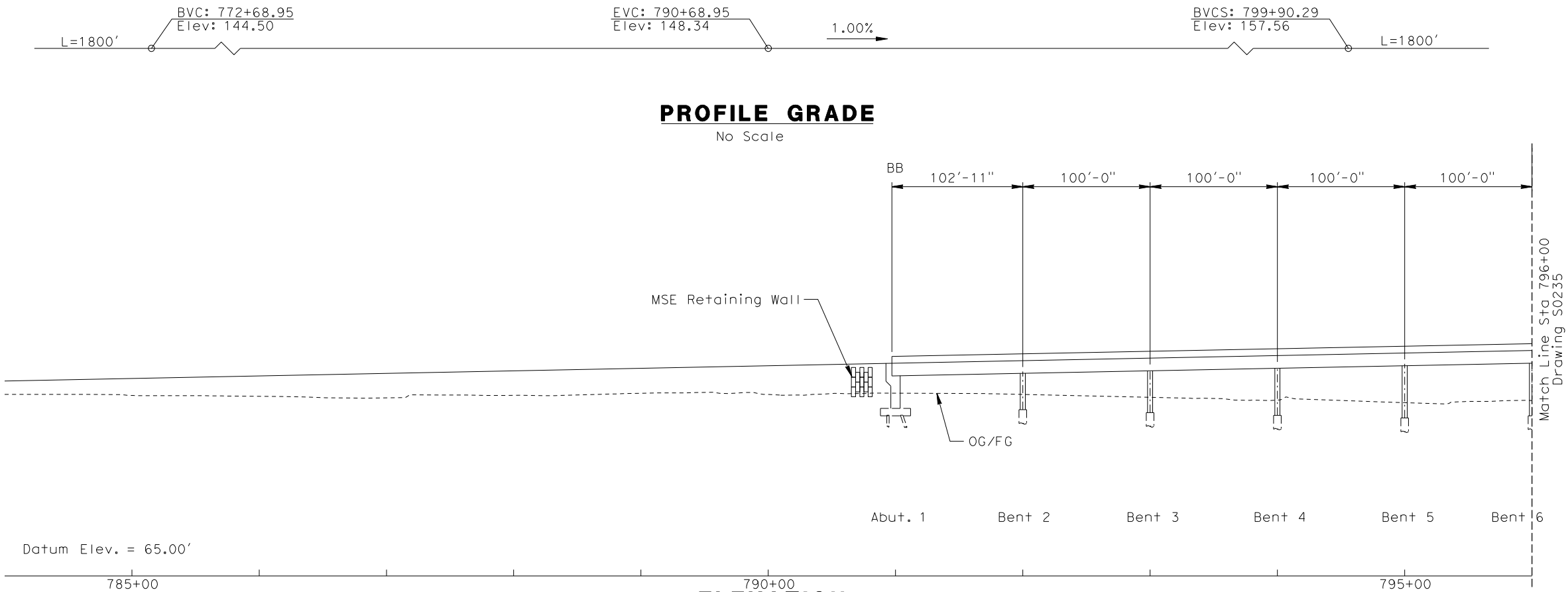
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
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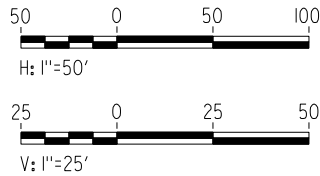
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #4

CONTRACT NO. HSR06-0005
DRAWING NO. S0233
SCALE AS SHOWN
SHEET NO. 224 OF 245



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06022011

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

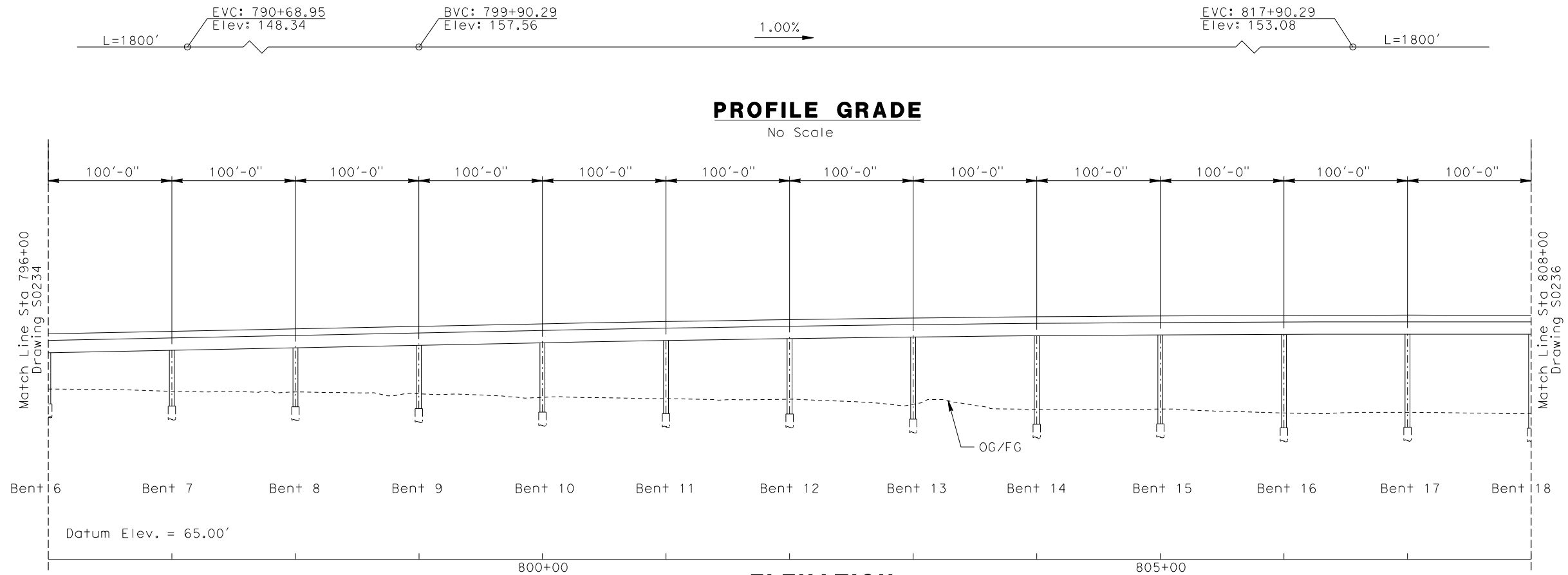


CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
Without ever leaving the ground.

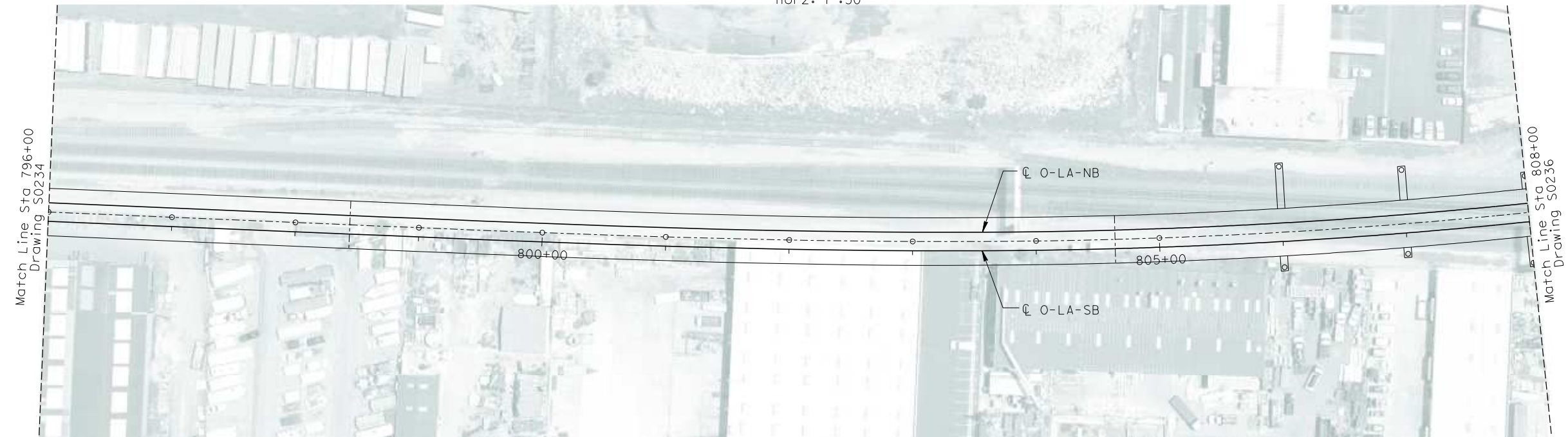
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #5

CONTRACT NO. HSR06-0005
DRAWING NO. S0234
SCALE AS SHOWN
SHEET NO. 225 OF 245

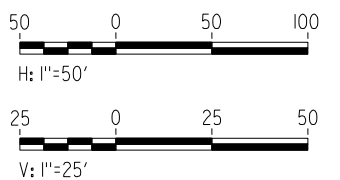


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

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020221.mxd

REV	DATE	BY	SUB	APP	DESCRIPTION

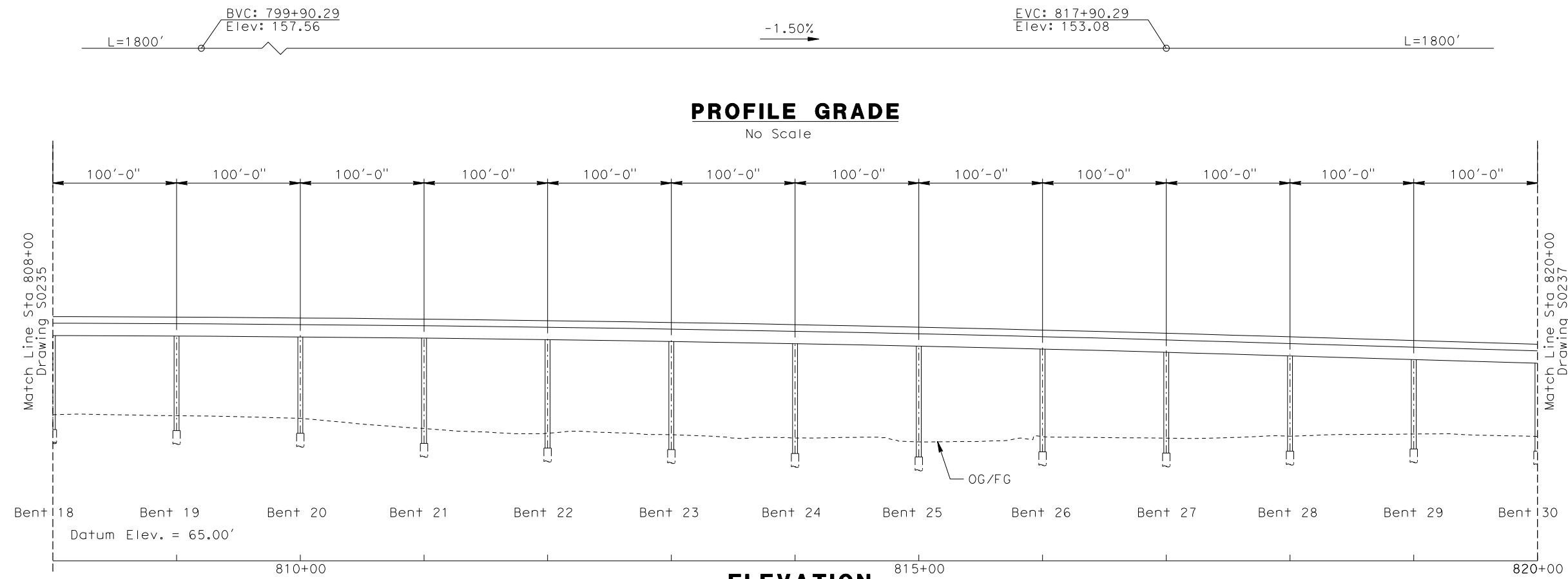
DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

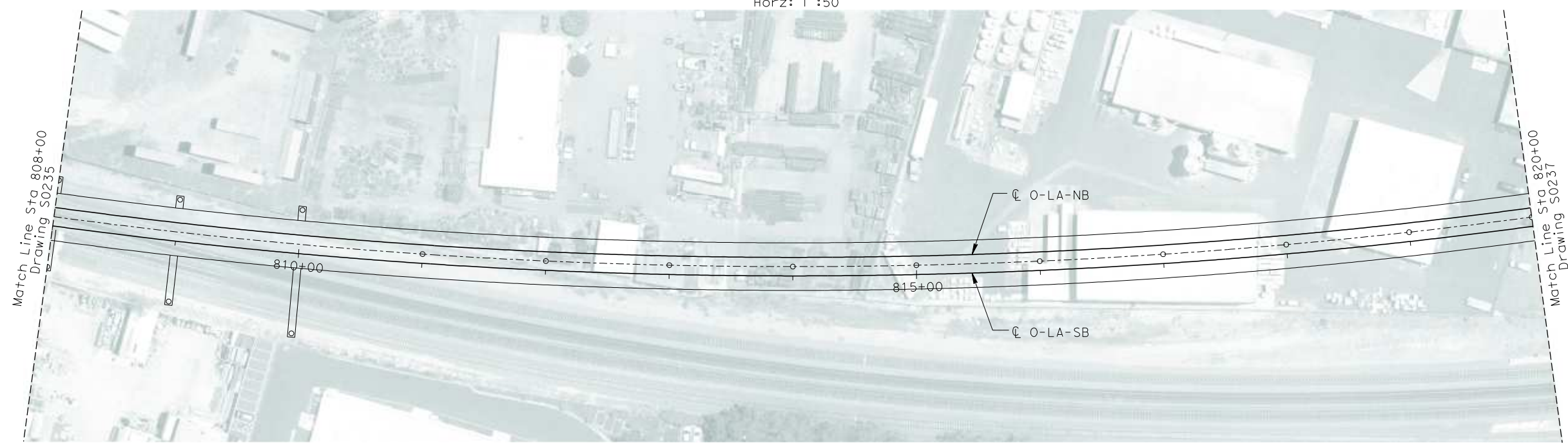
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #5

CONTRACT NO. HSR06-0005
DRAWING NO. S0235
SCALE AS SHOWN
SHEET NO. 226 OF 245

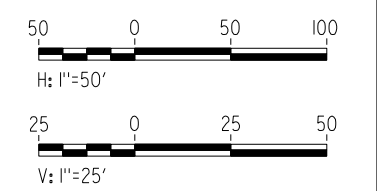


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

NOT FOR CONSTRUCTION
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02-13-09

REV	DATE	BY	SUB	APP	DESCRIPTION

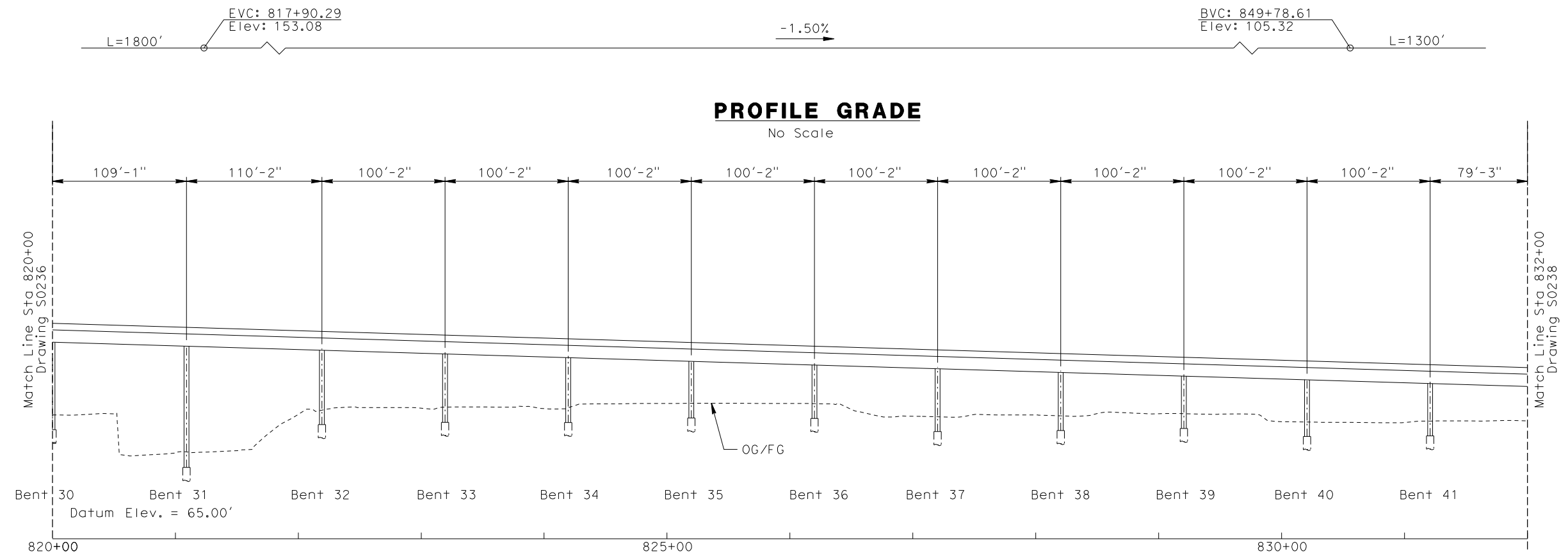
DESIGNED BY
W. H-H CHU
DRAWN BY
M.F. DE GUZMAN
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

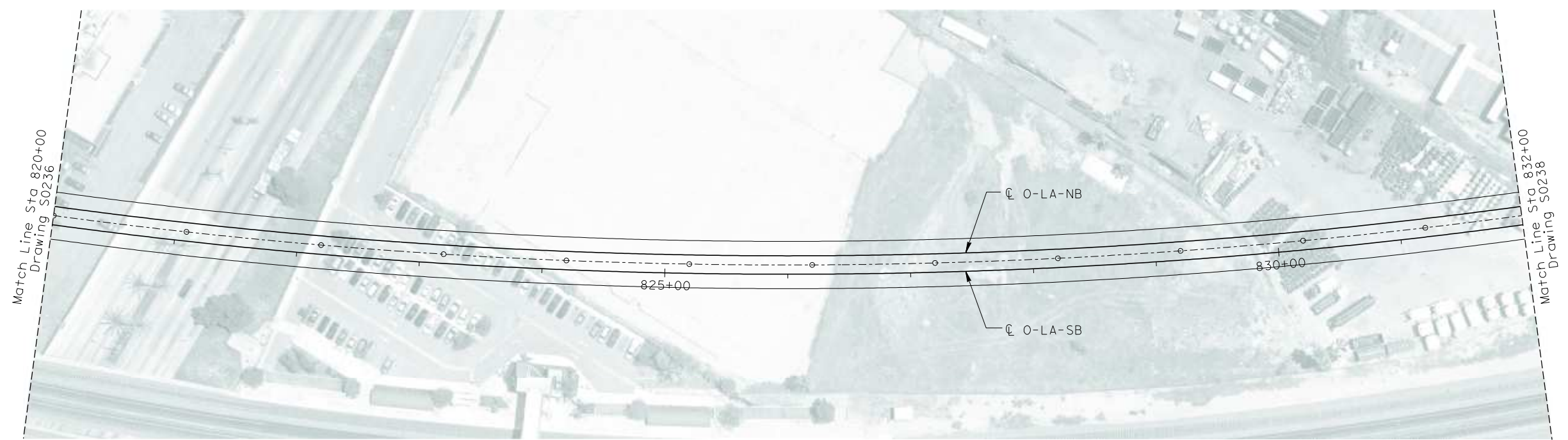


CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #5

CONTRACT NO.
HSR06-0005
DRAWING NO.
S0236
SCALE
AS SHOWN
SHEET NO.
227 OF 245



ELEVATION
Vert: 1"=25'
Horz: 1"=50'

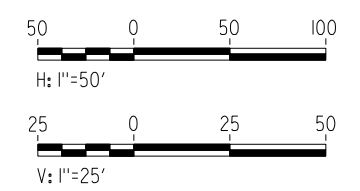


PLAN
1"=50'

To Union Station
←

To Anaheim
→


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REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



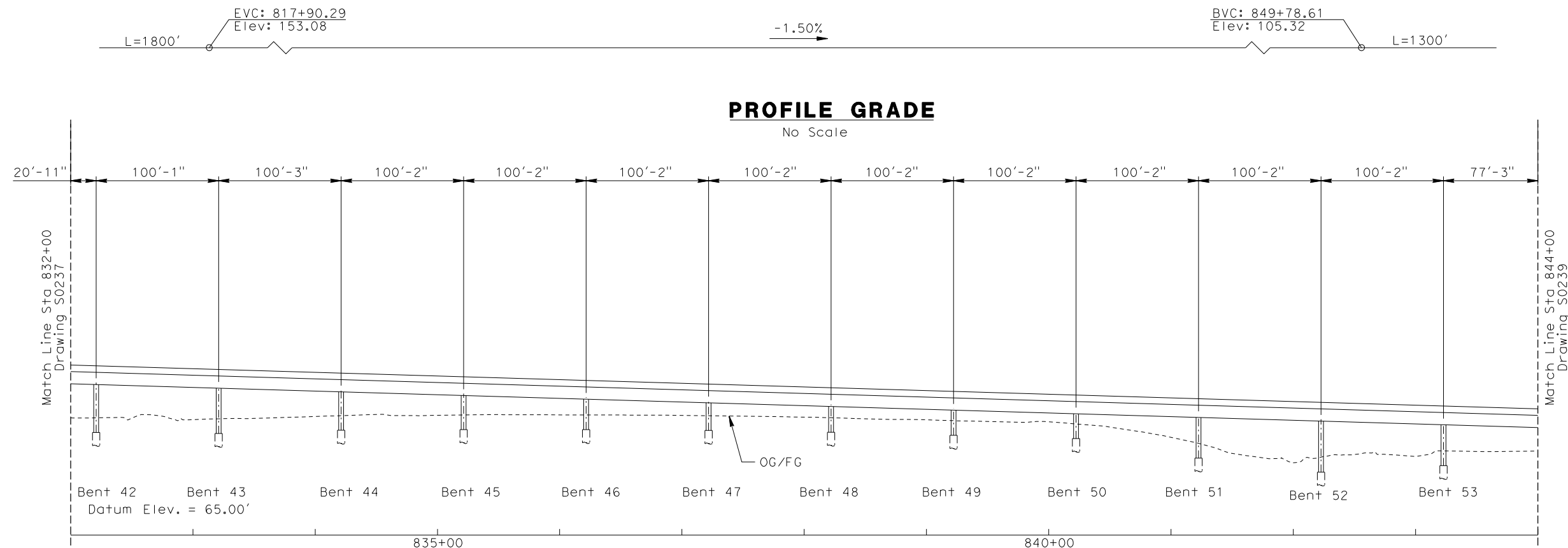


CALIFORNIA HIGH SPEED RAIL AUTHORITY
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

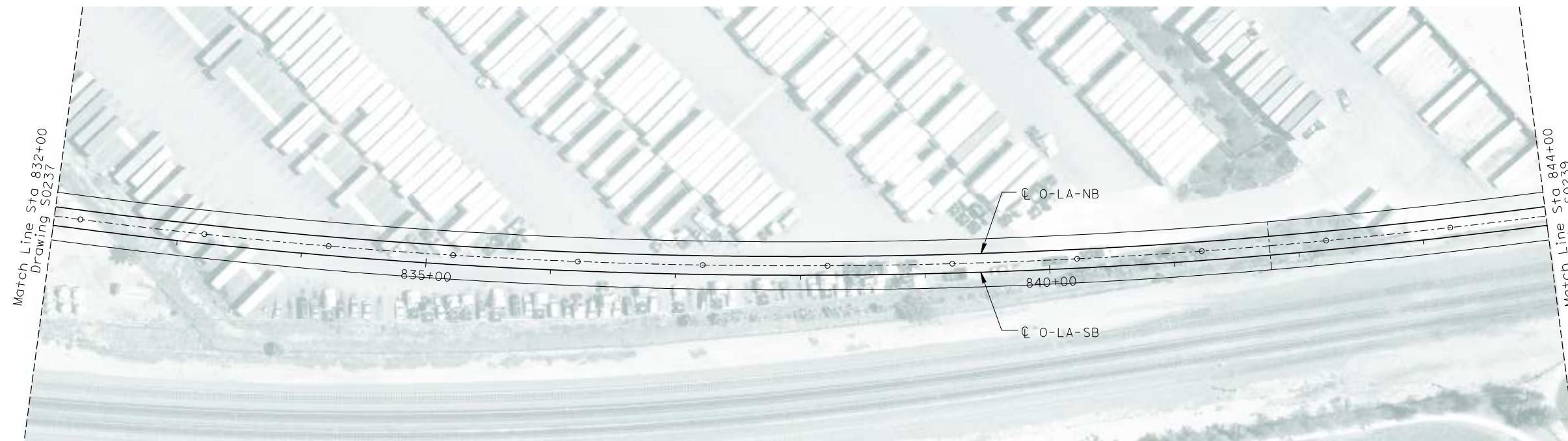
VIADUCT #5

CONTRACT NO. HSR06-0005
DRAWING NO. S0237
SCALE AS SHOWN
SHEET NO. 228 OF 245



ELEVATION

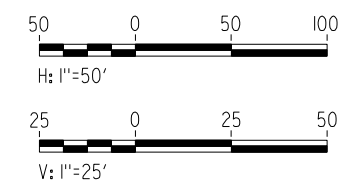
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PLAN

1"=50'


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060221.mxd \$\$\$\$\$\$SYTIME\$\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

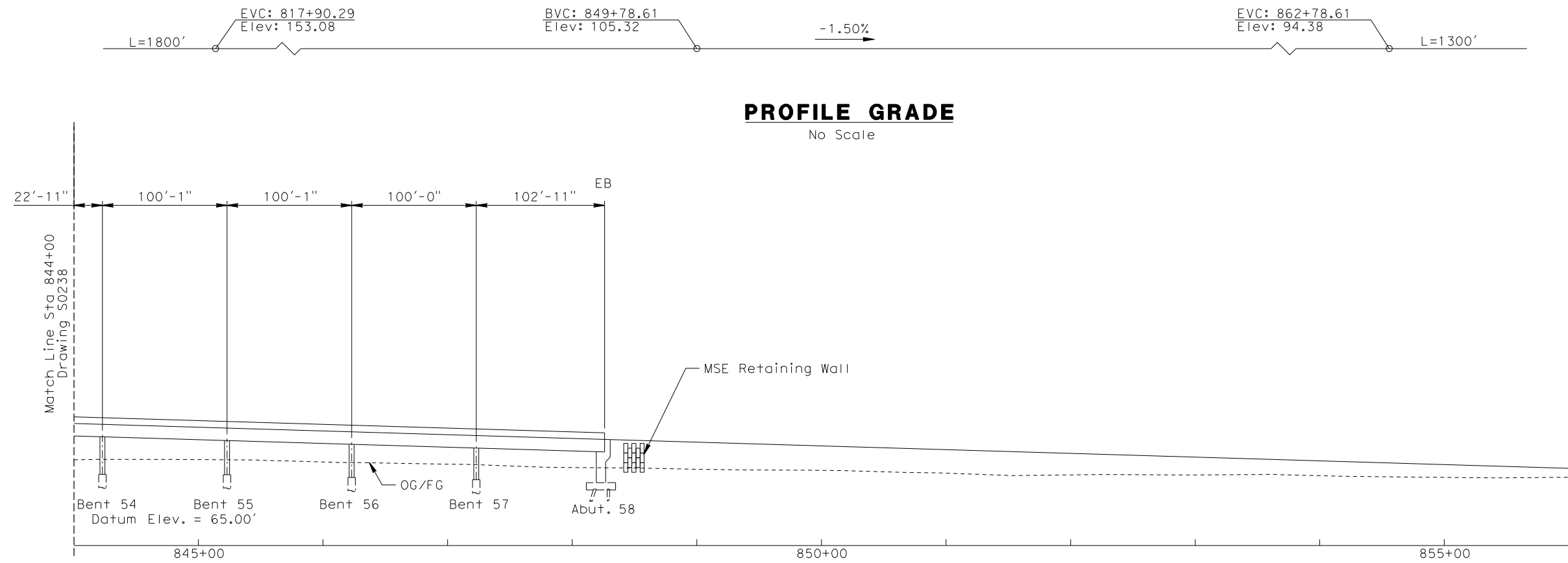
**STV Incorporated**
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



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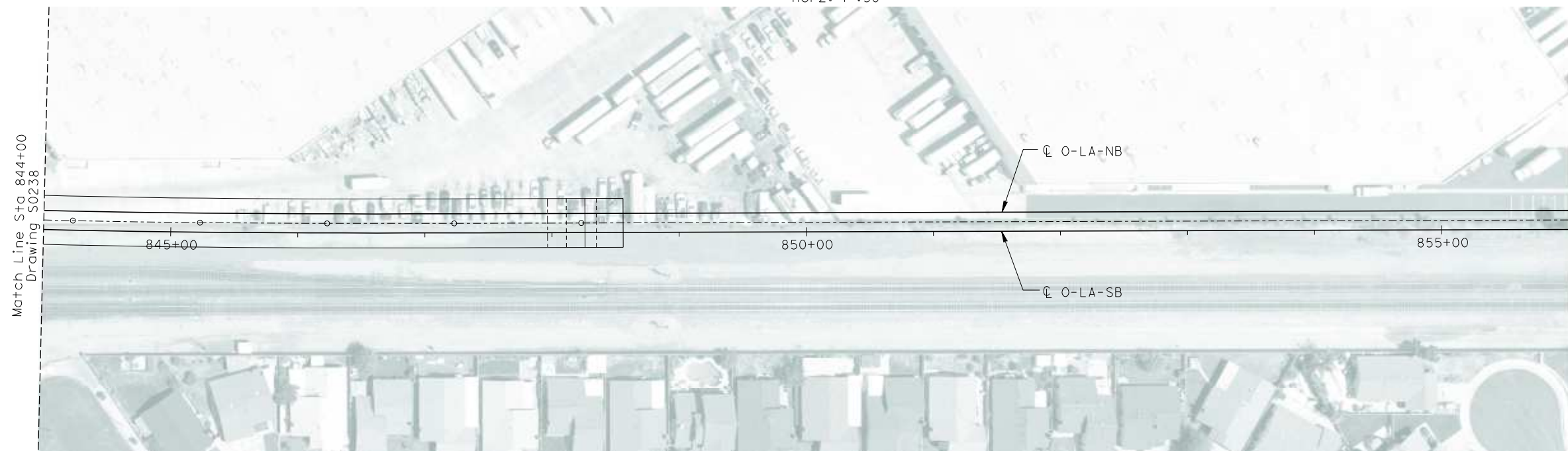
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #5

CONTRACT NO. HSR06-0005
DRAWING NO. S0238
SCALE AS SHOWN
SHEET NO. 229 OF 245



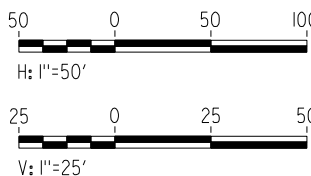
PROFILE GRADE
No Scale

ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

NOT FOR CONSTRUCTION
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dequzmmf \$\$\$\$\$\$SYTIME\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

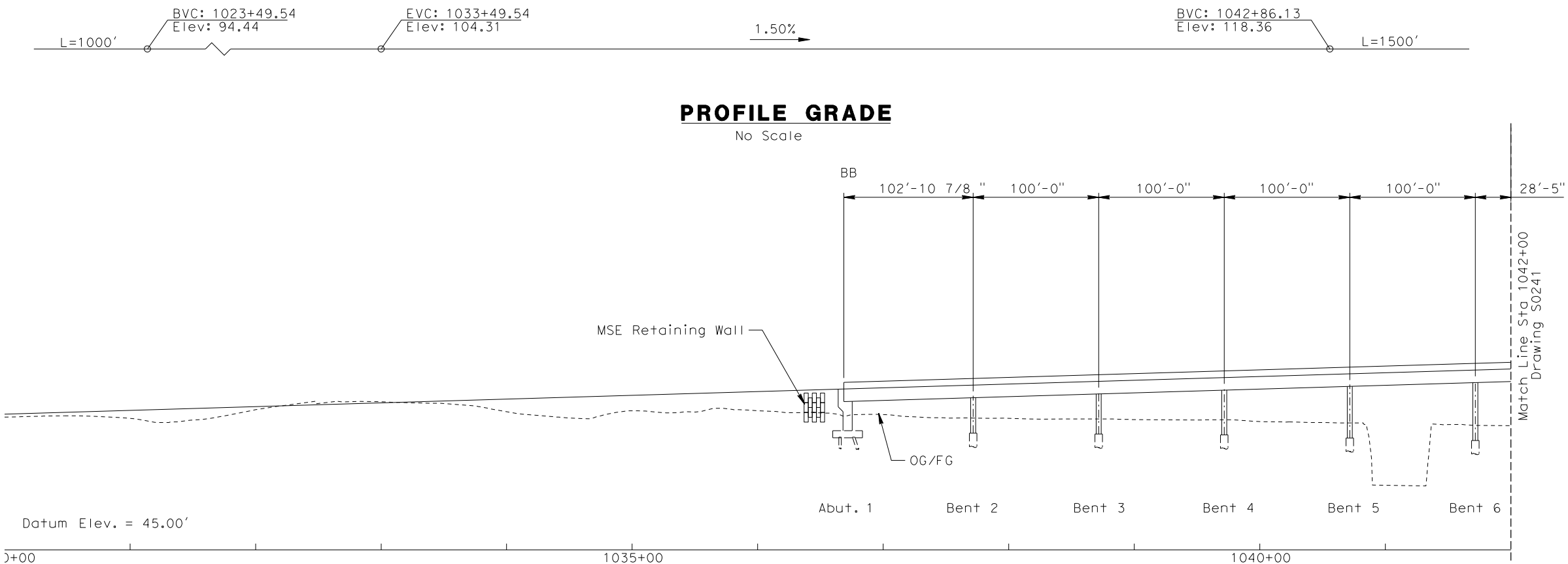
**STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
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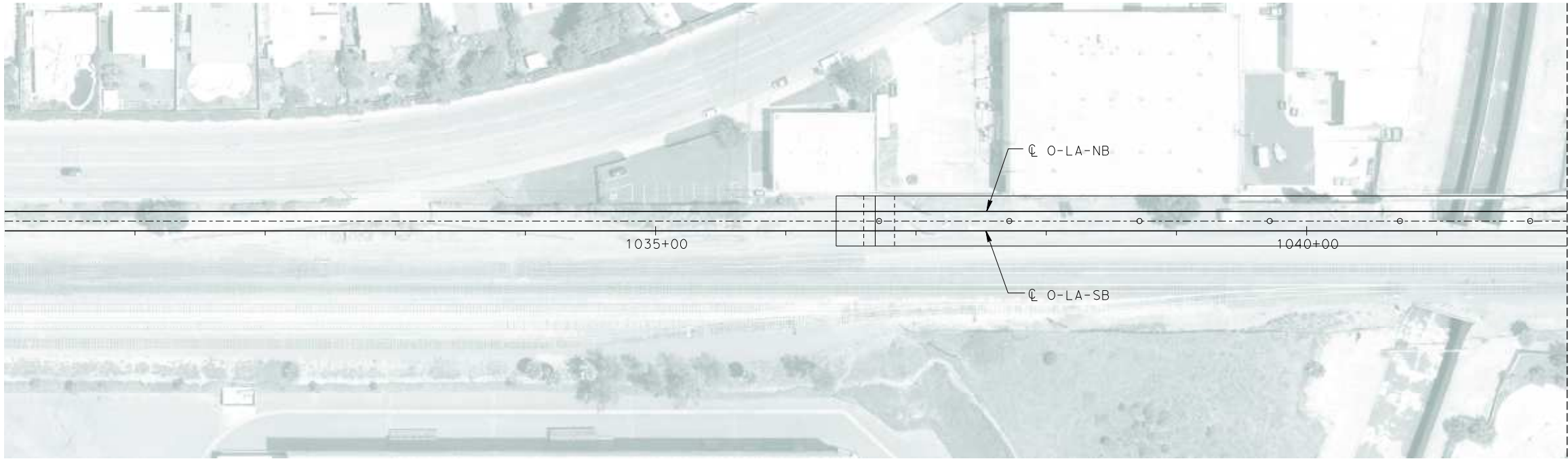
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #5

CONTRACT NO. HSR06-0005
DRAWING NO. S0239
SCALE AS SHOWN
SHEET NO. 230 OF 245

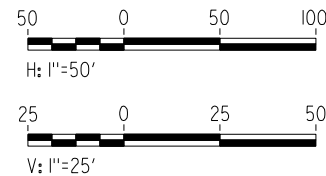


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'

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06022011

REV	DATE	BY	SUB	APP	DESCRIPTION

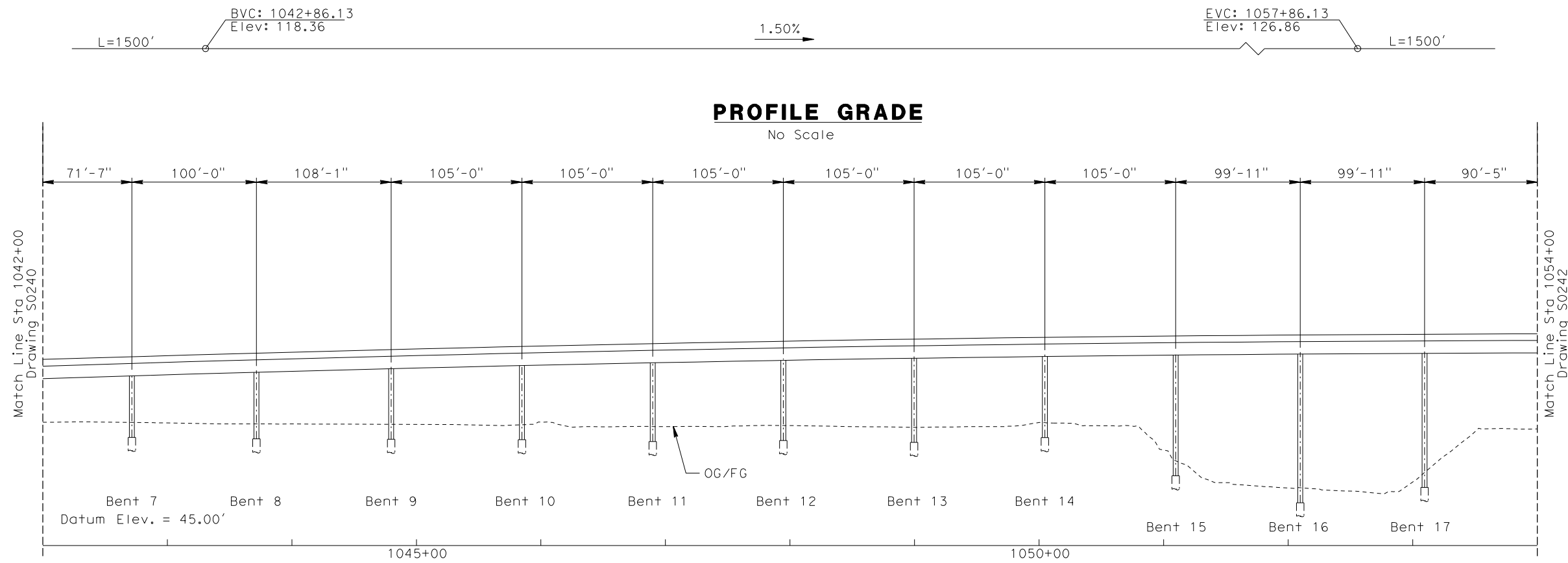
DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

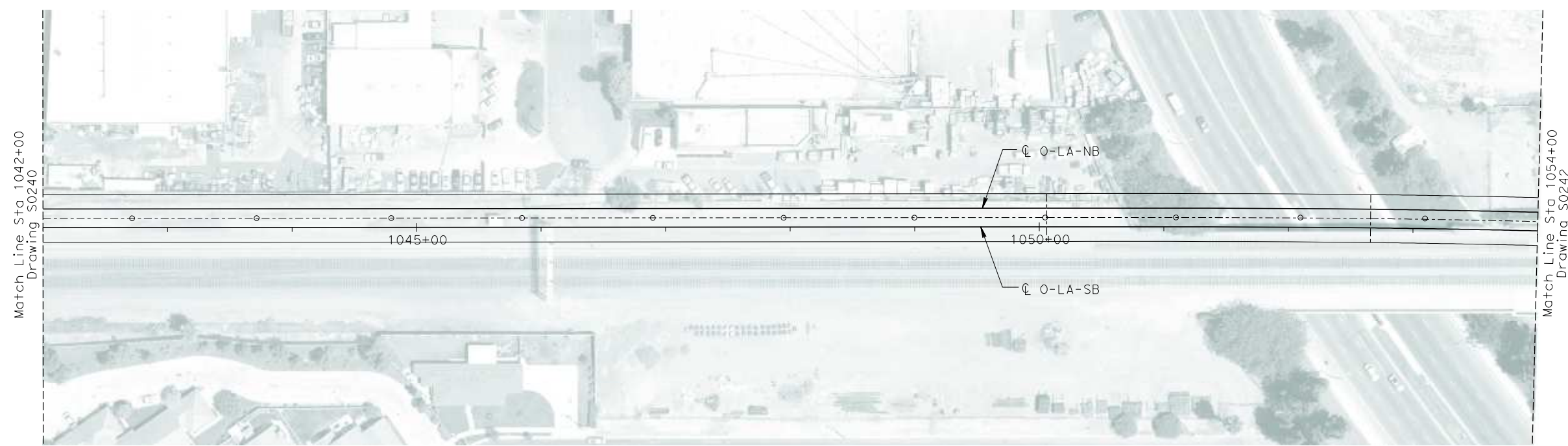
CALIFORNIA HIGH SPEED RAIL AUTHORITY
FLY CALIFORNIA
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #6

CONTRACT NO. HSR06-0005
DRAWING NO. S0240
SCALE AS SHOWN
SHEET NO. 231 OF 245



ELEVATION
Vert: 1"=25'
Horz: 1"=50'

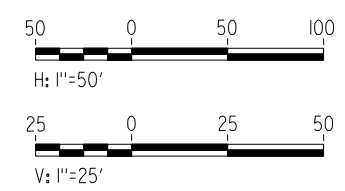


PLAN
1"=50'

To Union Station
←

To Anaheim
→


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\$\$\$\$\$SYTIME\$\$\$\$\$
dequzmmf

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY
W. H-H CHU
DRAWN BY
M.F. DE GUZMAN
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09

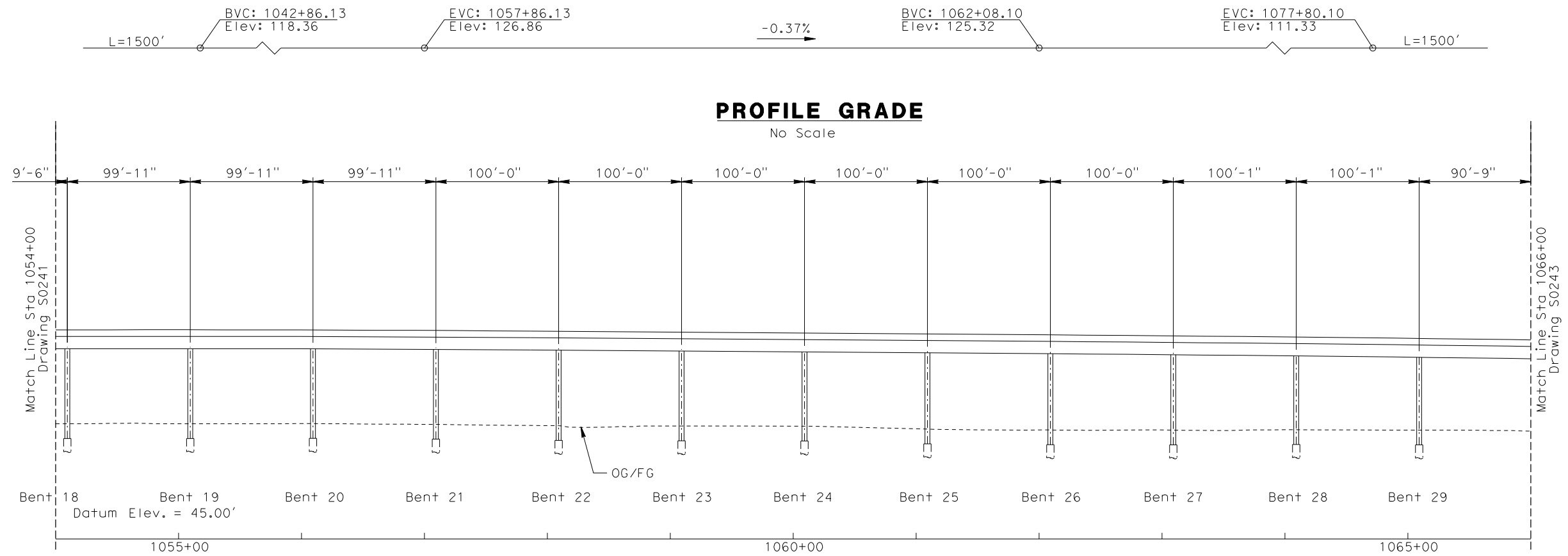
**STV Incorporated**
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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

**CALIFORNIA HIGH SPEED RAIL AUTHORITY**
FLY CALIFORNIA
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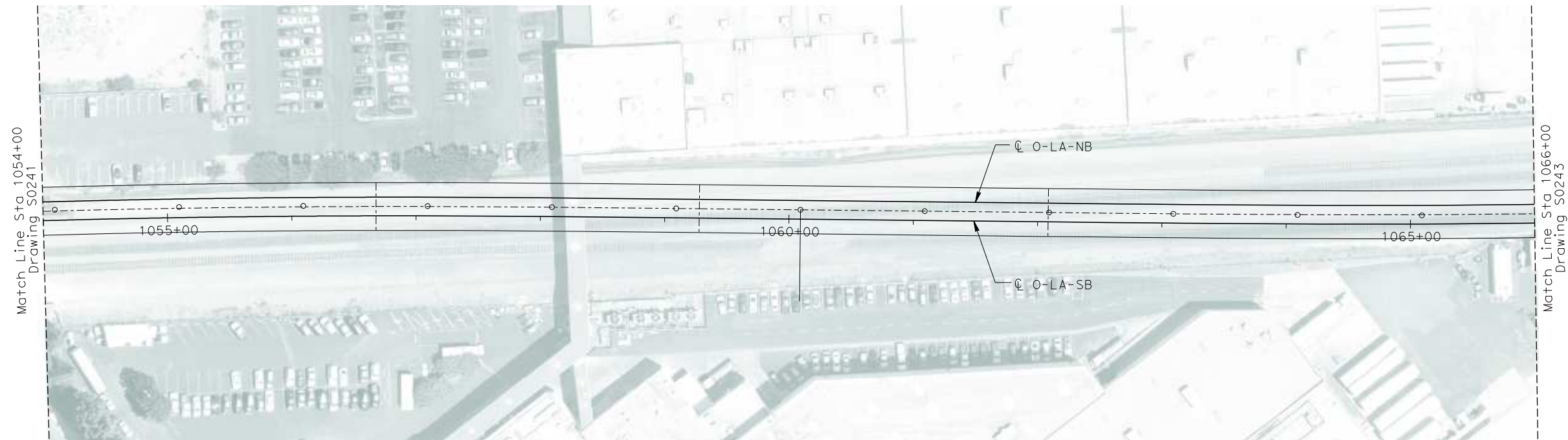
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #6

CONTRACT NO.	HSR06-0005
DRAWING NO.	S0241
SCALE	AS SHOWN
SHEET NO.	232 OF 245

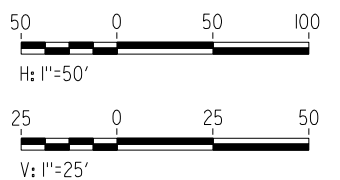


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'


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dequzmmf

REV	DATE	BY	SUB	APP	DESCRIPTION

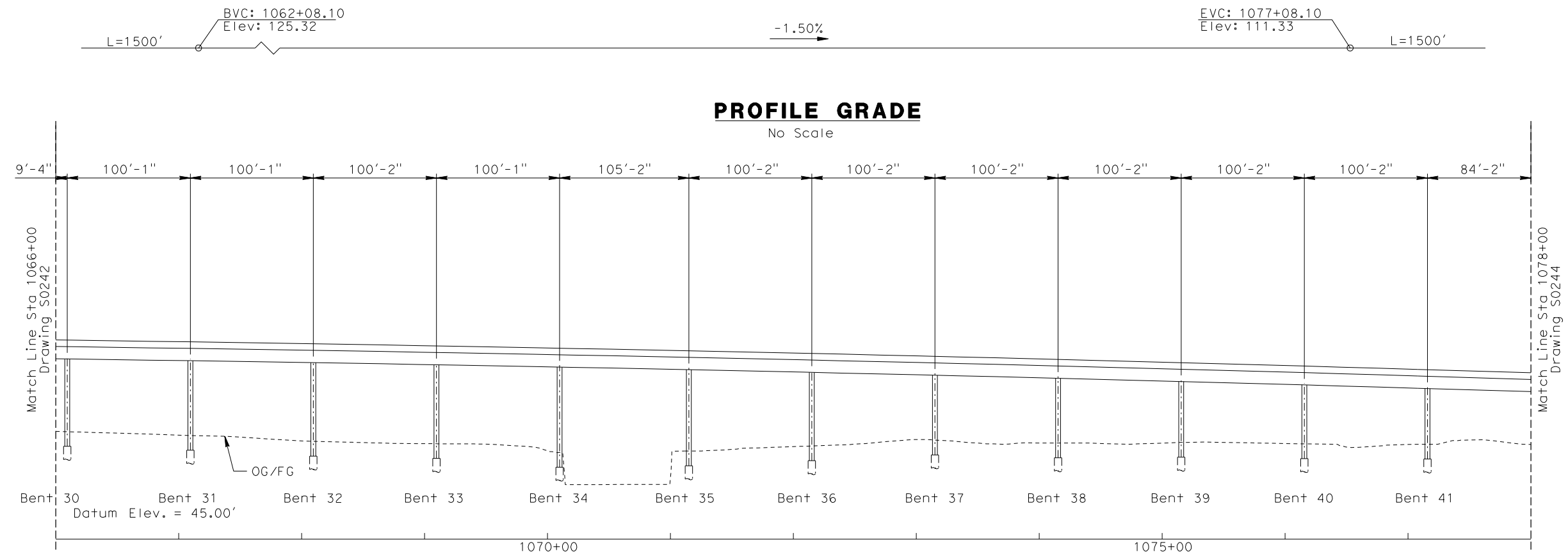
DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09

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ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #6

CONTRACT NO. HSR06-0005
DRAWING NO. S0242
SCALE AS SHOWN
SHEET NO. 233 OF 245

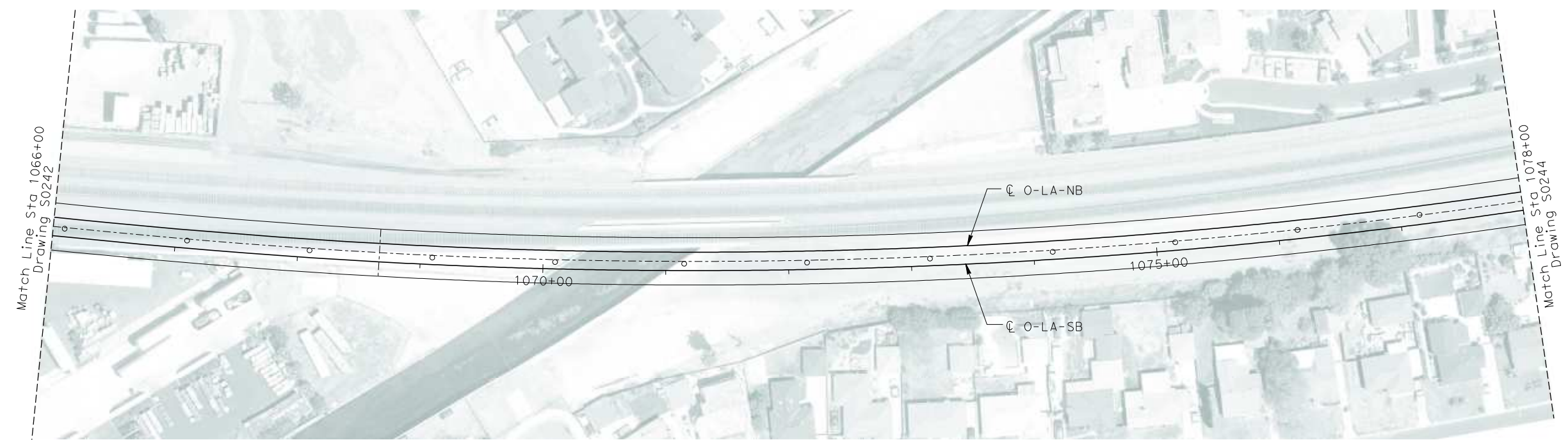


PROFILE GRADE

No Scale

ELEVATION

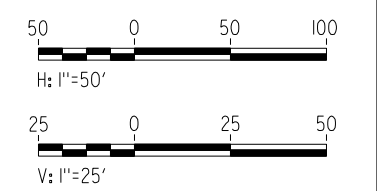
Vert: 1"=25'
Horz: 1"=50'



PLAN

1"=50'


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020421.mxd

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



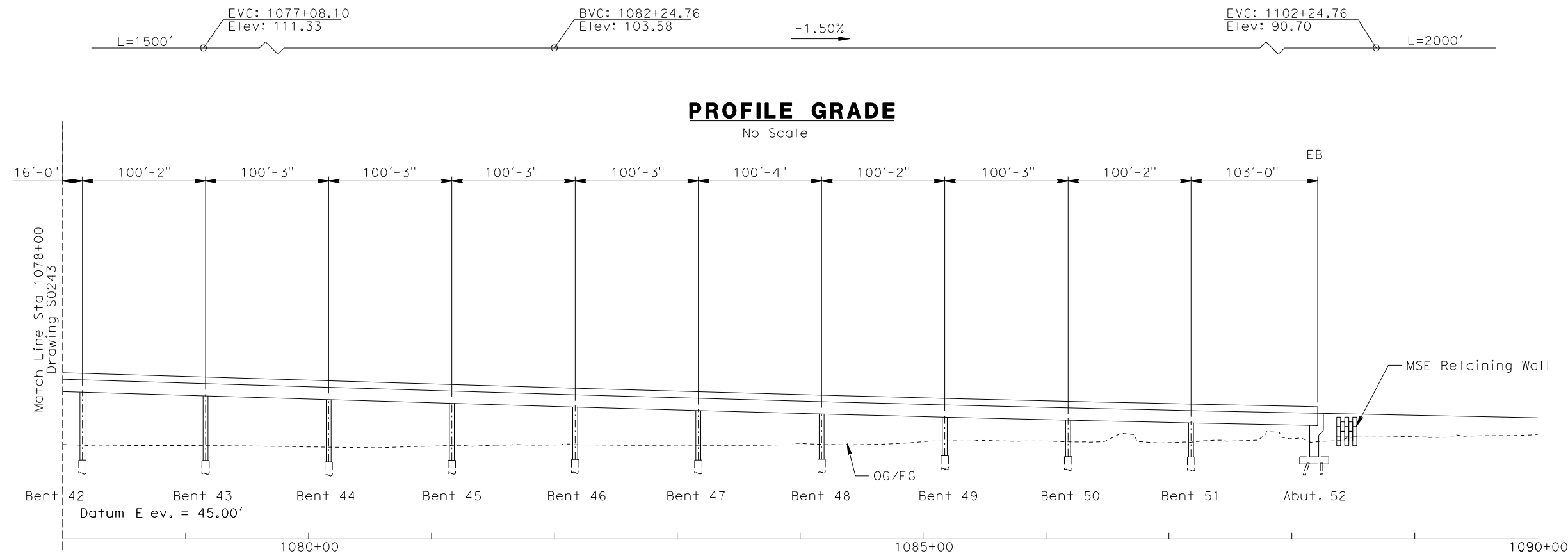
STV Incorporated
ENGINEERS/ARCHITECTS/PLANNERS/CONSTRUCTION MANAGERS
1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556



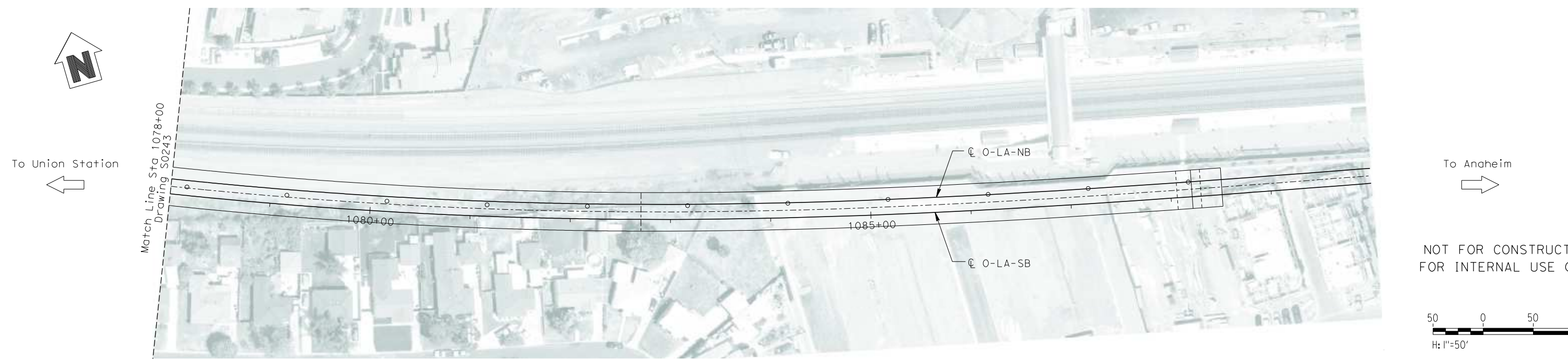
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

VIADUCT #6

CONTRACT NO. HSR06-0005
DRAWING NO. S0243
SCALE AS SHOWN
SHEET NO. 234 OF 245

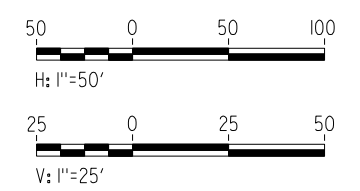


ELEVATION
Vert: 1"=25'
Horz: 1"=50'



PLAN
1"=50'


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dequzmmf

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
DRAWN BY M.F. DE GUZMAN
CHECKED BY A. LIU
IN CHARGE D. THOMSON
DATE 02-13-09



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1055 WEST SEVENTH STREET, SUITE 3150
LOS ANGELES, CA 90017-2556

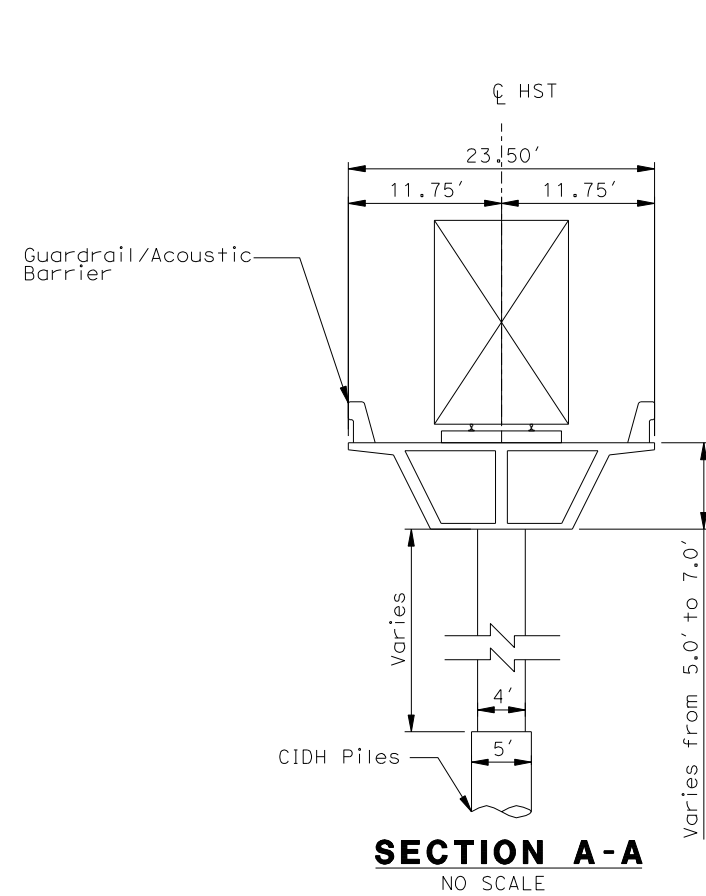




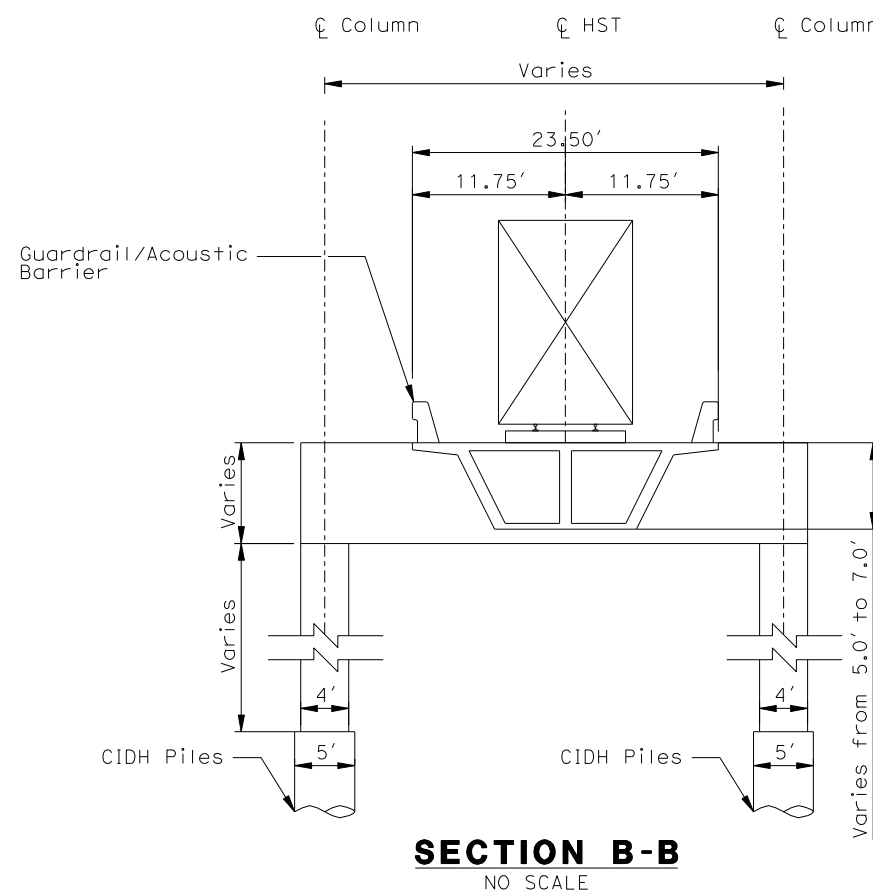
CALIFORNIA HIGH SPEED RAIL AUTHORITY
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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
VIADUCT #6

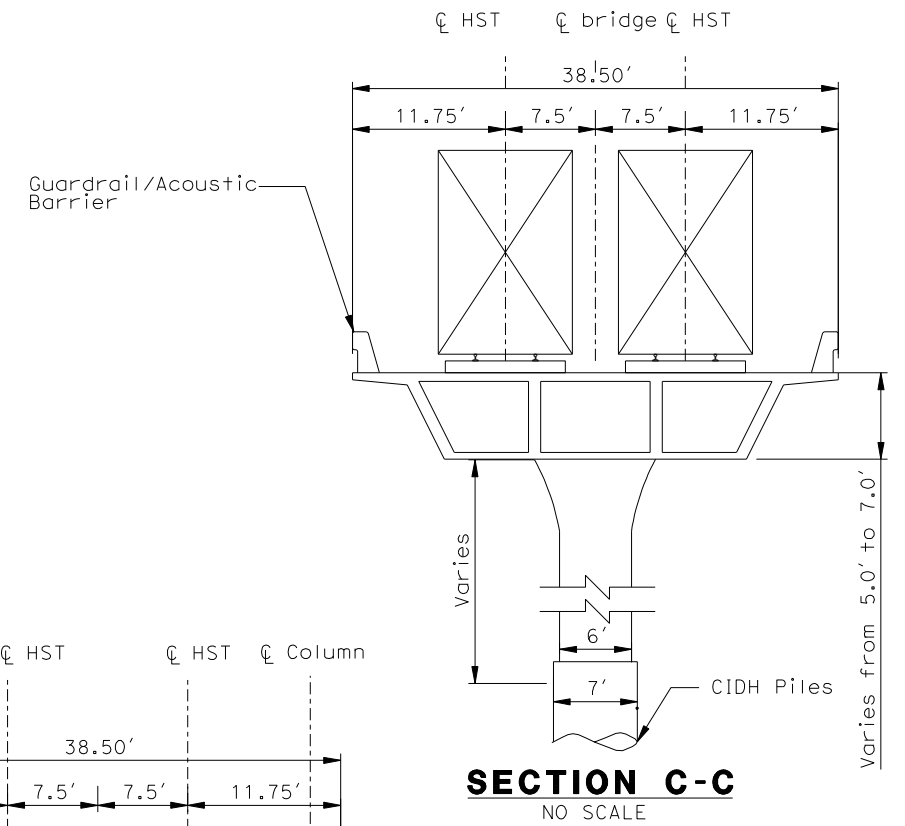
CONTRACT NO. HSR06-0005
DRAWING NO. S0244
SCALE AS SHOWN
SHEET NO. 235 OF 245



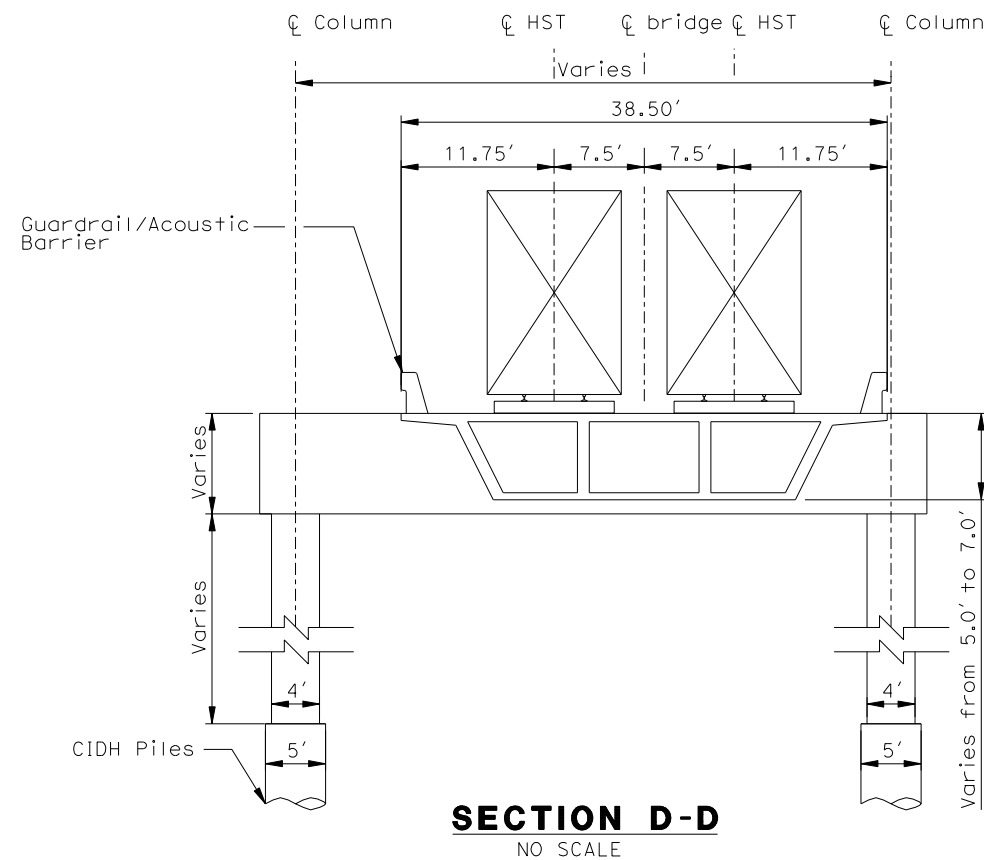
SECTION A-A
NO SCALE



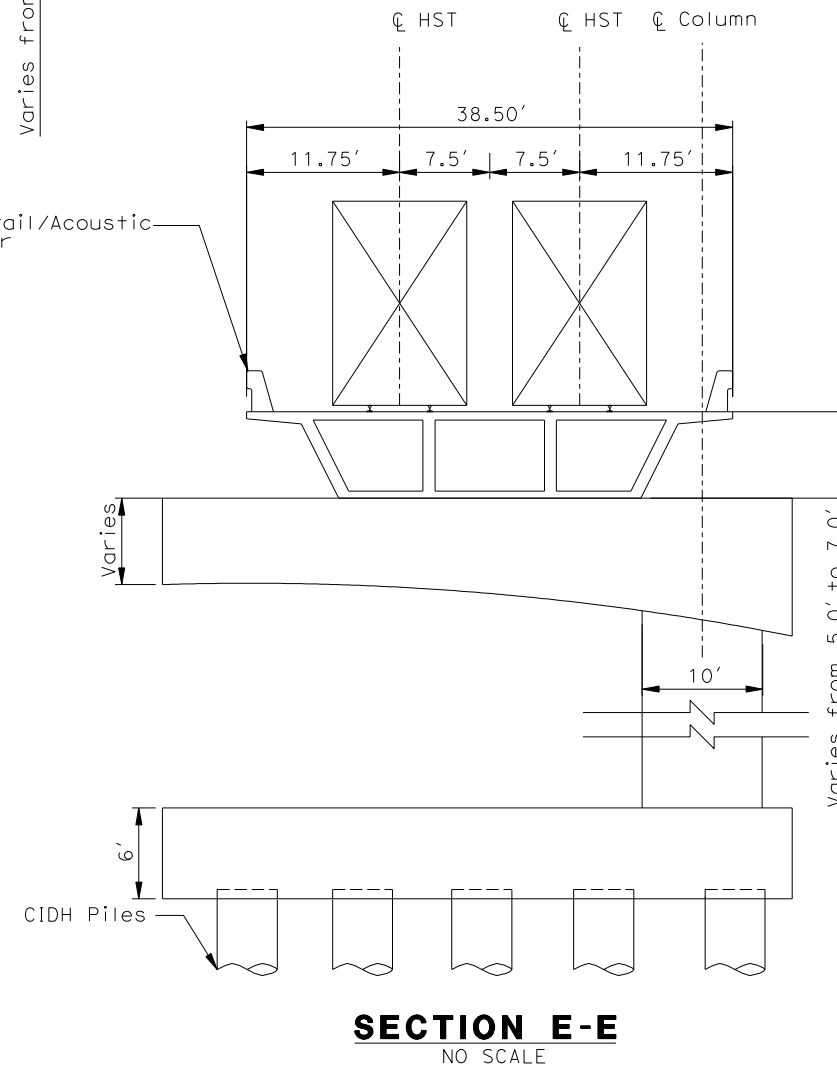
SECTION B-B
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SECTION C-C
NO SCALE



SECTION D-D
NO SCALE



SECTION E-E
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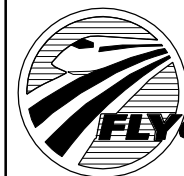
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[illegible]

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DRAWN BY	M.F. DE GUZMAN
CHECKED BY	A. LIU
IN CHARGE	D. THOMSON
DATE	02-13-09



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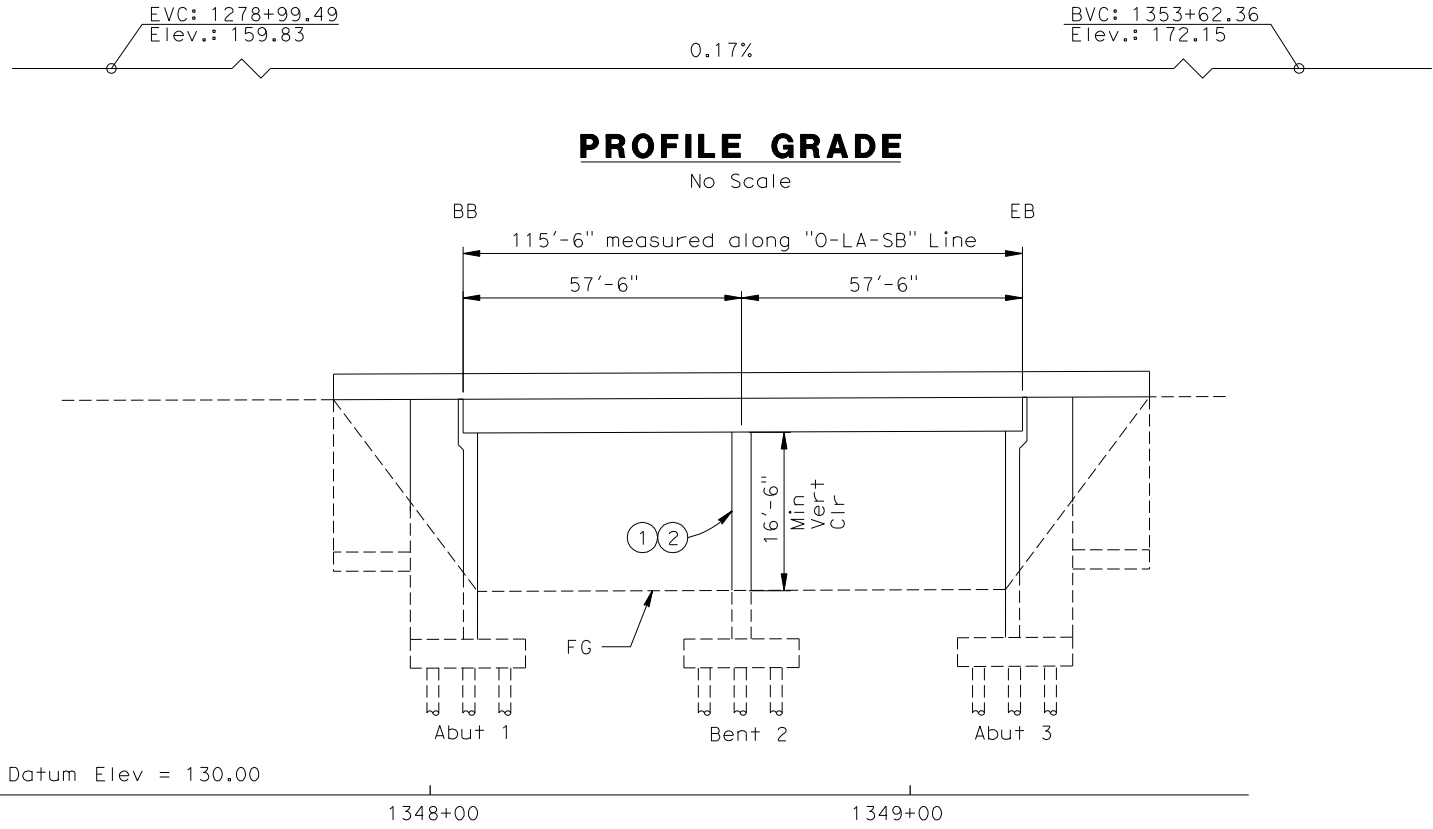
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

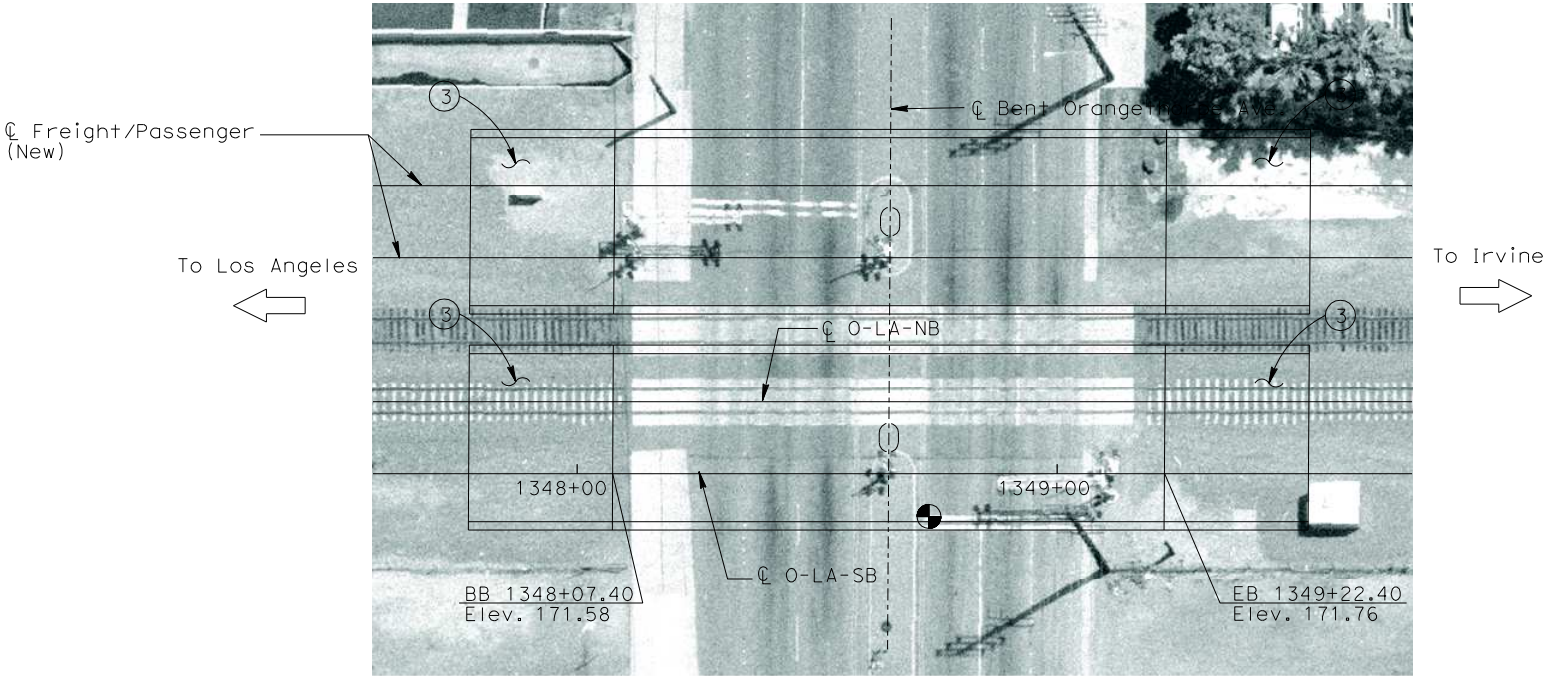
TYPICAL SECTIONS

CONTRACT NO.	HSR06-0005
DRAWING NO.	S0300
SCALE	NO SCALE
SHEET NO.	236 OF 245



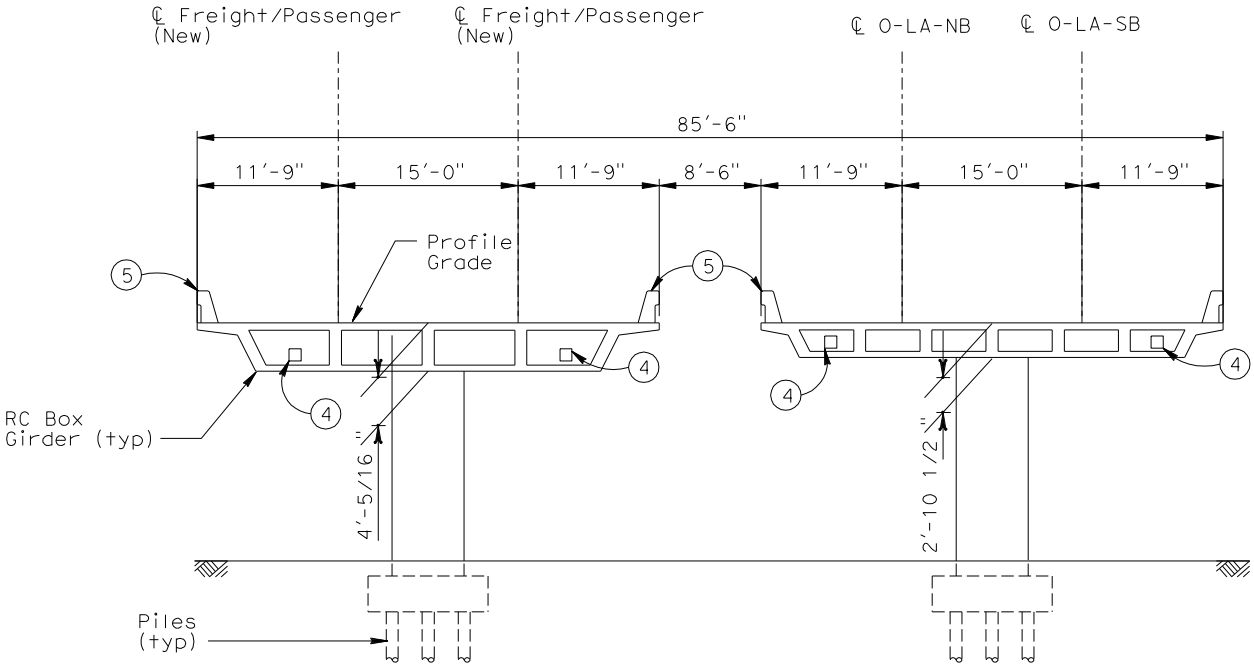
ELEVATION

Vert: 1":10'
Horz: 1":20'



PLAN

1":20'

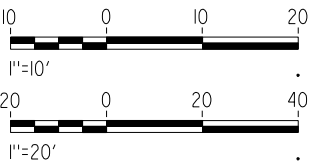


TYPICAL SECTION

1/8":1'

- NOTES
- ① PAINT "ORANGETHORPE AVE. UC"
 - ② PAINT "BRIDGE NO. 00-0000"
 - ③ STRUCTURE APPROACH SLAB
 - ④ FUTURE UTILITY OPENING
 - ⑤ GUARDRAIL/ACOUSTIC BARRIER
 - INDICATES POINT OF MINIMUM VERTICAL CLEARANCE


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\$\$\$\$\$DCN/SPEC\$\$\$\$\$
\$\$\$\$\$USER\$\$\$\$\$
\$\$\$\$\$SYTIME\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

DESIGNED BY W. H-H CHU
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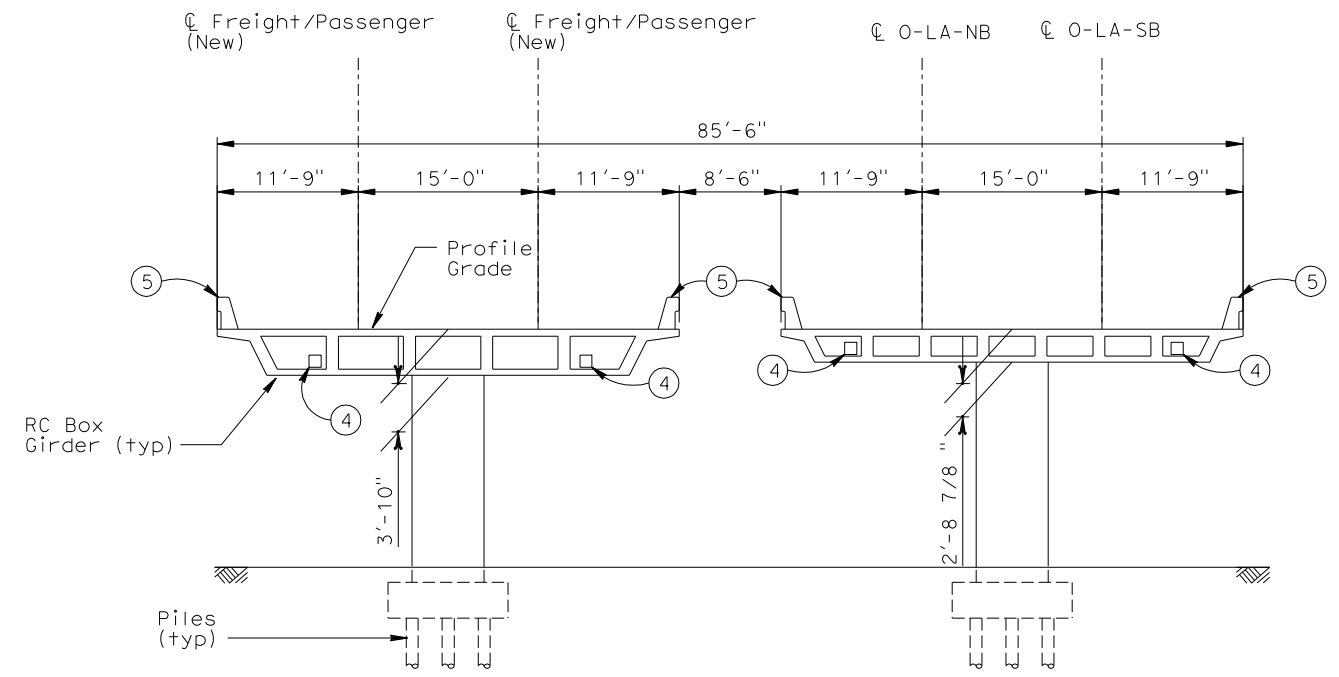
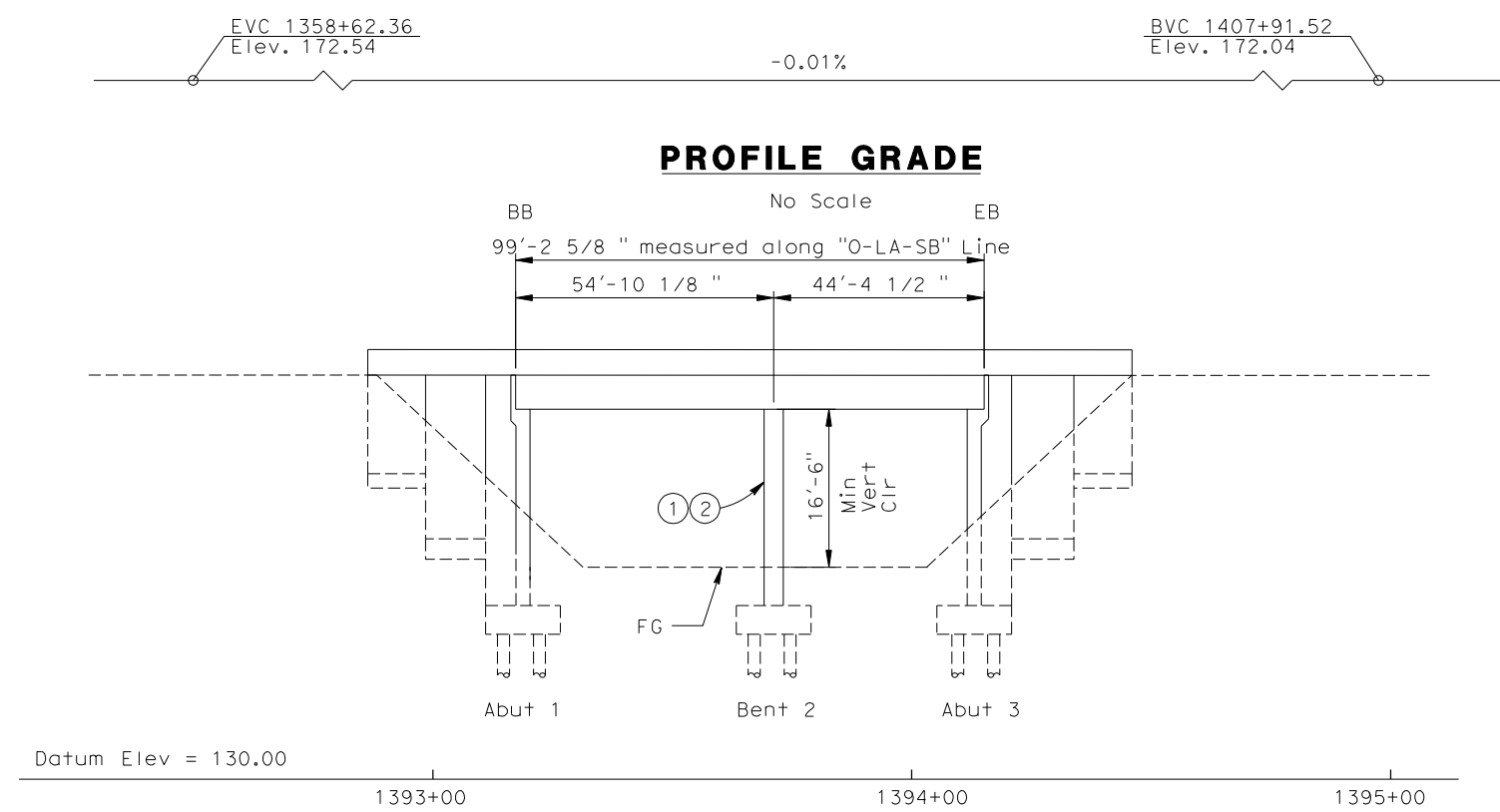
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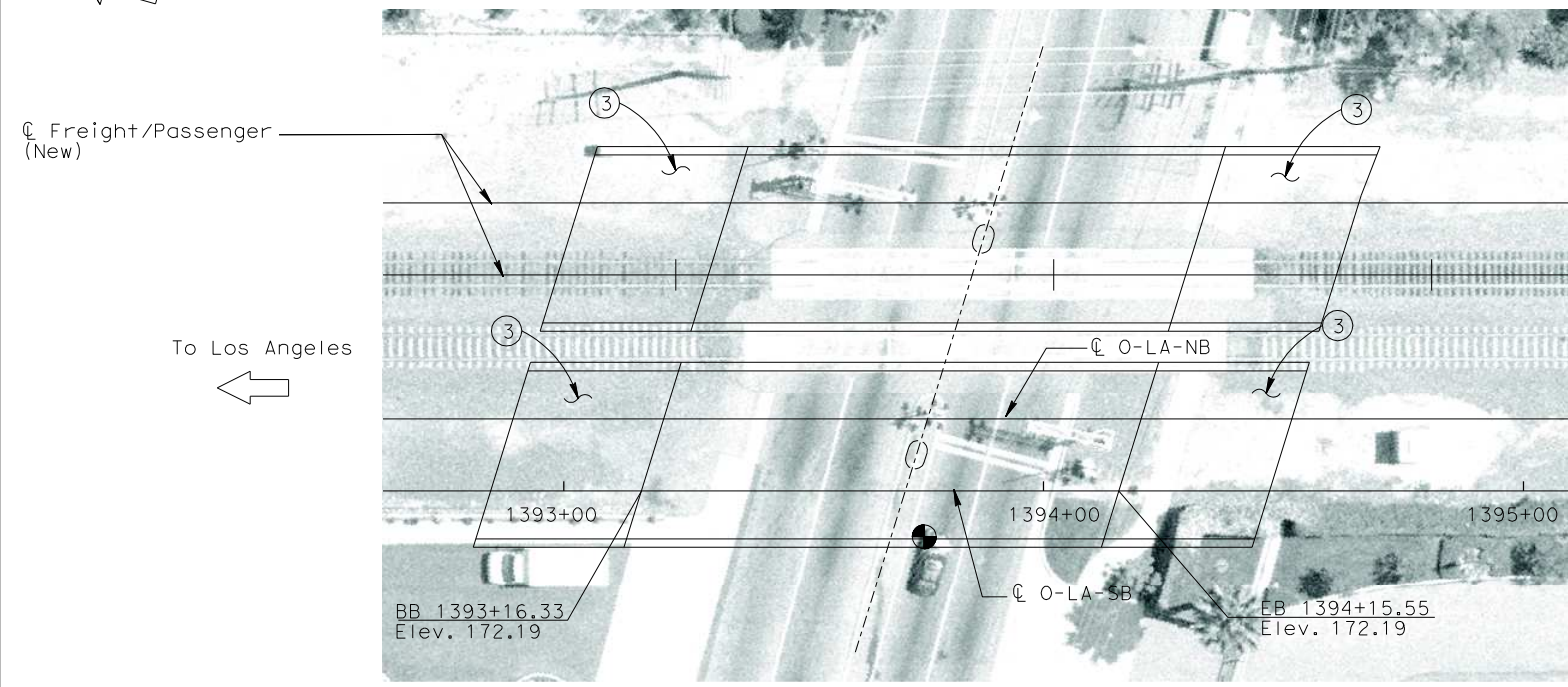
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

ORANGETHORPE AVENUE UNDERCROSSING

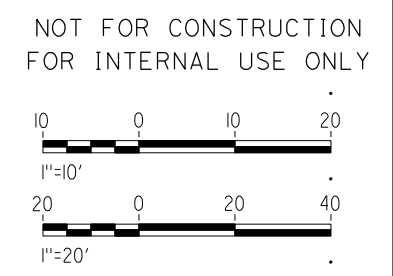
CONTRACT NO. HSR06-0005
DRAWING NO. S0400
SCALE AS SHOWN
SHEET NO. 237 OF 245



ELEVATION
Vert: 1"=10'
Horz: 1"=20'



- NOTES**
- ① PAINT "LA PALMA AVE. UC"
 - ② PAINT "BRIDGE NO. 00-0000"
 - ③ STRUCTURE APPROACH SLAB
 - ④ FUTURE UTILITY OPENING
 - ⑤ GUARDRAIL/ACOUSTIC BARRIER
 - INDICATES POINT OF MINIMUM VERTICAL CLEARANCE



\$\$\$\$\$DCN/SPEC\$\$\$\$\$SYTIME\$\$\$\$\$
\$\$\$\$\$USER\$\$\$\$\$

REV	DATE	BY	SUB	APP	DESCRIPTION

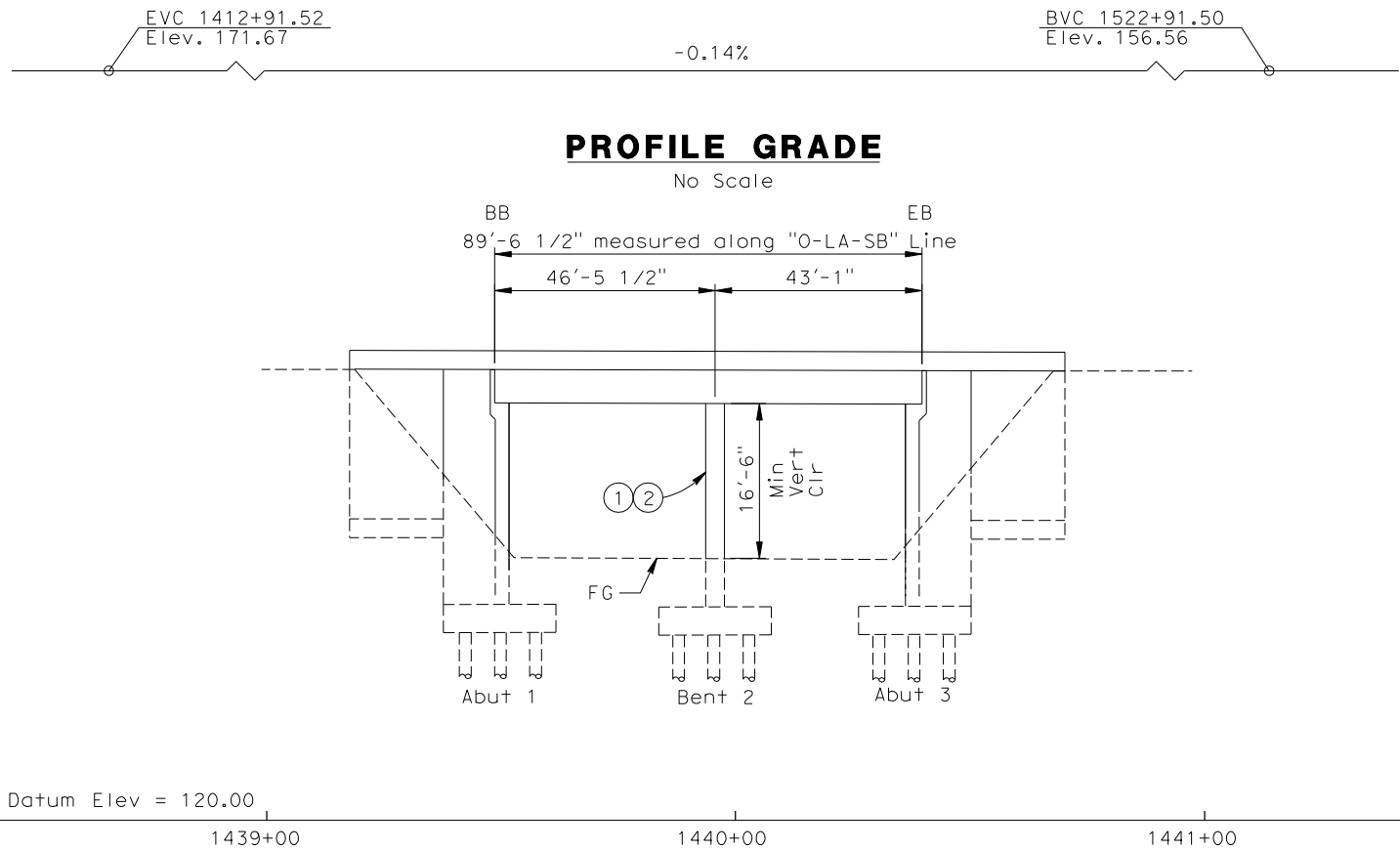
DESIGNED BY
W. H-H CHU
DRAWN BY
M.F. DE GUZMAN
CHECKED BY
A. LIU
IN CHARGE
D. THOMSON
DATE
02-13-09

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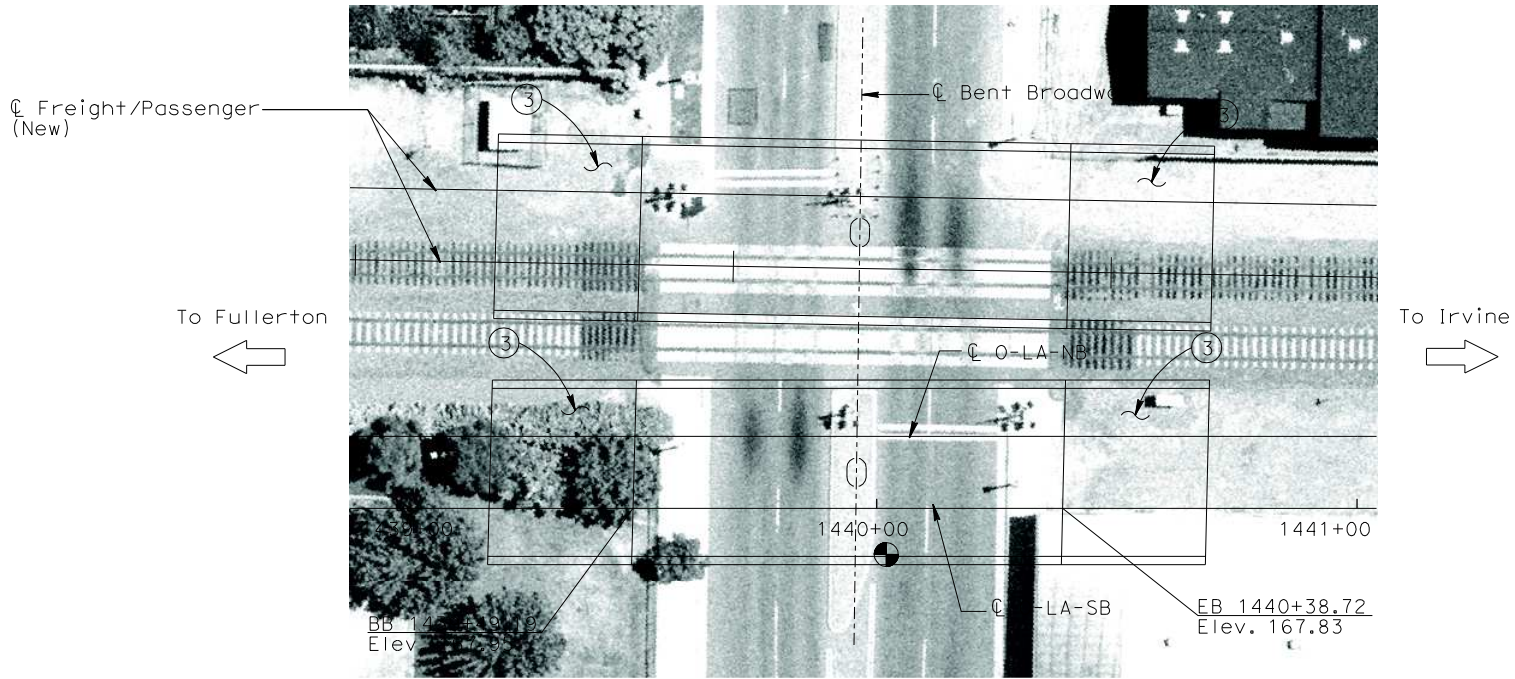
CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
LA PALMA AVENUE UNDERCROSSING

CONTRACT NO.
HSR06-0005
DRAWING NO.
S0401
SCALE
AS SHOWN
SHEET NO.
238 OF 245



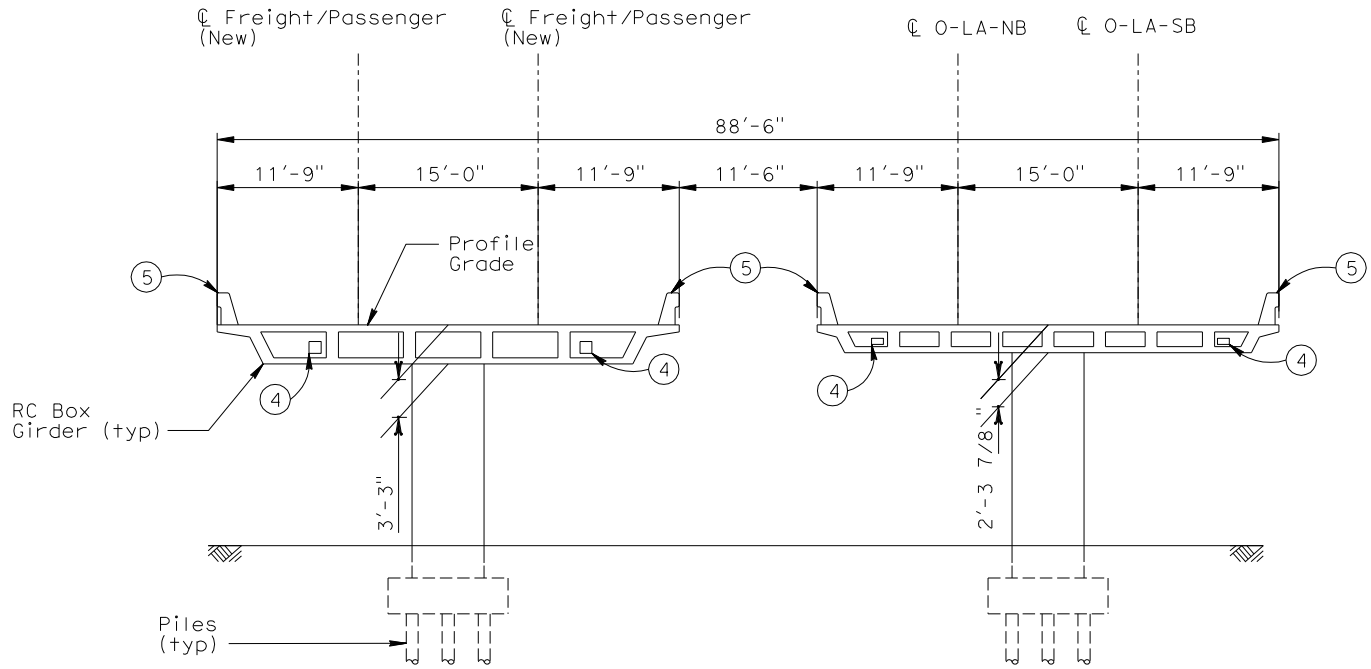
ELEVATION

Vert: 1":10'
Horz: 1":20'



PLAN

1":20'

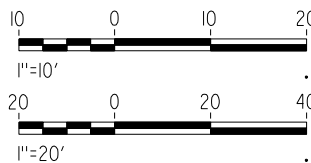


TYPICAL SECTION

1/8":1'


- NOTES
- ① PAINT "BROADWAY UC"
 - ② PAINT "BRIDGE NO. 00-0000"
 - ③ STRUCTURE APPROACH SLAB
 - ④ FUTURE UTILITY OPENING
 - ⑤ GUARDRAIL/ACOUSTIC BARRIER
 - INDICATES POINT OF MINIMUM VERTICAL CLEARANCE

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REV	DATE	BY	SUB	APP	DESCRIPTION

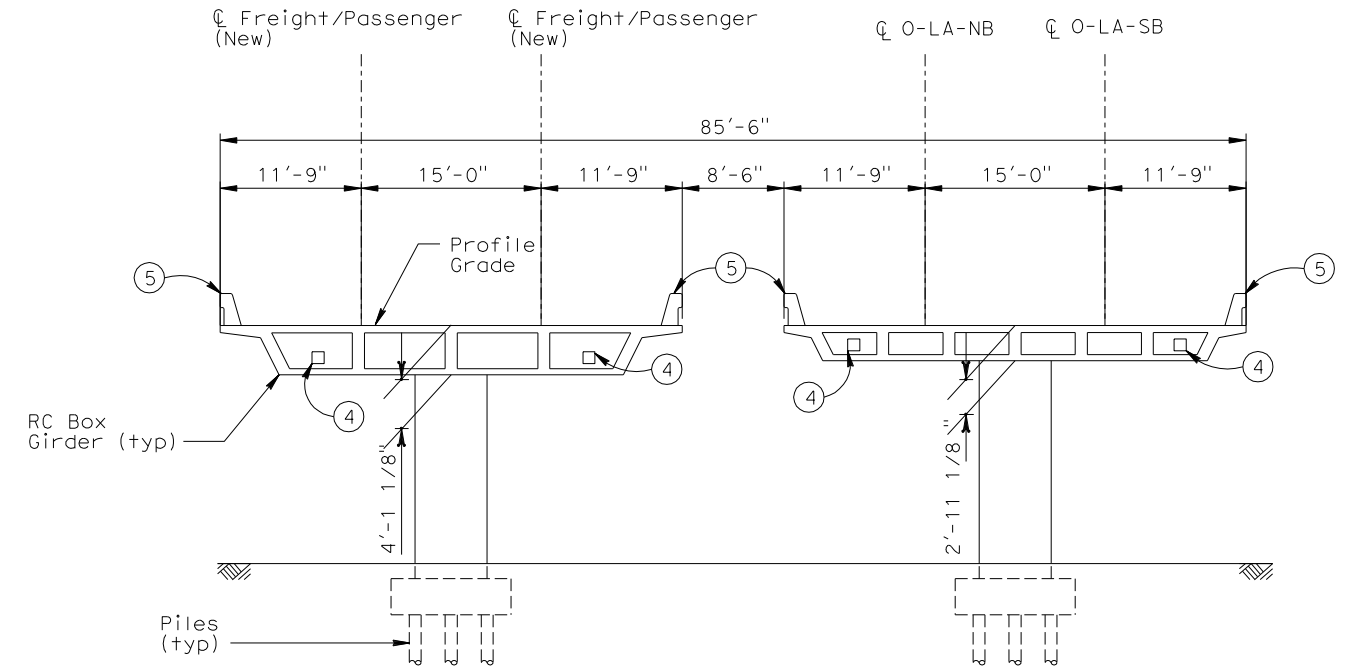
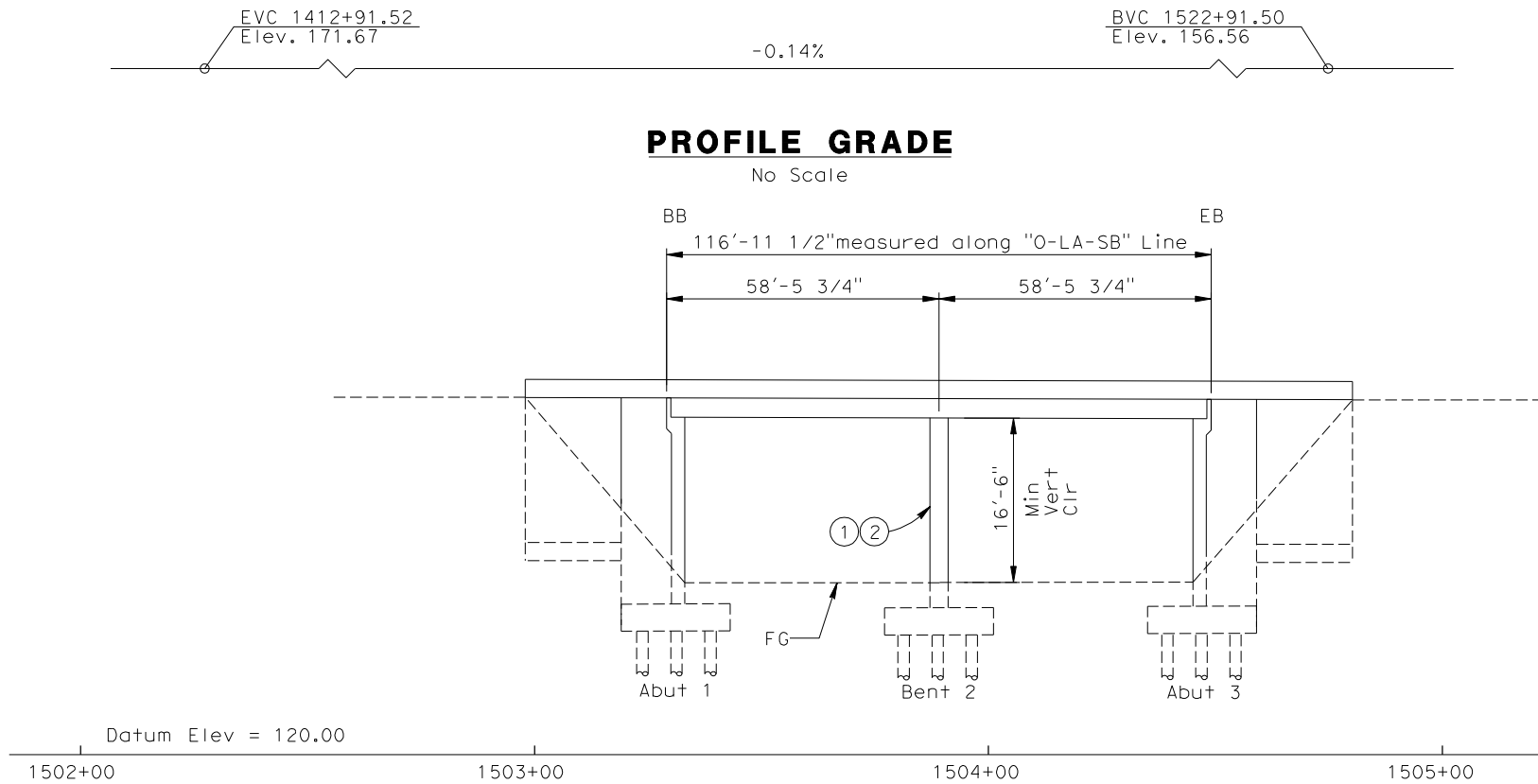
DESIGNED BY W. H-H CHU
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IN CHARGE D. THOMSON
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ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT
BROADWAY UNDERCROSSING

CONTRACT NO. HSR06-0005
DRAWING NO. S0402
SCALE AS SHOWN
SHEET NO. 239 OF 245



ELEVATION

Vert: 1":10'
Horz: 1":20'

TYPICAL SECTION

1/8":1'

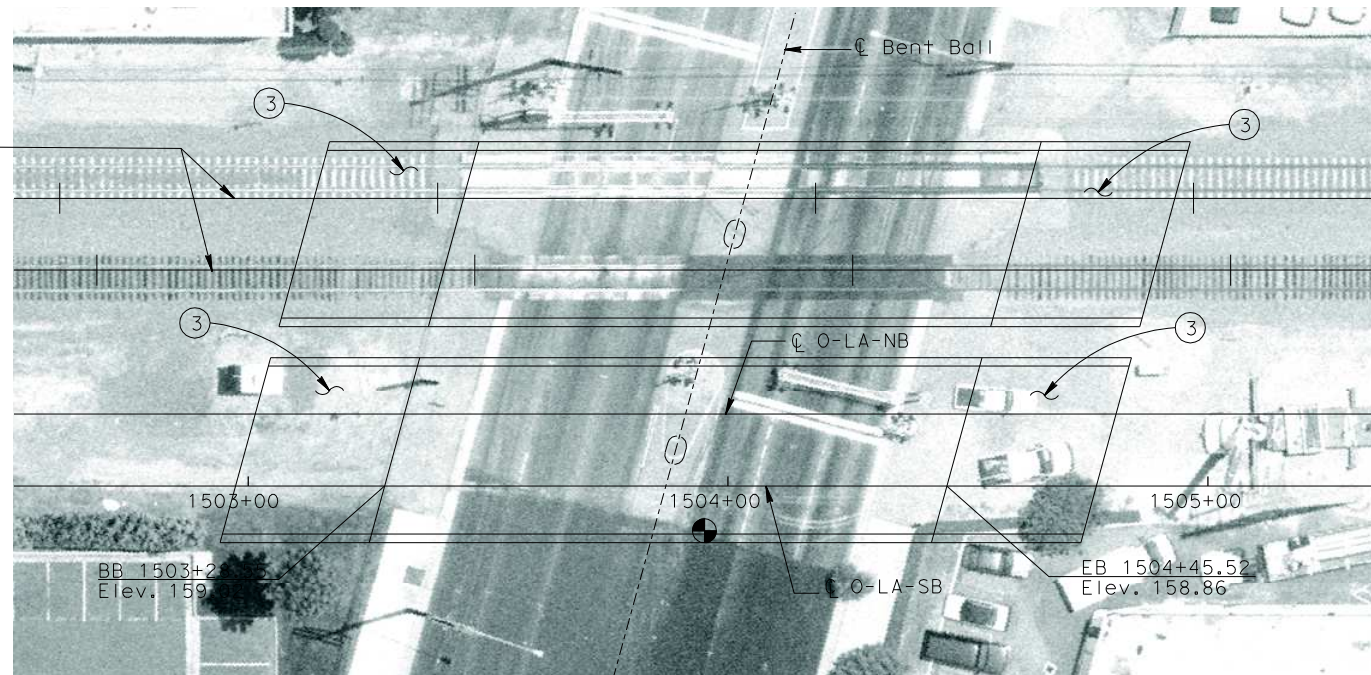


CL Freight/Passenger (New)

To Fullerton



To Irvine



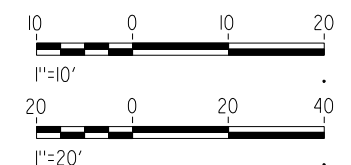
PLAN

1":20'

NOTES

- PAINT "BALL RD. UC"
 - PAINT "BRIDGE NO. 00-0000"
 - STRUCTURE APPROACH SLAB
 - FUTURE UTILITY OPENING
 - GUARDRAIL/ACOUSTIC BARRIER
- INDICATES POINT OF MINIMUM VERTICAL CLEARANCE

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REV	DATE	BY	SUB	APP	DESCRIPTION

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DATE 02-13-09



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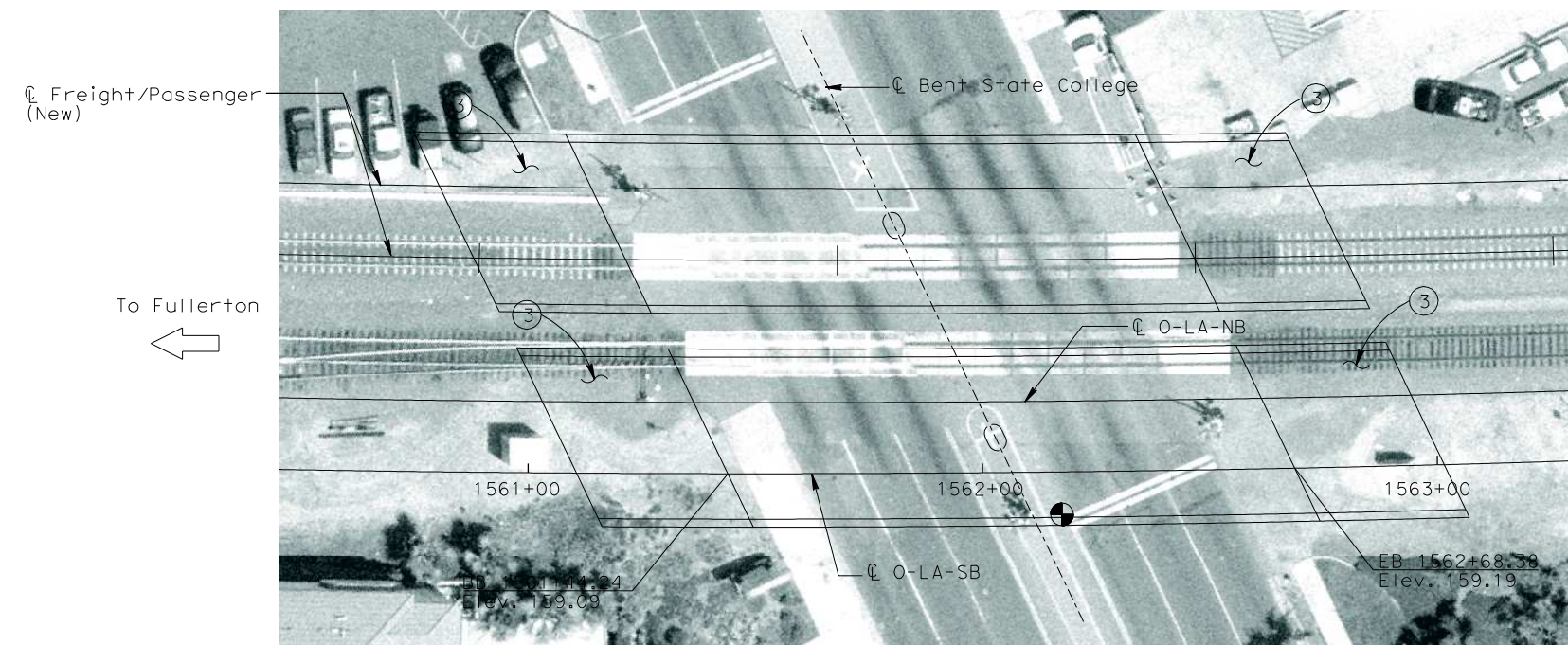
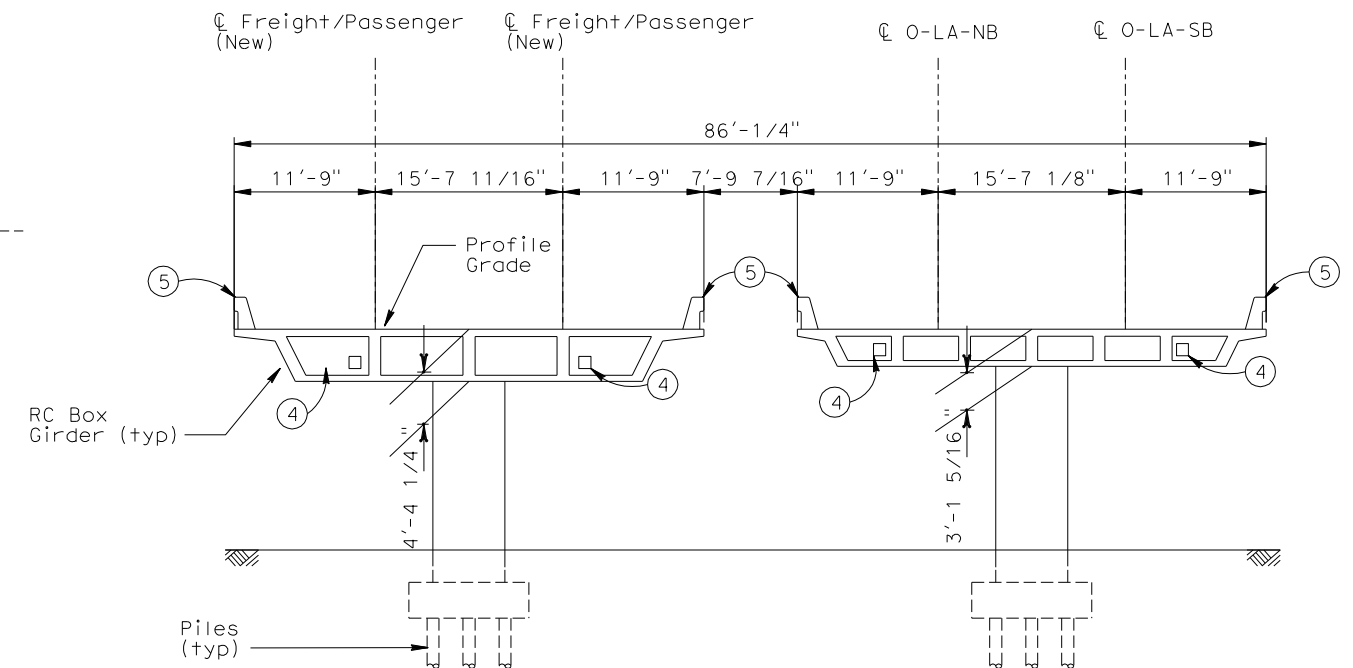
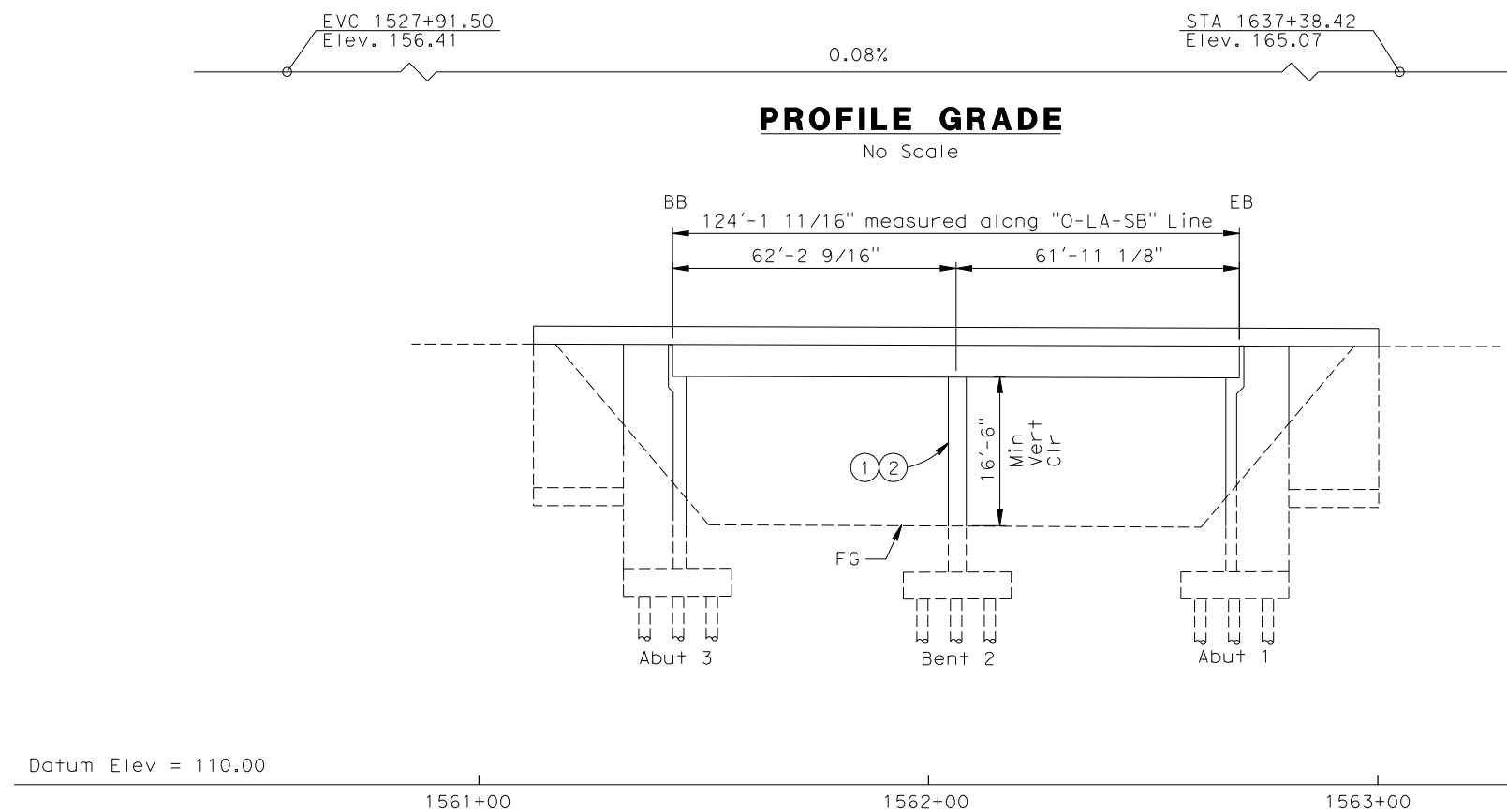
CALIFORNIA HIGH SPEED RAIL AUTHORITY

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CALIFORNIA HIGH SPEED TRAIN PROJECT
ORANGE COUNTY TO LOS ANGELES
BASELINE ALIGNMENT

BALL ROAD UNDERCROSSING

CONTRACT NO. HSR06-0005
DRAWING NO. S0404
SCALE AS SHOWN
SHEET NO. 241 OF 245



ELEVATION


Vert: 1":10'
Horz: 1":20'

TYPICAL SECTION

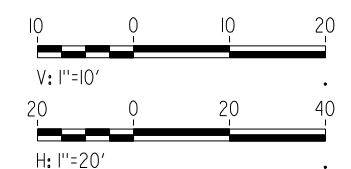
 $1/8" : 1'$

- NOTES

 - ① PAINT "ST. COLLEGE UC"
 - ② PAINT "BRIDGE NO. 00-0000"
 - ③ STRUCTURE APPROACH SLAB
 - ④ FUTURE UTILITY OPENING
 - ⑤ GUARDRAIL/ACOUSTIC BARRIER

 INDICATES POINT OF MINIMUM VERTICAL CLEARANCE

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